

CITY OF VALLEJO PLANNING COMMISSION

Charles Legalos, Chairperson
Kent Peterman, Vice Chair
Robert McConnell
Norm Turley
Gail Manning
Bruce P. Gourley
Suzanne Harrington Cole

MONDAY
3 DECEMBER 2007

7:00 P.M.

City Hall
555 Santa Clara Street
Vallejo, California 94590

Those wishing to address the Commission on a scheduled agenda item should fill out a speaker card and give it to the Secretary. Speaker time limits for scheduled agenda items are five minutes for designated spokespersons for a group and three minutes for individuals.

Those wishing to address the Commission on any matter not listed on the agenda but within the jurisdiction of the Planning Commission may approach the podium during the "Community Forum" portion of the agenda. The total time allowed for Community Forum is fifteen minutes with each speaker limited to three minutes.

Government Code Section 84308 (d) sets forth disclosure requirements which apply to persons who actively support or oppose projects in which they have a "financial interest", as that term is defined by the Political Reform Act of 1974. If you fall within that category, and if you (or your agent) have made a contribution of \$250 or more to any commissioner within the last twelve months to be used in a federal, state or local election, you must disclose the fact of that contribution in a statement to the Commission.

The applicant or any party adversely affected by the decision of the Planning Commission may, within ten days after the rendition of the decision of the Planning Commission, appeal in writing to the City Council by filing a written appeal with the City Clerk. Such written appeal shall state the reason or reasons for the appeal and why the applicant believes he or she is adversely affected by the decision of the Planning Commission. Such appeal shall not be timely filed unless it is actually received by the City Clerk or designee no later than the close of business on the tenth calendar day after the rendition of the decision of the Planning Commission. If such date falls on a weekend or City holiday, then the deadline shall be extended until the next regular business day.

Notice of the appeal, including the date and time of the City Council's consideration of the appeal, shall be sent by the City Clerk to all property owners within two hundred or five hundred feet of the project boundary, whichever was the original notification boundary.

The Council may affirm, reverse or modify any decision of the Planning Commission which is appealed. The Council may summarily reject any appeal upon determination that the appellant is not adversely affected by a decision under appeal.

If any party challenges the Planning Commission's actions on any of the following items, they may be limited to raising only those issues they or someone else raised at the public hearing described in this agenda or in written correspondence delivered to the Secretary of the Planning Commission.

If you have any questions regarding any of the following agenda items, please call the assigned or project planner at (707) 648-4326.

- A. ORDER OF BUSINESS CALL TO ORDER
- B. PLEDGE OF ALLEGIANCE
- C. ROLL CALL
- D. APPROVAL OF THE MINUTES: November 19, 2007
- E. WRITTEN COMMUNICATIONS: None.
- F. REPORT OF THE SECRETARY

Upcoming items, December 17, 2007:

- 1. Administrative Permit 07-0017 for a temporary storage building for Vic's Wheelhouse located on the Front Room parcel. Staff Planner: Katherine Donovan.

G. CITY ATTORNEY REPORT

H. REPORT OF THE PRESIDING OFFICER AND MEMBERS OF THE PLANNING COMMISSION AND LIAISON REPORTS

- 1. Report of the Presiding Officer and members of the Planning Commission
- 2. Council Liaison to Planning Commission
- 3. Planning Commission Liaison to City Council

I. COMMUNITY FORUM

Members of the public wishing to address the Commission on items not on the agenda are requested to submit a completed speaker card to the Secretary. The Commission may take information but may not take action on any item not on the agenda.

J. CONSENT CALENDAR AND APPROVAL OF THE AGENDA

Consent Calendar items appear below in section K, with the Secretary's or City Attorney's designation as such. Members of the public wishing to address the Commission on Consent Calendar items are asked to address the Secretary and submit a completed speaker card prior to the approval of the agenda. Such requests shall be granted, and items will be addressed in the order in which they appear in the agenda. After making any changes to the agenda, the agenda shall be approved.

All matters are approved under one motion unless requested to be removed for discussion by a commissioner or any member of the public.

K. PUBLIC HEARINGS

- 1. Parcel Map 04-0006 Extension for small lot subdivision located at 715-717 Ohio Street. Proposed CEQA Action: Exempt. Staff Planner: Marcus Adams, 648-5392. [Consent Calendar]

Staff recommends **approval** based on the findings and conditions.

- 2. Site Development 05-0007 is an application for a single-family residence in the View District located at 145 B Street. CEQA: Categorically Exempt. Staff Planner: Katherine Donovan, 648-4327.

Staff recommends **approval** based on the findings and conditions.

3. Tentative Map 07-0002, General Plan Amendment 07-0001, Zoning Map Amendment 07-0002, and Planned Development 07-0007 are applications for a 214-unit subdivision, including a 4.1 acre neighborhood park, on property previously developed with ball fields and a drive-in theater on the northeast corner of Rollingwood and Benicia Rd. Proposed CEQA Action: Mitigated Negative Declaration with Monitoring Plan. Staff Planner: Katherine Donovan, 648-4327.

Staff recommends **approval** based on the findings and conditions.

L. OTHER ITEMS

None.

M. ADJOURNMENT

MINUTES

- A. The meeting was called to order at 7:00 p.m.
- B. The pledge of allegiance to the flag was recited.
- C. ROLL CALL:
- Present: Harrington-Cole, Gourley, Manning, Legalos, Peterman, McConnell, Turley.
- Absent: None.
- D. APPROVAL OF THE MINUTES.
- Commissioner Peterman made a motion to approve the minutes of the meeting of November 5, 2007.
- Please vote.
- AYES: Harrington-Cole, Gourley, Manning, Legalos, Peterman, McConnell, Turley.
NOS: None.
ABSENT: None.
- It is unanimous. Motion carries.
- E. WRITTEN COMMUNICATIONS
- None.
- F. REPORT OF THE SECRETARY
- Michelle Hightower: Good evening Commissioners. On your December 3 agenda, you have three items coming forward. The first is an item that is being continued from tonight. It is a Vesting Tentative Map, General Plan Amendment, Zoning Map Amendment, and a Plan Development Application for a 214 unit subdivision, and a 4.1 acre neighborhood park that is the project being proposed by KB Homes. Also, you will have a Site Development Application for a single-family residence in the View District that's located on 145 B Street, and another item that is not on your agenda this evening but will be on the December 3 meeting agenda, is a Study Session for the Inclusionary Housing Ordinance that is coming back to you for review.
- G. CITY ATTORNEY REPORT
- None.
- H. REPORT OF THE PRESIDING OFFICER AND MEMBERS OF THE PLANNING COMMISSION AND LIAISON REPORTS
1. Report of the Presiding Officer and members of the Planning Commission – None.
 2. Council Liaison to Planning Commission – None. The liaison is not present.
 3. Planning Commission to City Council – None.
- I. COMMUNITY FORUM

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Chairperson Legalos: We have no cards.

J. CONSENT CALENDAR AND APPROVAL OF THE AGENDA

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All matters are approved under one motion unless requested to be removed for discussion by a commissioner or any member of the public.

Chairperson Legalos: On this evening's agenda, I would like to have item K-2 moved to K-1, and K-1 moved to K-2. Can we have a motion for approval of the Consent Calendar and the Agenda, please?

Commissioner Peterman: I move that we approve the Consent Calendar and the Agenda with the changes proposed by Chairperson Legalos.

Please vote.

AYES: Harrington-Cole, Gourley, Manning, Legalos, Peterman, McConnell, Turley.

NOS: None.

ABSENT: None.

It is unanimous. Motion carries.

K. PUBLIC HEARINGS

1. Tentative Map **07-0002**, General Plan Amendment **07-0001**, Zoning Map Amendment **07-0002**, and Planned Development **07-0007** are applications for a 214-unit subdivision and a 4.1 acre neighborhood park on property previously developed with ball fields and a drive-in theater on the northeast corner of Rollingwood and Benicia Rd. Staff Planner: Katherine Donovan, 648-4327.

Staff recommends a **continuance** to the meeting of December 3, 2007.

Commissioner Peterman: I move that we continue the item.

Please vote:

AYES: Harrington-Cole, Gourley, Manning, Legalos, Peterman, McConnell, Turley.

NOS: None.

ABSENT: None.

It is unanimous. Motion carries.

2. Planned Development **07-0001**, Tentative Map **07-0002**, Zoning Map Amendment **07-0001**, and Minor Exception **07-0001** are applications for a 24-unit townhouse complex located on El Dorado between Illinois and Arkansas.

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Proposed CEQA Action: Mitigated Negative Declaration. Staff Planner:
Katherine Donovan, 648-4327.

Katherine Donovan: Good evening Chairperson Legalos and Commissioners. First, I would like to announce that the mitigation monitoring program inadvertently got left out of your packet so that is what you saw before you when you sat down tonight. The project is located on the west side of the 1700 block of El Dorado Street, one block west of Broadway, and one block north of Tennessee. It is currently in use as an auxiliary storage lot for the former moving and storage company across El Dorado Street from the project. The proposed project would provide 24-townhomes in four buildings, two fronting on El Dorado Street, and two fronting on a Paseo, located between the project and the adjacent project to the west. The buildings would be three stories, with two-story elements on the corners to reduce the perceived height of the buildings. They would have two-car garages on the ground floor and living space on the upper floors. The garages would be accessed by a single driveway running from Illinois Street to Arkansas Street. There is a tot lot and a pocket park which would separate the adjacent buildings. The instrumentation of the project would require a Zoning Map Amendment to rezone the property from Intensive Use Limited to Planned Development Residential. A Master Plan Unit Plan for the specific design of the project, a Tentative Map to subdivide the six existing parcels into 24 individual parcels, and one remainder parcel, which contains the driveway, the tot lot and pocket park, and some of the landscape areas, and a Minor Exception of four inches to the required interior dimension of the covered parking spaces, and also to eliminate the five required guest parking spaces. The parcel is a transition parcel; it's between the commercial and industrial uses to the east and the low and medium density residential uses on the south and west. The design of the project is a more traditional design, and that has been done to make it compatible with the existing residential development in the vicinity. The Minor Exception to the interior of the garage is not considered significant, as when I looked at different jurisdictions, the actual size requirement for a garage is varied widely from an eight foot width, to a nine foot width, to the ten foot width that we require, so four inches seemed very insignificant, and not a safety factor. The exception to the guest parking, because the project has only the two curb cuts, one on Illinois, and one on Arkansas; there is more on-street parking than would normally be found in a project of this type where you would have individual driveways, significantly lowering the amount of on-street parking that would be available. Additionally, although the parking could be provided on-site, it would require removing the tot lot and the pocket park, and, we felt that having those open space amenities, significantly improved the project overall. Also, before I close, I wanted to make a couple of corrections on the Conditions of Approval, Page 1: The landscaping conditions should read, "fourteen street trees," not, "four street trees," and at the very end of that landscape conditions, there was a number "h" that was inadvertently left in from another project. It does not refer to this project, which is probably why it didn't make sense when it referred to Sonoma Boulevard. One other correction, in the requirements for lifting the project out of the flood zone, there was a requirement for a Final Map Revision from FEMA. The timing of that is that it should occur prior to final occupancy. The document referred to "prior to building permit approval," but that is not correct. And, Staff recommends that the Planning Commission forward a recommendation of approval on the Zoning Map Amendment and the Plan Development and approve the Tentative Map 07-0002 and Minor Exception 07-0001, subject to the findings and conditions contained in the Staff Report and Resolution. And, if you have any questions, I would be happy to answer.

Commissioner Gourley: Would this project generally be considered in-fill?

Katherine Donovan: Yes, definitely.

Commissioner McConnell: Thank you Mr. Chairperson. Counting the lot to the immediate west of this application where I believe there is a small duplex located, this application will border right onto that lot. Is that correct?

Katherine Donovan: That's correct.

Commissioner McConnell: Okay and it is going to replace only the storage yard of the moving company that is located at that location now. Is that correct?

Katherine Donovan: It is that whole section there. There is also a small, sort of industrial building, corrugated metal building on the corner. That would also go.

Commissioner McConnell: Okay, what impact is this going to have upon the moving company with respect to where they are going to place all of those containers that they have.

Katherine Donovan: The moving company has already moved, basically. They have re-located out of town, and I am not sure the provision for getting all that stuff out . . . if the moving company still has some responsibility or not, but it would be as part of the project. It would all be taken away.

Commissioner McConnell: Are there any present proposals for the use of that storage facility building at this time?

Katherine Donovan: I have heard rumors, but there is nothing specific that has been submitted to the Planning Division.

Commissioner McConnell: How many additional spaces on the street do you anticipate being freed up by the fact that we are only going to have two curb cuts?

Katherine Donovan: It was in the Staff Report, and I don't remember off the top of my head. I think it was in the vicinity of eight to ten.

Commissioner McConnell: Okay. Thank you.

Commissioner Turley: Thank you Mr. Chairperson. Did Mr. McConnell just refer to the storage facility on this piece of property?

Katherine Donovan: He asked if the company that is using this piece of property that is located across the street . . .

Commissioner Turley: I was thinking about the one that is on the property. That is being torn down, right?

Katherine Donovan: All of the buildings on the property are going to be torn down.

Commissioner Turley: Do you plan on replacing the broken curb, gutter, sidewalk, and pavement, on through streets?

Katherine Donovan: Yes, that is always a requirement of projects.

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Commissioner Turley: At first I thought that maybe this project might be too high in terms of forty feet. I do notice there is a two-story apartment house right nearby which might be twenty-five feet high, and then; there is a church. Anyway, there is Old Path Miracle Cathedral on Arkansas Street. That church is pretty high, in terms of maybe around forty or fifty feet.

Katherine Donovan: I don't think it is quite that high, but it is quite tall.

Commissioner Turley: With the cross . . . and then there is a two-story house on Napa Street which might be twenty-five feet high, and then; there is that fort masonry storage building on El Dorado Street, right there at the corner, across from the project. And, then on page 4, No. 6, it mentions more parking on that private driveway to be used as a fire lane, and I was under the impression that the tenants, or the owners, would be parking on that private driveway.

Katherine Donovan: They would be using the private driveway to access their garages. They would be parking in the garages.

Commissioner Turley: But, they would not be parking on this private driveway?

Katherine Donovan: No.

Commissioner Turley: Okay. On the project description, in the first paragraph, it refers to a low-gated fence. Could you tell me what kind of a fence you are referring to?

Katherine Donovan: I believe we can ask the applicants to be sure. The last time I discussed this with them, they were considering a wrought iron type fence, about three feet high, that would just border the yards to provide a safe play area for children.

Commissioner Manning: Katherine, a couple of questions on the garages themselves. There is really no place for them. They have to pull into the garages? Right? There is no . . .

Katherine Donovan: Right. There is no alternative.

Commissioner Manning: And then, just one point of clarification, on page 4 from the Fire Requirements, it says: "If security gates are desired." There are no security gates?

Katherine Donovan: They are not proposing security gates. This is a standard condition from the Fire Department, so that, if at some point, security gates at the driveway entrance were requested, they would have this special lock box that the Fire Department could access and get into the . . .

Commissioner Manning: But, that's not part of the . . . ?

Katherine Donovan: No, it is not part of it.

Chairperson Legalos: Ms. Donovan, this is a loaded question. Regarding the requirement for automatic fire sprinkler and extinguisher systems, we don't generally require that in residential properties. Is that a standard requirement also of the Fire Department?

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Katherine Donovan: It is, and I am not sure if you are aware, but as of January 1, we will be adopting the 2007 Building Code, and it is a requirement in the 2007 Building Code, so we will be seeing fire sprinkler systems in all new construction.

Chairperson Legalos: Including single-family homes?

Katherine Donovan: I am not 100 per cent sure on the single-family, but on commercial, industrial, and multi-family, we will.

Chairperson Legalos: Because, the only case I can recall that we required them on single-family homes, was in Phase Two, Sycamore Place because of site-offsets that, I think, were only three feet total.

Katherine Donovan: A lot of the changes in the new Building Code are related to safety issues. So, there is a lot of ADA access and fire safety type things.

Chairperson Legalos: Speaking of ADA access, that issue was raised in Attachment 7, as to the lack of elevators. Is that also a requirement in a three story building?

Katherine Donovan: The architect could probably tell you more carefully, but I was the interpreter between the chief building official and the architect, and it appears that they would not be required. However, he could give you the ins and outs more than I could.

Chairperson Legalos: Attachment 7 also mentions a concern about the backyards of three of the residents in the adjacent partnership development projects, facing the applicant's minimum side setbacks. Can you tell me what the significance is of that?

Katherine Donovan: I think that the adjacent property owner is concerned about the project that he has been approved to build in that there will be an impact upon the privacy of those backyards. He is concerned because this project actually has a fifteen foot setback to the building line and, as you will notice, the third floor steps back in a little further, except on two of the units.

Chairperson Legalos: Because of the height?

Katherine Donovan: Because of the height of the building.

Chairperson Legalos: Right. He also mentions upper story balconies on six of the units, but it seems like these are two different things. There is a statement, "upper story balconies of six units overlook the backyard." And then, there is a statement that the backyards of three of the residences in the adjacent partnership development, "face the applicant minimum side setbacks." How is that an issue if the backyard faces the side setback?

Katherine Donovan: Well, I think his concern is simply that he feels the project would have an impact on privacy. However, when we are reviewing this as a project, we keep in mind that as it is now, the zoning would allow a seventy foot high building to be built there which would have significantly further impacts, and in addition; there is a fifteen foot setback which is more than we require on a single-family home. You could easily put a two-story, single-family home next door with no significant changes that would only be five feet away. So, we didn't think that this was an unreasonable . . .

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Chairperson Legalos: Well, that is why I am asking. I didn't see the significance of that. It didn't seem unreasonable to me either. Then, the last concern in Attachment 7, was potential contamination from vehicle repair and maintenance uses on that parcel. Do we know that that is the case? That those uses were there?

Katherine Donovan: It has been cleared. They have done all of their environmental work. I got documentation. It was signed off by the County Health Department. So, the site is clean.

Chairperson Legalos: Then, I had a couple of other questions. Has any consideration been given to installing solar panels?

Katherine Donovan: I believe, and you can ask the applicant, that was going to be offered as an option. They are very interested in using green materials on this building and I think, at the building permit stage, we will see a lot more of that, but they have been talking about different green techniques. I know that was one of the options.

Chairperson Legalos: Then, in connection with that also . . . in the past the Commission has asked that energy-style appliances be installed, and I would like to ask about that.

Katherine Donovan: That's a building code requirement.

Chairperson Legalos: So, we don't have to make it an explicit condition any more? And, my last comment is: I didn't see any requirement, and this also may be in the new landscaping law, for continuous landscape maintenance, or landscape maintenance of any kind. Is that because it is included in the new law?

Katherine Donovan: It will be part of their CC&Rs. There will be a Homeowner's Association and CC&Rs, and we will review it at that time. That will be part of their requirements.

Commissioner McConnell: Katherine, two quick follow-up questions. If this project was reduced to two stories, what impact, if any, would that have upon the privacy factor of the adjoining property?

Katherine Donovan: I really don't think it would have a significant impact because on a two-story project, they are going to be looking over any six foot fence that would be between the two parcels, and if you look at the elevations; the three-story element is set back from, even more than the fifteen feet of the entire building. The three-story portion is set back quite a bit, so, I don't think lowering it a story would significantly increase the privacy of the property next door.

Commissioner McConnell: Would adding a screening requirement between the two lots provide for additional privacy?

Katherine Donovan: I think the height of the screening would have to be significant enough that it would cause those backyards to be so shaded that they would be unpleasant to use, which would pretty much defeat the purpose.

Commissioner McConnell: The weather in opposition raises the point about runoff in that there would be excessive runoff as a result of the ground cover. Has there been any thought given about that argument by staff?

Katherine Donovan: Actually, since the entire site is currently paved, and once this project is completed, there would be significant landscaping areas; it would actually reduce runoff. In addition, as part of the project, all runoff from the site would have to be delivered into the storm drain system. That is a standard requirement.

Chairperson Legalos: I just had one other technical question. Are we considering a Tentative Map or a Vesting Tentative Map, this evening? I believe you said Tentative Map.

Katherine Donovan: I believe it is a Tentative Map, but let me double check. Yes, it is a Tentative Map, and I have one other thing I want to say. I was supposed to say that you would also be adopting the Mitigated Neg Dec for the project.

Chairperson Legalos: Thank you. If there are no further questions from the Commission, I will open the Public Hearing. Does the applicant wish to address the Commission?

Thank you Mr. Chairperson. For the record, my name is Fred Sessler. I am a commercial real estate broker here in Vallejo, and I am representing the property owner who will also perhaps be the co-developer of the property, Mr. Buck Kamphausen. Basically, we are planning a local ownership project. These will be properties built for sale, to be occupied, hopefully to work-force people. We like the project area because we are close to Kaiser Hospital, and we think that nurses coming into the City of Vallejo, particularly with Kaiser's last expansion, will require this type of housing. When Mr. Kamphausen called me to look for a piece of property, we actually started out looking for a warehouse site, which this property is zoned for. As you well know, Mr. Kamphausen is an antique car collector and also one of the largest antique car auctioneers in the United States, and we have determined that this particular piece of property, somewhere around 39,000 square feet, could accommodate a 20,000 foot warehouse. Mr. Kamphausen took a ride by one day, and we drove around the neighborhood, and he said a warehouse wouldn't fit in this particular property. We called off our plans to do a warehouse. At this time, he advised me that he would like to do some kind of project that would be good for the neighborhood. And, I have a friend of mine from Southern California who would be the co-developer on this particular project, Mr. Glen Gordon, who is with us this evening. He has done projects of this type and is somewhat of an expert in this type of development. So, basically, by abandoning the warehouse, and a little bit of local ownership, we have a sincere interest in fulfilling some of the goals and objectives that the City Council has set out. One question was asked: "Is it in-fill?", and it most certainly is. It will hit a niche in the market that nobody else has, at least, achieved at this point and time. In answer to Commissioner McConnell's questions, the moving company is on month-to-month, and we can move them out anytime we wish. They are occupying a small, 5,000 square foot warehouse on the northeast corner of the existent building, not onsite, but on the Broadway frontage. For the question of the runoff, I would like to introduce to you, the experts that we have with us this evening. Bob Karne from Karne Engineering is here, and he will answer any questions of a technical nature. We have the architect who specializes in this type of project, Ray Haggy, and Ray has some answers to a couple of the questions which you folks have brought up. The co-

developer, Glen Gordon is here, and, if you want the three of them to come up here, Mr. Chairperson, and you may fire some of your more technical questions at them. We will take them on, one by one. I thank you and I think we are trying to go by the books, and we hope for your approval of the project. Thank you.

Commissioner Peterman: I am not sure exactly whether to ask this question or not, but near the Eldorado Village Master Plan, under the Pedestrian Areas and Public and Private Open Spaces, the last sentence reads: "In addition, each Unit's private open space is also oriented towards either the streets or the Paseo, providing a village-like arrangement of defensible, private space that encourages community interaction." I am just unclear as to what defensible private space is. I have lots of images running through my mind, including large domes on top of the units, but I am sure that is not what it is.

My name is Ray Haggy. That's not what we are proposing. I am the architect. That language comes from something I learned when I was at U.C. Berkeley, as a student. On a multi-family complex, you want to make sure that you don't create hidden places where people can hide. That's when crime takes place when you have a large, block-sized project. We call it defensible in that it is visible. It makes more sense, when your neighbor can see your yard as well as you, your kids, your pets. You're safer than in a lot of places where you have got everything oriented where you can't watch somebody else's front door, or their back door or see what is going on. That is all I really meant by that, is the idea that the little pocket park and the tot lot just makes a place where people can interact with one another, because people like to be able to walk and talk with their neighbors.

Commissioner Peterman: Yes, I remember the very first project that I voted on in the Planning Commission was a very similar project, and they had absolutely no fenced yards for children and so, I applaud you, not only for the yards but putting in the tot lot and the pocket park. Thank you.

Commissioner McConnell: Thank you Chairperson Legalos. Mr. Haggy, could you explain for me the philosophy behind using three stories here in an attempt to diminish what might appear to be an otherwise, very tall building?

Glen Gordon: My name is Glen Gordon. I am actually the co-applicant, and I am going to let Ray speak to this too, but I will tell you, first of all, just one other comment on the defensible area. Really, what also that is, is those are the *private* areas for the individual units as well. So, you have the common areas with the adult lot and the tot lot, and then you have also, your private areas that again, as Ray was saying, that are open, but they are the private areas for each unit as well. With respect to the three-story and why we went with this scheme right now: one of the things that this is, as we talked about before, is an in-fill lot. I had a number of different challenges when Buck and I sat down on this. The primary one was: how to create housing in a market today where prices have escalated. Now, true, we are having a softer market with the "housing crisis," supposedly, but one of the things that has not come up very much in the press, is the cost of construction and how much that has gone up in the last three years alone. I can tell you right now that the cost of construction really is not coming down. One point is the major component of labor and construction costs, but the other major component is your materials. This has actually gone into a global marketplace today. When we are talking about concrete, copper, plastics, petroleum products, and everything else, all of these are affected, and; this is one area that has not come down. So, one of the aspects of the planning of this project originally is that Ray and I go back quite a bit, and we are able to sit down

and trade ideas. The idea of creating units 1) for sale - trying to hit a target market where we are more affordable than any single-family home that can be built basically on a for sale basis, on a new lot today. 2) Also programming this for families, and when we call work-force housing or family housing and stuff like that where we have actually programmed this, not just for, say, an immediate family, but whether it is parents who may want to live with the family as well. These units are designed for that, and that is what calls for the design of the unit itself where we have entry level; we have a separate unit downstairs on another side that will be facing a Paseo, or the private area of the patios and stuff. Then, you have your common areas within your units. These are your living, dining, and kitchen areas are on the second level, or in one scheme, we actually have bedrooms on that level too as well as a master bedroom up on top. This gives space. We are giving square footages. These are not actually very small units. We tried to give the most amount of square footage to a family, thinking of families and people like this. And, that is what dictated the design.

Commissioner McConnell: How does using the three-level building create an impression that it will be lesser than a three-story building? You are basically telling us. . .

Glen Gordon: We use by terracing back. In other words, not just flat walls going straight up. There could be good design even that way too. It is all worked into the design but what we do is that, you have a base level and you begin to have balconies and articulation that comes up and then you also terrace back on top of these units.

Commissioner McConnell: Okay, all of these units are going to be looking into this pass-through driveway, right?

Glen Gordon: No. The driveway. . . I don't know, Katherine, if we can put up the slide from the PowerPoint.

Commissioner McConnell: My concern is about privacy and noise control.

Glen Gordon: Basically what you have is, in a sense the driveway is an internal street, if you want to call it that. By having the internal street where these people pull in, all of their garages are internal to the project, therefore, they are not facing anyone else's home. They also act as sound barriers.

Ray Hagg: One of the design concepts of the projects is that they are all provided with horizontal setbacks and what we use in this project is called a vertical setback so at each floor of the building it starts to what we call "layer-cake," or step back as you go up. This is so you have at the edges . . . you always want to be stepping down to like a two-story area, and then, we have also really increased the setbacks between us and the adjoining property with that Paseo in the back splitting it up between a circulation space right along the property line, and still providing each tenant with a private yard area in front of their unit. I think the neighbor next door is concerned that we designed a better product.

Ray Hagg: And, this is the driveway where you see the crosshatch pattern, the stamped patterns. Your cars enter there (words not understandable) the garages are all (not understandable) these units or these twelve units here face the street and those face the Paseo and cars are coming in (not understandable).

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Glen Gordon: This project also is ADA compliant. As you can see, on the ground floor, on the front of each unit, there is a bedroom, a bathroom and the stairwell. (not understandable) You can circulate the entire side with a wheelchair.

Chairperson Legalos: Excuse me, gentlemen. Please use the microphone. These meetings are televised, and the public may not be able to hear what you are saying. Thank you.

Commissioner McConnell: Are there any additional design concepts that you can think of that might add some added privacy to the neighboring property that is concerned about it?

Glen Gordon: We plan to have a landscape area there anyway, but, as Katherine was saying: 1) The wall requirement has a height requirement to it. This limits the height of it. As we can plan certain landscapings there, they can go above the wall, but at a certain point, what you start doing is shading as opposed to therefore creating darker spaces which we don't want to do. It is not good for the neighbor. It is not good for our project as well.

Ray Haggy: Also consider, we have a fairly large setback. I bet it is probably larger than the one he has proposed on his site, so he could move his buildings back the same distance we are.

Commissioner McConnell: Okay, has there been any thought given to having double paned windows to cut down on the noise.

Glen Gordon: As far as all of the energy requirements, we are required by Codes today to do that.

Commissioner McConnell: And, also to cut down on noise proliferation?

Glen Gordon: All of the windows will be double paned.

Commissioner McConnell: As a matter of fact, one of our challenges today is what you asked about before, and Katherine spoke to it. As far as green, we will study everything we possibly can. Again, this is like walking a tightrope in terms of trying to keep costs under control and everything else, but we are looking, besides any code issues that there may be to meet, which is just standard procedure for us, we will look at everything that is possible in terms of other materials and other forms of building as far as green is concerned with respect to the project. And, what type of controls will the Homeowner's Association place on the private park and the tot lot so that we don't hear complaints in the future about neighbors or other people coming in and using the facility?

Glen Gordon: We handle that during the CC&R stages. You can have people, if they are invited into the project. This is private property. This is not open. This is not a public park. These are not public parks or anything like that. It will be the responsibility of the Homeowner's Association to police that, and there will be remedies written into something. There are rules and regulations that all of the homeowners will have to sign when they purchase their units.

Commissioner McConnell: So, you are going to put the onus on the Homeowner's Association to enforce that requirement?

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Glen Gordon: No, in the development stage, we create rules and regulations. After they are turned over to the Homeowner's Association, they have the power at that time to change certain things. We don't have any power at that point in time.

Commissioner McConnell: You are pretty close in distance to Vallejo High School, so you will probably have some people walking through there, I would suspect. Finally, what price range do you anticipate these units ultimately being sold for?

Glen Gordon: Somewhere in the \$400,000 range. Hopefully, in the low \$400,000 range.

Commissioner McConnell: So, in 24 units, that is a pretty good return. Thank you.

Commissioner Turley: I read this material here, but I don't recall reading anything about the size of the garages. Would those be double garages, side by side?

Glen Gordon: Yes, that is exactly what those are.

Commissioner Harrington-Cole: Thank you. Can you tell me approximately how many people you think will end up living on this lot?

Glen Gordon: I haven't actually done a calculation, but in fact all of the units right now are designed between three and four bedrooms.

Commissioner Harrington-Cole: So, would you say 100 people is not out of line?

Glen Gordon: I would say somewhere between 75 to 100 people. Something like that.

Commissioner Manning: Do you have any sense of when you would start to build this project?

Glen Gordon: This is the first of the planning procedures. I would like to thank the City and the staff, especially Katherine, for their help in helping marshal this through. As we know, the next phases will be to go into actual construction documentation on this and then proceed forward from there. So, we are looking, hopefully, within the next twelve months or so to start.

Commissioner Manning: You are not going to wait for any changes in the housing market or anything like that?

Glen Gordon: I am pretty aggressive, and there are certain times where we see opportunities. Again, one of the things that we are looking at is affordability, and with brand new units at this price, there really is nothing on the market place. Also, looking forward down the road, we have crises but you probably wouldn't even see at the earliest, this coming on line, until probably late to mid June of 2009. At that point in time, I feel that that would probably be a good time to be on the market.

Commissioner Manning: Okay, thank you. And, one more question: In the meantime, the lot, the tenant that is on there, you say you are renting to him month to month, what plans do you have while you are going through the process here, to clean that area up and make it . . . ?

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Glen Gordon: Actually, the cleanup on a lot of it has started already. It is cleaner than it was. There are less storage facilities there right now, and hopefully, he will continue to start cleaning that up. Again, he knows he is in a temporary position now, so it behooves him over a certain period of time to start sooner, other than later on the rest of the cleanups.

Commissioner Manning: Okay, thank you.

Commissioner Harrington-Cole: You know, I am sorry. I have read this, and I have listened to you, and I think it is just clicking. The front doors of the units that face El Dorado Street . . . those front doors will face El Dorado Street? That's their front door?

Ray Haggy: That's correct.

Commissioner Harrington-Cole: But, what you are expecting them to do, is to drive around and park in the garage to go in, and not park on the street and go in their front door?

Ray Haggy: No. If you were a resident, you would park in your garage and there is a door into your house. So, we assume that the people who live in the project, the residents, the owners, will park in their garage. Any visitors would park in the street and would walk in. That is why we wanted to create, literally, a second street or what we call a Paseo, which in Spanish is just a "walkway" or a "street" in the back.

Commissioner Harrington-Cole: Now, the people who live behind the first units, their garages will be on Paseo but their front doors will be where?

Glen Gordon: Their front doors are on Paseo, off of Paseo itself, so they're private area and the public access, or visitor access, I should say, is along the Paseo.

Commissioner Harrington-Cole: On the top?

Glen Gordon: Yes, the very top.

Commissioner Harrington-Cole: So if I was coming to visit you, I would park on the street and I would walk along that area until I got to your unit?

Commissioner McConnell: I wanted to follow up on a question that Commissioner Harrington-Cole asked about the number of people living there. Because it causes me some concern as to whether we are requiring sufficient parking or providing enough parking out there. You have ten three-bedroom units and fourteen four-bedroom units. That is 86 bedrooms. No guest parking at all. Reduced sized garages, which raises a question of where people are going to store their stuff, and I am concerned about whether or not we are saturating the neighborhood when all these people who are going to live out there start bringing in their one and a half to two cars per unit.

Glen Gordon: I will answer your first question. The actual requirement for guest parking on-site was five spaces, as it was stated that we could fit at least ten spaces along El Dorado itself right now. That was the reason for that. With respect to the garages and the variance on the width of that, that is really what is called for door opening and interior space. There are storage spaces within the

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garages themselves. These are basically twenty feet wide, but when you are looking at providing what is called "shear", and these are design issues and door openings, this is where the variance is coming in terms of the actual finished end-to-end. And, we are talking about from your Code to the Minor Exceptions, six inches in total of width, per unit, times 24 units. Basically what you are also looking at is the thickness of the walls too that are coming into that.

Commissioner McConnell: Would adding diagonal parking on El Dorado assist in freeing up more spaces?

Glen Gordon: I would not suggest that in more of a residential or transitional neighborhood. If you are going to use diagonal parking, it is really for commercial areas.

Commissioner McConnell: There are three churches in that area that I am aware of, and they do have evening services and activities, as well as traditional Sunday services out there. I can envision parking becoming a problem, particularly if the Hells Angels also have an event going on at the clubhouse just across the street. So, I am a little concerned about the parking adequacy, and I am just looking for some reassurance here.

Katherine Donovan: If I could address that question. Actually, the President of the Vallejo Motorcycle Club came in to discuss the project with us, and he had some concerns about parking. One of the things that he said is that the club and the churches in the vicinity are in contact with each other, and they stagger their events to make sure that they are not all having events at any given time. It does not appear that on a regular basis there is much of a parking problem. If you looked at the churches, you would notice they all have their own, fairly substantial, parking lots. He did say that occasionally, like on the really big football nights, the Vallejo Hogan game, or graduation; they can get parking all the way this far, but it is not a regular event, and in my mind it is sort of like when you are planning a shopping center. Do you plan the parking for the normal day or do you plan it for Christmas season? The general answer to that is, we plan it for the normal day. So, it is possible that there are times during the year that you might have to circle the block or go halfway down the block to find a parking space, but it wouldn't be the norm.

Commissioner McConnell: Do we have any other developmental plans out in that area at all? There is another empty lot, to the west, I think.

Katherine Donovan: There is an approved project on that empty lot to the west. That is the party who wrote that letter, and, that concerns kind of a townhouse or a single-family cluster type development, of six units.

Commissioner McConnell: That's all residential, right?

Katherine Donovan: That's all residential.

Chairperson Legalos: Do you have any further comments to make, gentlemen?

Glen Gordon: We ask that you please approve the project, and we appreciate your support.

Chairperson Legalos: We have no cards on this item, so, I will close the Public Hearing and bring the matter back into the hands of the Commission.

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Commissioner McConnell: I will move the adoption of the Zoning Map Amendment to rezone the property as Motion No. 1 based upon the findings and facts as stated in the Staff Report.

Commissioner Harrington-Cole: Can we have some discussion first?

Chairperson Legalos: Certainly.

Commissioner Harrington-Cole: I have a couple of issues. I walk that neighborhood all of the time. I live about six blocks over, and the dogs and I walk that often. My first issue is security. With the tot lot and the other lot there, and Hogan High School being so close; there are always people hanging on street corners and sitting on fences and hanging out in empty lots, every day. And, we just heard from Hyde Park that had fences and a much different neighborhood. I am very concerned about security at this site. I am very concerned that you're building an attractive place to hang out, and these are working people. They are going to be gone all day. My first concern is security. It is a small lot, in my opinion, and I think you are putting a lot of people on it. One hundred people on that space can't help but be congested and cause issues. My last concern is that I think people in my neighborhood have garages, but you park on the street and run in the front door. It is so much easier than going around and getting in the garage. So, I am still concerned about the parking issue. I also think maybe three stories is a little tall. I like the design, and I think it works, but I am concerned that it is going to overpower the neighborhood. So, I have a few issues I would like to talk about.

Commissioner Manning: I actually like this project very much, and one of the reasons is that it is an in-fill project. It does address a real need that we have in our community for this range of housing for our working people in our community. I do recognize Commissioner Harrington-Cole's concerns about the size, but I really think this is where buildings are going in the newer developments and everything. This project is more clustered. I think this is a real improvement for the neighborhood. What there is now is real blight, and what's bordering there is a lovely, little historic community from post-World War II, with some wonderful houses. This project and the one next to it which we heard about a few months ago and approved, are what I think is exactly what we need to be doing in Vallejo for in-fill housing. I think that parking in all of our communities is going to continue to be a problem, and I don't necessarily think that is a bad thing. I think that people need to learn to modify their lifestyles to adjust to that. So, I am going to vote for this, and I think the design is really well thought out. It is the right thing for the community. It is going to change the neighborhood. No project is perfect, and change is never comfortable, but I think this is going to be a real asset to our community. I thank the developers for coming here to do this.

Commissioner McConnell: I also have some of the concerns that Commissioner Harrington-Cole has expressed about the height appearances, the parking, and the congestion. This is why I raised the questions. However, this particular application consists of several different steps. The first step we are on is simply the rezoning of the property. The questions relating to the heights and the congestion and the security on the park are properly raised when we get to the Tentative Map plan, and maybe we can have a more meaningful discussion at that time about some of your concerns. So far, I am only offering one motion. We will get to the others in the due course of time.

Chairperson Legalos: I too am going to vote in support of this project. I am very pleased to see entry-level, new housing, being proposed for Vallejo. I think that

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some of Commissioner Harrington-Cole's concerns about loitering and vandalism may be mitigated by the fact that these are destined not to be rental units, but to be owned by the residents. In general, when we have a home ownership project, there are less problems of that nature. May we have the motion again, please.

Commissioner McConnell: This first motion is simply to approve the Tentative Map Amendment to rezone the property.

Commissioner Harrington-Cole: I am sorry, but this is a bit confusing. There are a number of resolutions in your packet. Perhaps Katherine can read it off, but I think the first one was to recommend to City Council that the Zoning Map be amended.

Commissioner McConnell: Yes, that's the extent of the first motion.

Please vote:

AYES: Harrington-Cole, Gourley, Manning, Legalos, McConnell, Peterman, Turley.

NOS: None.

ABSENT: None.

It is unanimous. Motion carries.

Commissioner McConnell: When I looked at this application, I broke it down into three different motions, not four, the way Katherine is putting them. The next one I had would be the Tentative Map itself to develop 24 units on individual lots. Now, that would authorize 24 actual units, the number of units that the applicant is asking for. If we are concerned about putting too many units on this one lot, then this would be the time to discuss those concerns on this motion. I initially had the same concern. I am concerned about concentration, about density. My calculations were with 86 bedrooms, at two and a half people per bedroom, you could have 196 people living there, with very few limited parking spaces, except for those on the streets. This was a concern that I reflected upon when I read the Staff Report. Between giving up the tot lot and the park, I am inclined to go with the application because I wouldn't want to lose those two amenities. If we have any other thoughts about mitigation or how to address that concern, I would be very happy to hear them at this time before I make the motion.

Commissioner Harrington-Cole: I do have concerns that this is going to be an extremely congested spot. I think there are too many units. I am concerned that we are going to cram a lot of people in a small space, and it is going to cause a lot of problems. So, I will probably vote against this.

Commissioner Turley: I think two and a half people per bedroom is a little excessive. I am thinking in terms of four people per family, and if there are 24 units, that would be 24 families, or roughly, 100 people. I have heard some pros and cons on this, but I think I would have to support this also.

Commissioner Gourley: I understand Commissioner Harrington-Cole's concern about the density, but I also applaud the fact that we are doing something which is not popular today. The in-fill projects seem to be the way to go. Her concerns about the security of the area, I think, will be answered by the fact that you would have people living there instead of people hanging out in an empty lot. If I am not mistaken, didn't this used to be a maintenance facility for garbage? Or, is that a block away?

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Katherine Donovan: I think that is a different location.

Commissioner Gourley: It is very close in that neighborhood, but again, the storage lots for a moving company, after dark, tend to breed people who hang out. The motorcycle club cleans its area very nicely, and they do a good job, and their activities with the church are commendable. I don't think we will have the issues, and any issues we might have, I think, are going to be mitigated by the fact that we do have a project with up to 100 or 200 people living there, depending on how happy a family they are.

Commissioner McConnell: I am also concerned about the concentration, but, the reality of our situation is that with the added people coming into California; we will either build out or we will build up. Those are our two choices, and, three stories designed to look like two is a reasonable accommodation. The reality is that as our town increases in population, and it will, we are going to have more concentrated density neighborhoods. I think the best we can ask for is to try to make them appealing, to try to make them attractive and well-maintained. I advocated very heavily for step backs or vertical setbacks on building Downtown and didn't get anywhere, so I am glad to see it on this project. I think this is something I can support. We have to have some confidence in the developer and understand they are going to be putting a lot of money and effort into this, and even with 86 bedrooms; it is better to have new housing than industrial along that area of town. It is something I am inclined to support also.

Commissioner Peterman: I would just like to reiterate that the idea of having the tot lot and the park and having yards that you can see into would certainly help with security. I think every time you have something like the tot lot or you have that kind of a small mini-pocket park, you will get people who are going to be out there, who are going to be involved, who are going to be talking to their neighbors, and that helps to increase security.

Commissioner McConnell: At this time I will move the adoption of the Tentative Map to develop 24 townhouse units on individual lots on the findings and facts as presented in the application.

Commissioner Harrington-Cole: I just wanted to point out that the resolution has three different items on it, and I didn't know if you wanted to take the other two later or you wanted to group them all because this resolution is for the approval of the Tentative Map also has a Minor Exception and a finding of Resolution for Approval for the Master Plan.

Commissioner McConnell: I will be happy to put them all in one motion. I was breaking them down which has been our practice in the past but we did elect . . . If everybody is prepared to vote on the entire thing, I will withdraw my motion and move that we adopt the Tentative Map to develop 24 townhouse units and also recommend the Minor Exception to allow an interior garage width of 19 feet and 4 inches.

Please vote.

AYES: Gourley, Manning, Legalos, McConnell, Peterman, Turley.

NOS: Harrington-Cole.

ABSENT: None.

Six yes. One no. Motion carries.

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Commissioner Harrington-Cole: I am sorry. I did not hear the last entitlement. I just want to be sure that everybody votes on it. That is the recommendation that City Council approve the Master Planning Unit Plan.

Commissioner McConnell: You are right. I didn't make that, so I will do so at this time.

Please vote.

AYES: Gourley, Manning, Legalos, McConnell, Peterman, Turley.
NOS: Harrington-Cole
ABSENT: None.

Six yes. One No. Motion carries.

Chairperson Legalos: We have a motion.

Commissioner Peterman: I move that we vote on the Mitigated Negative Declarations.

Please vote.

AYES: Gourley, Manning, Legalos, McConnell, Peterman, Turley.
NOS: Harrington-Cole.
ABSENT: None.

Six yes. One no. Motion carries.

L. OTHER ITEMS

None.

M. ADJOURNMENT

There being no further business to discuss, this session of the Vallejo Planning Commission is now adjourned at 8:00 p.m.

Respectfully submitted,



(for) DON HAZEN, Secretary

**STAFF REPORT – PLANNING
CITY OF VALLEJO
PLANNING COMMISSION**

DATE OF MEETING: December 3, 2007

PREPARED BY: Marcus Adams *M.A.*

PROJECT NUMBER: PM #04-0006

**PROJECT
DESCRIPTION:**

The applicant is requesting extension of a tentative parcel map approved on November 19, 2004, for a maximum time period of 12 months. The parcel map would subdivide a 6,516 square foot parcel containing two single family homes into two separate 3,258 square foot parcels.

Due to health issues, the property owner has been delayed in meeting the conditions of approval required to be completed prior to recording the parcel map.

RECOMMENDATION: Approve, subject to original conditions

CEQA: Categorically Exempt (Section 15315) (Class 15)

PROJECT DATA SUMMARY

Name of Applicant: Virgil Chavez Land Surveying

Date of Completion: November 14, 2007

General Plan Designation: Residential-High Density

Zoning Designation: Low Density Residential (LDR)

Site/Surrounding Land Use:

Site: 715/717 Ohio St./ APN 0056-106-030

North: Residential/Commercial

South: Residential

East: Residential

West: Residential/Commercial

Lot Area: 6,516 square feet

Total Floor Area/Ratio: 1,974 square feet/.30

Parking Required/Provided: 2 spaces per parcel required/ 4 provided

BACKGROUND SUMMARY

The applicant is requesting a one year extension of Parcel Map #04-0006 (approved November 19, 2004) to allow for time to complete the conditions of approval prior to recording the parcel map. The approved parcel map permits the subdivision of a 6,516 square-foot lot containing two single-family homes into two separate lots. Each lot would be comprised of 3,258 square feet and contain one single family home. Subdivisions into parcels less than 5,000 square-feet are allowed under the provision of Vallejo Municipal Code Section 15.20, Small Lot Subdivision. Such subdivision is allowed when two homes exist on one parcel, subject to several requirements. The proposed lot configuration is illustrated on the Tentative Parcel Map prepared by Virgil Chavez Land Surveying, dated July 2004.

ANALYSIS

Parcel Map #04-0006 will expire on November 19, 2007. The applicant has submitted an application requesting the extension of the parcel map. Typically, time extensions for non-Planning Commission approved permits are approved by staff. However, the process for parcel map extensions is the same as Tentative Maps which requires approval by the Planning Commission per Section 15.14.090 of the Vallejo Municipal Code. Major conditions still to be met include:

- Installation of exterior fire sprinklers over side property line windows
- Procurement of building permit for installed fire rated windows
- Building Division inspection of both homes for minimum energy conservation standards and substandard conditions
- Planning Division approval and planting of street trees
- Repair of broken sidewalk and gutter fronting the property

As noted in the November 19, 2004 staff report, the proposed subdivision would result in two parcels, each at 13.3 units per acre resulting in a density which would be conditionally consistent with the General Plan land use designation of Residential-High Density.

ENVIRONMENTAL DETERMINATION

The project meets the requirements for Class 15 Categorical Exemption, "Minor Land Divisions," Section 15315; Article 18 of the California Environmental Quality Act because the proposed subdivision of property is of four or fewer parcels and is in conformance with the General Plan and zoning district. No variances or exceptions are required.

CONCLUSION/RECOMMENDATION

The applicant filed for the time extension prior to the expiration of the parcel map and has demonstrated that they are working towards completing staff's conditions of approval. Staff therefore recommends approval of the time extension (12 months) to allow the applicant to complete the required conditions of approval prior to recording the parcel map.

FINDINGS

The Planning Commission finds, based on the facts contained in this staff report attached herein and incorporated herein by this reference, and given and the evidence presented at the public hearing, and subject to the conditions attached to this resolution that:

1. The tentative map extension is consistent with the goals and policies of the Vallejo General Plan as discussed in the "Analysis" section of this staff report.
2. The project is consistent as conditioned with the Subdivision Ordinance and Zoning Ordinance of the City of Vallejo, as discussed in the "Background Summary" section of this staff report.
3. As conditioned, the tentative map extension will not result in any significant environmental impacts which can not be mitigated, as per the "Environmental Determination" section of this report.

EXPIRATION

Approval of the tentative parcel map extension shall expire automatically twelve months after its approval unless prior to the twelve month expiration date, the map is extended by the planning commission for an additional twelve to twenty-four months. Prior to the expiration of an approved or conditionally approved tentative map, upon an application by the subdivider to extend that map, the map shall automatically be extended for sixty days or until the application for the extension is approved, conditionally approved, or denied, whichever occurs first. If the planning commission denies the application for extension, the subdivider may appeal to the city council within fifteen days after the commission has denied the extension, pursuant to the appeal procedures in Section 15.08.080.

APPEAL

The applicant or any party adversely affected by a decision of the Planning Commission may within ten days after the rendition of the decision of the Planning Commission appeal in writing to the City Council by filing a written appeal with the City Clerk and Planning Division. Such written appeal shall state the reason or reasons for the appeal and why the applicant believes he or she is adversely affected by the decision of the Planning Commission. Such appeal shall not be timely filed unless it is actually received by the City Clerk or designee

no later than the close of business on the tenth calendar day after the rendition of the decision of the Planning Commission. If such date falls on a weekend or city holiday, then the deadline shall be extended until the regular business day.

ATTACHMENTS

1. Resolution
2. MapQuest® directions
3. Supporting materials for extension
4. Tentative parcel map and garage plans
5. November 19, 2004 staff report
6. Pictures of site
7. Conflict of Interest Map

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC 07-

**A RESOLUTION OF THE PLANNING COMMISSION
APPROVING TENTATIVE PARCEL MAP EXTENSION
#04-0006**

Bowab Residences

The subject property is located at 715/717 Ohio Street between Sonoma Blvd. and Sutter Street.

APN# 0056-106-030

I. GENERAL FINDINGS

WHEREAS an application was filed by Virgil Chavez Land Surveying seeking approval to extend Tentative Parcel Map #04-0006 for an additional twelve months; and

WHEREAS the City of Vallejo Planning Commission conducted a duly noticed public hearing to consider the application for the Conditional Use Permit on December 3, 2007 at which testimony and evidence, both written and oral, was presented to and considered by the Planning Commission; and

WHEREAS based on evidence received at the public hearing, the Planning Commission makes the following factual findings:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS

Section 1. The Planning Commission finds that on the basis of the whole record before it there is no substantial evidence that the project will have a significant effect on the environment per Section 15315, Class 15 Categorical Exemption, "Minor Land Division" of the California Environmental Quality Act.

III. FINDINGS RELEVANT TO PARCEL MAP PROJECT APPROVAL AND FOR DETERMINATION OF PROJECT CONSISTENCY WITH APPLICABLE GENERAL PLAN

Section 2. The Planning Commission finds that applicant submitted Parcel Map extension application #04-0006 for a twelve month expiration extension pursuant to the City of Vallejo Municipal Code Chapters 15.08.110(E)(2).

Section 3. Planning Commission finds, based on the facts contained in the staff report attached herein and incorporated herein by this reference, and given and the evidence presented at the public hearing that:

1. The tentative map extension is consistent with the goals and policies of the Vallejo General Plan as discussed in the "Analysis" section of the staff report.
2. The project is consistent as conditioned with the Subdivision Ordinance and Zoning Ordinance of the City of Vallejo, as discussed in the "Background Summary" section of the staff report.
3. As conditioned, the tentative map extension will not result in any significant environmental impacts which can not be mitigated, as per the "Environmental Determination" section of the report.

IV. RESOLUTION APPROVING THE TENTATIVE PARCEL MAP APPLICATION FOR A TWELVE MONTH EXTENSION

NOW, THEREFORE, LET IT BE RESOLVED that the Planning Commission hereby APPROVES the Tentative Parcel Map extension application (PM# 04-0006) for a twelve month extension, based on the findings contained in the staff report attached hereto and incorporated herein.

V. VOTE

PASSED AND ADOPTED at a regular meeting of the Planning Commission of the City of Vallejo, State of California, on the 3rd day of December, 2007, by the following vote to-wit:


AYES:
NOES:
ABSENT:


CHARLES LEGALOS, CHAIRPERSON
City of Vallejo PLANNING COMMISSION
Attest:


Don Hazen
Planning Commission Secretary

Maneuvers

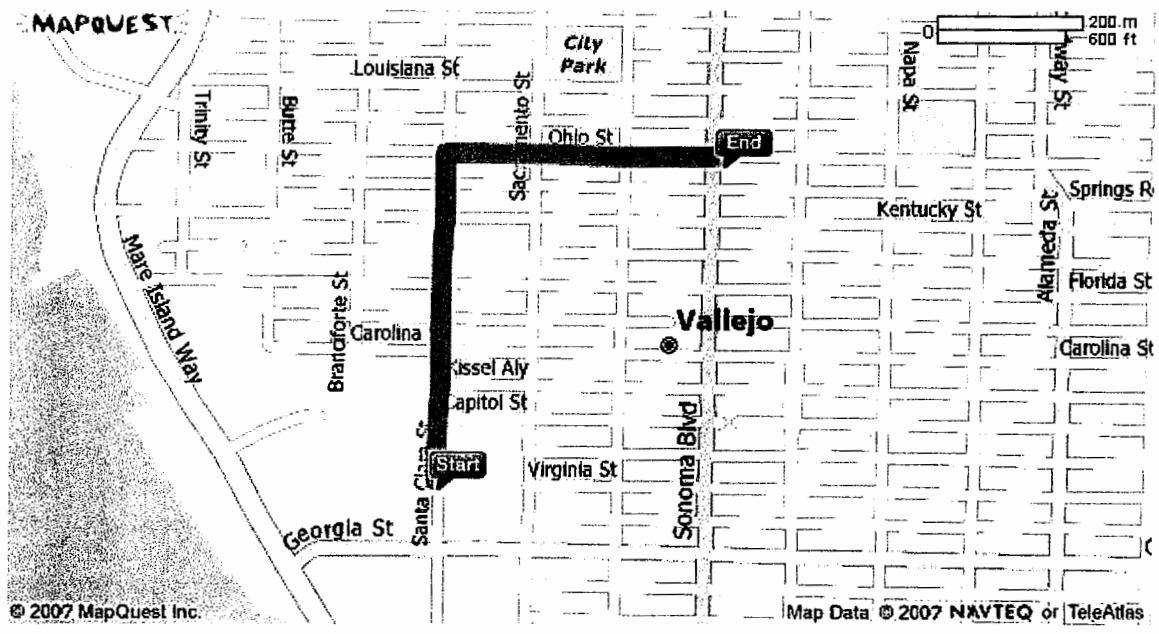
Distance Maps

-  **1:** Start out going NORTH on SANTA CLARA ST toward CAPITOL ST. 0.3 miles [Map](#)

-  **2:** Turn RIGHT onto OHIO ST. 0.2 miles [Map](#)

-  **3:** End at **717 Ohio St**
Vallejo, CA 94590-5122, US [Map](#)

Total Est. Time: 3 minutes **Total Est. Distance:** 0.62 miles



Virgil Chavez Land Surveying

721 Tuolumne Street
 Vallejo, California 94590
 (707) 553-2476 • Fax (707) 553-8698

October 24, 2007
 Project No.: 2468-00

Judith Cassmore
 717 Ohio Street
 Vallejo, Ca. 94590

Subject: 715/717 Ohio Street
 MS 04-0006
 Vallejo, CA

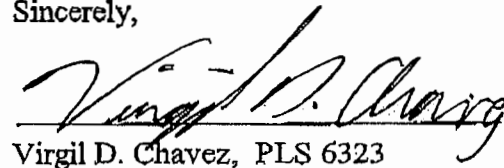
Dear Judith:

This is to let you know that I think we have run out of time for the above referenced project. It is my opinion that even if all conditions have been completed, there is not enough time to have this map recorded prior to November 19, 2007. I know we have been working earnestly to complete the requirements that pertain to this project. I have been in contact with your Title Company going back to July. I have called them again recently and have been informed that the lender has not responded to repeated requests. If we cannot get their signature & notary, we simply cannot record. I am enclosing an application which you can use for an extension of the Conditions of Approval for the Parcel Map. Please take it or mail it to the City of Vallejo Planning as soon as possible. There is a fee for the extension. The fees were changed recently. It is my understanding that the current fees are \$ 860 for the Parcel Map extension and \$ 209 for noticing fee. Please call me if you have any questions. My contact at the Title Company is Laura Wilder. Her number is 916-630-8545. She can provide more detail about the lender.

860
 209

 \$ 1069

Sincerely,


 Virgil D. Chavez, PLS 6323

Cc: Laura Wilder
 NATCo
 3805 Atherton Road #101
 Rocklin, Ca. 95765

John Bond
 310-659-7743 Fax
 212-764-5165 Fax

April 12, 2006

City of Vallejo
Planning Division
555 Santa Clara Street
Vallejo, CA 94590

RE: Time Line Extension on Approval of Subdivision of Parcel Map #04-0006
715 & 717 Ohio Street
Vallejo, CA 94590
Assessor's Parcel No. 0056-106-030

To Whom It May Concern:

We would like to request a ~~6 month~~^{9c} 1 year extension of the time line on approval of Parcel Map #04-0006 in order to meet the requirements of the Planning Department of the City of Vallejo, and to meet the concerns expressed by the Building Department regarding this subdivision.

The extension is requested partly because we were embroiled in a conflict with the tenants who were living at 715 Ohio when we bought the property. This conflict took over six months to resolve, and we could not move forward in any meaningful way until the issue was settled. Our progress on the project was also hindered by my ill health last fall.

We have obtained a Building Permit, No. 06-0005 to satisfy the requirement by the Planning Department to build a carport or garage located in the rear of Parcel 2 (717 Ohio). We have opted to build a one car garage and should be in the approval phase for the footings and perhaps the concrete slab for this garage by the original expiration date of May 19, 2006.

In order to satisfy the requirement of the Building Department for one hour fire rated windows. Please see attached letter from Leon McNeil, Chief Building Official of the City of Vallejo. We are ordering said windows from Fyre-Tec™, a company that sells fire rated windows directly to consumers. The lead time for delivery of these windows is at least 8 weeks. We are also waiting for a bid to install exterior sprinklers above each window from Cosco Fire Protection, which is also a requirement stipulated by the Building Department.

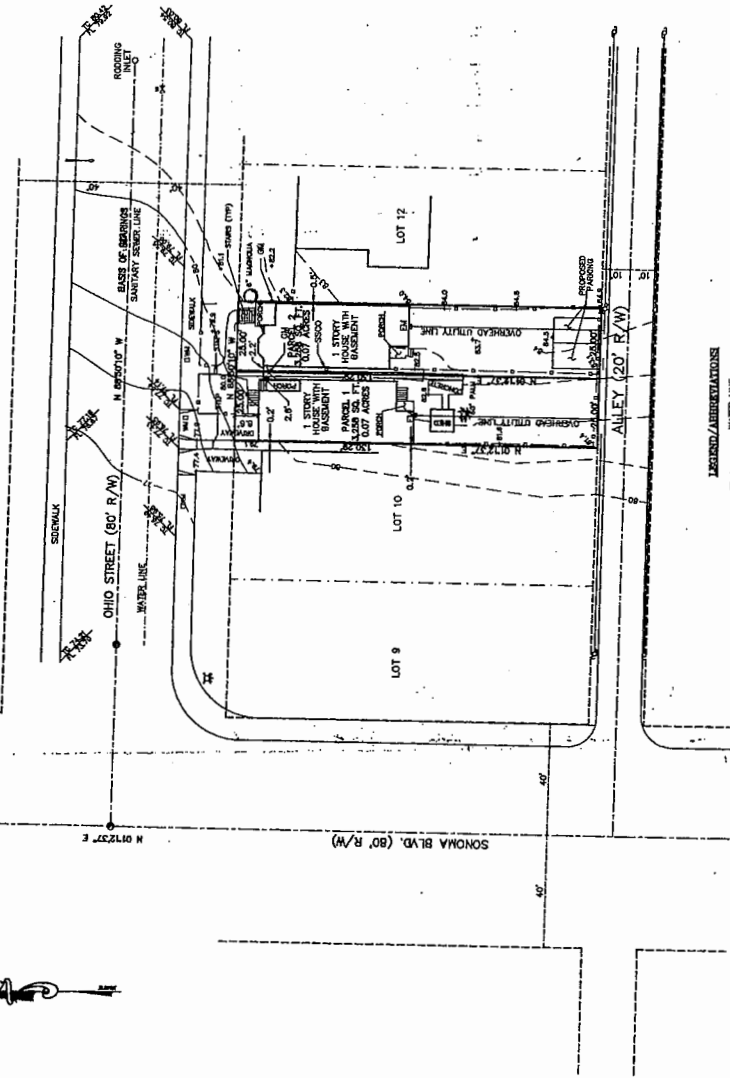
Thank you for your consideration of this request,



Judith Cassmore for
John Bowab

Cc: John Bowab

I MAPS 123



- LEGEND/ABBREVIATIONS**
- WATER LINE
 - LOT LINE
 - RIGHT OF WAY LINE
 - WATERSHED
 - SPOT ELEVATIONS
 - EXISTING FIRE HYDRANT
 - SANITARY SEWER MANHOLE
 - STREET LIGHT POLE
 - TOP OF CURB
 - SANITARY SEWER CLEANOUT

UNDERGROUND UTILITIES.
 ALL UNDERGROUND UTILITIES SHOWN HEREON ARE FROM EXISTING FIELD SURVEY. NO WARRANTY, EXPRESSED OR IMPLIED, IS MADE AS TO THE COMPLETENESS OR CORRECTNESS OF THEIR LOCATION.

ZONING.
 THIS PROPERTY IS ZONED LDR (LOW DENSITY RESIDENCE).

NOTES
 OWNER/SUBSCRIBERS: LINDY OWEN
 ADDRESS: 8390 PALO VERDE ROAD
 CITY, CA. 94532
 SITE ADDRESS: 715 AND 717 OHIO STREET
 VALLEJO, CA. 94590

BASIS OF BEARINGS.
 THE BASIS OF BEARINGS FOR THIS SURVEY TAKEN AS N. 85° 07' 0" W. AS SHOWN ON BOOK 28 SURVEYS, PAGE 45, SOLANO COUNTY RECORDS.

BENCHMARK.
 FROM THE INTERSECTION OF 5TH AND 11TH STREET, 200' +/- -
 TO THE INTERSECTION OF 5TH AND 11TH STREET, 200' +/- -
 TO THE INTERSECTION OF 5TH AND 11TH STREET, 200' +/- -
 TO THE INTERSECTION OF 5TH AND 11TH STREET, 200' +/- -

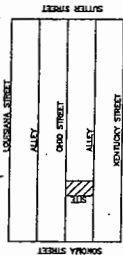
REFERENCES
 (1) - BOOK 1 OF MAPS, PAGE 123
 (2) - BOOK 28 OF SURVEYS, PAGE 45

FLOOD ZONE DATA.
 THIS SITE IS ZONED LDR AN AREA OF UNIFORM FLOODING
 PANEL NO. 080524, 0058 C, MAP REVISION DATE: FEBRUARY 22, 1983.

NOTES.
 EXISTING RESIDENCE ON PARCEL 1 HAS A SINGLE CAR GARAGE AND PARTIAL BUDGET.

SURVEYED BY:

WIRIL D. CHAVEZ, PLS 0323



VERTICAL MAP NOT TO SCALE

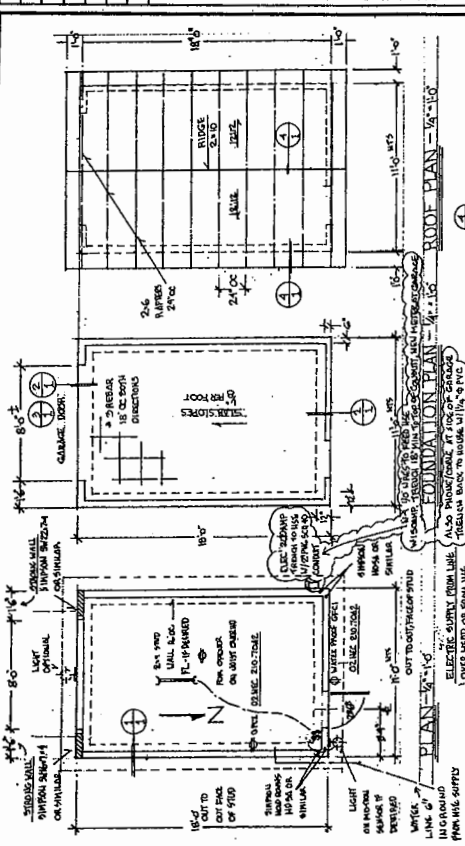
TENTATIVE PARCEL MAP
 A RESUBDIVISION OF LOT 11 IN BLOCK 397 OF THE
 'MAP OF CITY OF VALLEJO' (BOOK 1 OF MAPS, PAGE 123)
 VALLEJO, SOLANO COUNTY, CALIFORNIA

DATE: JULY 2004
 DRAWN BY: VIRGIL CHAVEZ LAND SURVEYING
 SCALE: 1"=20'
 CHECKED BY: SHE OBERG
 PROJ. MGR: WILDC FILE# 2448.CDS
 PROJ. NO. 2448-001/CAD FILE: 2448PTM

SHEET NO. 1
 OF 1

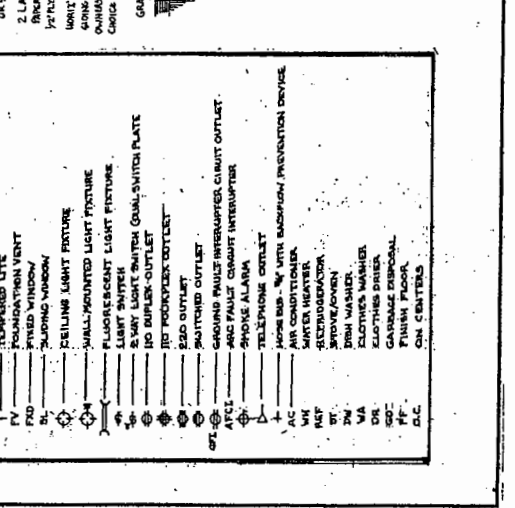
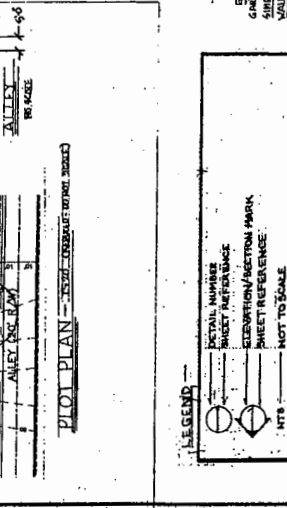
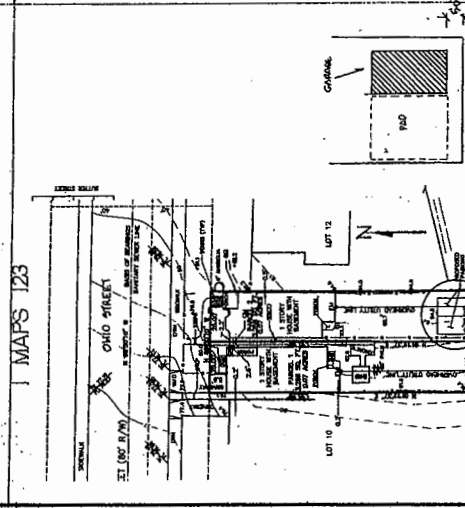
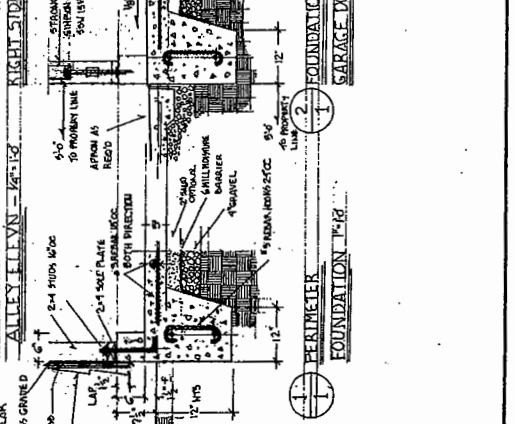
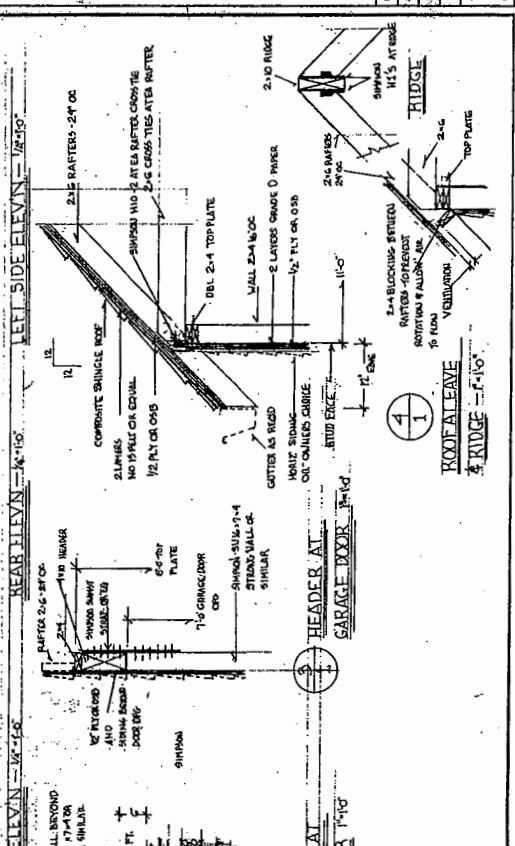
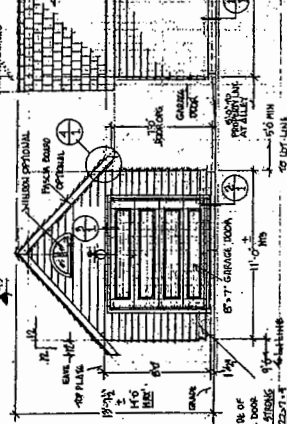
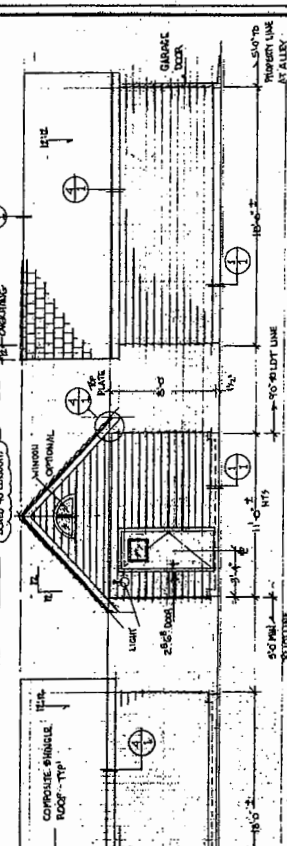
DATE: JULY 2004
 DRAWN BY: VIRGIL CHAVEZ LAND SURVEYING
 SCALE: 1"=20'
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 PROJ. MGR: WILDC FILE# 2448.CDS
 PROJ. NO. 2448-001/CAD FILE: 2448PTM

DATE: JULY 2004
 DRAWN BY: VIRGIL CHAVEZ LAND SURVEYING
 SCALE: 1"=20'
 CHECKED BY: SHE OBERG
 PROJ. MGR: WILDC FILE# 2448.CDS
 PROJ. NO. 2448-001/CAD FILE: 2448PTM



GENERAL NOTES:

- 1) ALL WORK SHALL CONFORM TO THE MINIMUM STANDARDS OF THE CITY OF VALLEJO AND THE CITY OF VALLEJO BUILDING CODE AND THE CITY OF VALLEJO PLUMBING CODE AND THE CITY OF VALLEJO ELECTRICAL CODE AND THE CITY OF VALLEJO MECHANICAL CODE. ANY VARIANCE IN THESE DOCUMENTS WITH THESE CODES SHALL BE CALLED TO THE ATTENTION OF THE DRAFTSMAN/DESIGNER BEFORE CONSTRUCTION BEGINS. ONCE CONSTRUCTION BEGINS IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ALL LABOR, MATERIALS AND EQUIPMENT TO COMPLETE THE JOB TO MEET ALL APPLICABLE CODE REQUIREMENTS.
- 2) ANY CONFLICTS DISCOVERED IN THE DRAWINGS OR BETWEEN THE DRAWINGS AND WRITTEN MATERIALS AND/OR ANY SUBSTITUTIONS MUST BE BROUGHT TO THE ATTENTION OF THE ARCHITECT IMMEDIATELY FOR CORRECTION BEFORE CONSTRUCTION BEGINS. WRITTEN CORRECTIONS SHALL BE OBTAINED FROM THE ARCHITECT IMMEDIATELY.
- 3) FOR REVISIONS AND ADDITIONS ONLY: IT IS THE PURPOSE OF THESE DRAWINGS TO SHOW THE GENERAL ARRANGEMENT OF THE WORK TO BE DONE TO THE EXISTING STRUCTURE. ARCHITECTURAL CONDITIONS OR EXISTING WALLS, ROOFS AND CEILINGS WHICH ARE TO BE REVEALED AND PERMANENTLY MARKED AS TO BE REVEALED DURING CONSTRUCTION. THE DRAFTSMAN/DESIGNER DOES NOT ASSUME ANY RESPONSIBILITY FOR THE EXISTING STRUCTURE.



LEGEND

1	SECTION NUMBER
2	PROJECT REFERENCE
3	SECTION REFERENCE MARK
4	SHEET REFERENCE
5	HOT TO SCALE
6	TEMPERED LITE
7	FINISH WINDOW
8	SLIDING WINDOW
9	CEILING LIGHT FIXTURE
10	WALL MOUNTED LIGHT FIXTURE
11	FLUORESCENT LIGHT FIXTURE
12	LIGHT SWITCH
13	2 WAY LIGHT SWITCH
14	DOOR/OUTLET
15	DOOR/OUTLET
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18	DOOR/OUTLET
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100	DOOR/OUTLET



STAFF REPORT AND DETERMINATION
CITY OF VALLEJO PLANNING DIVISION

Application Number: Parcel Map #04-0006

Action: **APPROVE** Parcel Map #04-0006 subject to the findings and conditions contained in the staff report.

Date of Action: **November 19, 2004**

1. **PROJECT DESCRIPTION:** The applicant is requesting approval of a Parcel Map to allow the subdivision of a 6,516 square-foot lot containing two single-family homes into two separate lots. Each lot would comprise of 3,258 square feet and contain one single family home. Subdivision into parcels under 5,000 square-feet is allowed under the provision of Municipal Code Section 15.20, Small Lot Subdivision. Such a subdivision into lots of less than 5,000 square feet is allowed when two homes exist on one parcel, subject to several requirements. The proposed lot configuration is illustrated on the Tentative Parcel Map prepared by Virgil Chavez Land Surveying, dated July 2004.
2. **LOCATION:** 715 and 717 Ohio Street/APN: 0056-106-030
3. **APPLICANT:** Virgil Chavez
312 Georgia Street #225
Vallejo, CA 94590
4. **PROPERTY OWNER:** Leroy and Vera Ginn
6390 Palo Verde Road
Castro Valley, CA 94552
5. **EXISTING LAND USE:** The property contains two existing single family homes.
6. **SURROUNDING LAND USE:** The property is surrounded by one-and-two story single-family and multiple family homes to the south and east; single-family and multiple family homes and commercial uses beyond to the north and west.
7. **CONSISTENCY WITH THE GENERAL PLAN:** The General Plan Land Use designation for this site is Residential-High Density. The proposal to subdivide the parcel into two parcels is consistent with this land use designation.

8. **ENVIRONMENTAL REVIEW:** The proposed development was determined to be exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315, Class 15. (division of land in an urbanized area zoned for commercial use into four or fewer parcels where the division is in conformance with the General Plan and zoning). No special circumstances, such as a sensitive environmental location, hazardous materials or historic resources have been identified during review of the project, which would require environmental review.
9. **CONFORMANCE WITH ZONING REGULATIONS:** The property is zoned LDR – Low Density Residential. The parcel is 6,516 square feet and is propose to be divided into two 3,258 square foot parcels. The LDR – Low Density Residential zoning district requires that each lot be a minimum of 5,000-square feet and in this particular case, each lot is proposed to be 3,258-square feet. The applicant is applying for a small lot subdivision under Section 15.20 of the Zoning Ordinance and is not subject to the requirements of the LDR – Low Density Residential district.

The following outlines the requirements for a Small Lot Subdivision as outlined in Section 15.20 of the Vallejo Municipal Code:

- A. *The lot to be subdivided has no more than one dwelling unit for every two thousand five hundred square feet of area in such lot;*

The site contains two single-family homes and both are facing Ohio Street. Upon approval of the subdivision, the lots proposed lots would be 3,258 square feet.

- B. *All structures on the lot are existing structures as defined in Section 16.040.550 of the zoning ordinance and each existing structure has no more than two dwelling units;*

The site has existing two single-family homes, each containing one living unit.

- C. *The building official, or his or her designee, has inspected (which may include, in the discretion of the building official, a pest control inspection by a state licensed contractor) the structures on the lot to be subdivided and all substandard conditions found therein (including pest damage if found) have been corrected to the satisfaction of the building official. "Substandard condition" means any or all conditions enumerated in the Uniform Housing Code under Substandard Buildings;*

As a condition of approval, the applicant will be required to meet this requirement.

- D. *Each structure in the lot to be subdivided has the following minimum energy conservation features: R-30 ceiling insulation, hot water heater blanket,*

caulking throughout, gasketing of windows and exterior doors, water conservation devices;

As a condition of approval, the applicant will be required to meet these requirements.

- E. The location of all structures, easements, set backs and property lines, as shown on a survey, prepared by a state licensed civil engineer, meets the requirements of the zoning ordinance, the Uniform Building Code and Uniform Fire Code, then in effect;*

The existing structures would be required to meet the requirements of the Uniform Building Code and Uniform Fire Code requirements. The houses were built circa 1910, and therefore do not meet the current setback standards. The existing houses at 715 and 717 Ohio Street do not meet the current setback standards for the front and side yards. None of the building setbacks are made more non-confirming with the approval of this subdivision and therefore will not intensify the non-conformity of both proposed parcels.

- F. The subdivision shall not create any safety hazards;*

Staff is unaware of any safety hazards at the site.

- G. Each subdivided lot has frontage or access to an existing public right of way;*

Both Parcel 1 (715 Ohio Street) and Parcel 2 (717 Ohio Street) have frontage on Ohio Street.

- H. Each subdivided lot has sufficient parking as required by the zoning ordinance or, in the determination of the development services director;*

- I. There is sufficient parking in the immediate neighborhood; and*

Parcel 1 (715 Ohio Street) has an attached one car garage and one uncovered parking space available in the existing driveway. For Parcel 2 (717 Ohio Street) the applicant is proposing a detached carport located in the rear of the proposed parcel. According to the submitted tentative map, the carport is located on the rear property line. The required setback for carports that front on alley ways is five feet per Section 16.62.150 B. As a condition of approval, the applicant will be required to submit elevations and a revised site plan showing that the proposed carport meets the setback requirement of five feet and maximum height requirements for detached structures in the Low Density Residential zone.

2. *The subdivision itself will not cause a parking problem;*

Each home has existed on the site for many years, and the subdivision would not increase in non-conformity as the project does not propose any additional dwellings or square footage for the existing dwellings.

- I. *Each subdivided lot will meet the open space requirements of the medium density residential district of the zoning ordinance;*

The open space requirement is 300 square feet of usable open space per unit with a minimum dimension of 15 feet. Both lots have adequate area and will meet the 15-foot dimension. Both houses have approximately 2,000 square feet of outdoor area.

- J. *The requirements of Section 16.70.070 of the zoning ordinance (street trees) have been met;*

The applicant will be required to provide one street tree on Ohio Street.

- K. *All drainage and sanitary sewer problems on the lot, as individuated on the drainage plan, which shall be submitted as part of the application, have been reviewed by the Vallejo Sanitation and Flood Control District and have been corrected;*

The Vallejo Sanitation and Flood Control District has reviewed this project and has determined that the proposed project meets this requirement.

- L. *Each subdivided lot has its own water and sewer service; and*

Each home has individual water and sewer connections.

- M. *All improvements and repairs with respect to streets, curbs, gutters, and sidewalks, as required by the public works department, have been completed.*

The Public Works Department has made this requirement a condition of approval.

10. CONFORMANCE WITH SUBDIVISION REGULATIONS: The project meets the requirements and is in conformance with Section 15.14 of the Vallejo Zoning Ordinance (Subdivisions/Parcel maps) and Section 66463, Article 5 of the State of California Subdivision Map Act.

11. NOTICING AND PUBLIC COMMENTS: Notice of the proposed parcel map and minor exception applications were sent to: property owners within 200 feet of the subject property, and the Sub-divider on November 1, 2004. No comments were received from the public regarding this proposal.

12. DEPARTMENT AND AGENCY REVIEW: Notice of the application was sent to the Building Division, the Traffic Engineer, the City Engineer, Vallejo Sanitation and Flood Control District, Fire Prevention, and the Water Superintendent. Comments from these departments are incorporated into the staff analysis and conditions of approval sections of this staff report.

13. STAFF ANALYSIS AND DETERMINATION: The site has two small single-family residences facing Ohio Street. The existing parcel is 6,516 square feet and proposed to be divided into two parcels. Both Parcel 1 (715 Ohio Street) and Parcel 2 (717 Ohio Street) would be 3,258 square feet, measuring 130.29 feet deep by 25.0 feet wide. The LDR – Low Density Residential zoning district requires a minimum lot size of 5,000 square feet, and a minimum of 2,500 square feet per unit. However, Section 15.20 of the subdivision ordinance allows the creation of smaller lots with a minimum lot size of 2,500 square feet subject to several criteria. As described above, the proposed subdivision as conditioned meets the criteria required for a Small Lot Subdivision. Section 15.20 of the subdivision ordinance also requires that, in the determination of the Development Services Director, adequate parking is available in the immediate area and that the subdivision would not cause a parking problem. Parcel 1 has one covered parking space and one uncovered parking space located in the existing driveway. At the time this staff report was prepared, no off-street parking existed for Parcel 2. The applicant is therefore proposing to construct a two-car carport for Parcel 2 to meet the City's requirement of one covered parking and one uncovered parking space. Staff has determined that since the houses already exist and the applicant is providing covered parking for both units, the parking demand would be no greater than it is currently. In addition, surrounding streets have adequate street parking. Staff believes that the configuration of these houses warrants approval of this small lot subdivision, which would tend to stabilize this neighborhood by encouraging owner-occupancy of existing multi-unit lots.

Findings:

These findings are based upon all evidence in the record including the staff report, testimony, and written correspondence, all of which is incorporated by reference:

These findings are made based upon all the evidence in the record including the staff report, testimony and written correspondence, all of which is incorporated by reference:

1. The lot to be subdivided has no more than one dwelling unit for every two thousand five hundred square feet of area in such lot;
2. All structures on the lot are existing structures as defined in Section 16.040.550 of the zoning ordinance and each existing structure has no more than one dwelling units;

3. The building official, or his or her designee, will inspect (which may include, in the discretion of the building official, a pest control inspection by a state licensed contractor) the structures on the lot to be subdivided and all substandard conditions found therein (including pest damage if found) will be corrected to the satisfaction of the building official, prior to the recordation of the parcel map. "Substandard condition" means any or all conditions enumerated in the Uniform Housing Code under Substandard Buildings;
4. Each structure in the lot to be subdivided will be required to have the following minimum energy conservation features: R-30 ceiling insulation, hot water heater blanket or energy efficient hot water heater, caulking throughout, gasketing of windows and exterior doors, water conservation devices;
5. The location of all structures, easements, set backs and property lines, as shown on a survey, prepared by a state licensed civil engineer, meets the requirements of the zoning ordinance, the Uniform Building Code and Uniform Fire Code, then in effect;
6. The subdivision shall not create any safety hazards;
7. Each subdivided lot has frontage or access to an existing public right of way;
8. Each subdivided lot, as conditioned, will have sufficient parking as required by the zoning ordinance or by the determination of the Community Development Director. The project will not result in a hazard to pedestrian or vehicular traffic;
9. Each subdivided lot will meet the open space requirements of the medium density residential district of the zoning ordinance;
10. The requirements of Section 16.70.070 of the zoning ordinance (street trees) will be met through project conditions;
11. All drainage and sanitary sewer problems on the lot, as individuated on the drainage plan, which shall be submitted as part of the application, have been reviewed by the Vallejo Sanitation and Flood Control District and will be corrected through project conditions of approval;
12. Each subdivided lot as conditioned, will have its own water and sewer service; and
13. All improvements and repairs with respect to streets, curbs, gutters, and sidewalks, as required by the public works department, will be completed as a condition of project approval.
14. The proposed project meets the intent and specific standards prescribed in the Zoning Ordinance, per Section 9 of this report.

15. The parcel map is consistent with the goals and policies of the Vallejo General Plan, per Section 7 of this report.
16. As conditioned, the parcel map will not result in any significant environmental impacts that cannot be mitigated, per Section 8 of this report.
17. The parcel map will not result in any significant environmental impacts and is determined to be exempt from CEQA as described in Section 8 of this report.
18. The small lot subdivision will result in development characteristic of the neighborhood and will not adversely affect any development or persons upon abutting properties.

14. CONDITIONS REQUIRED, PRIOR TO RECORDING OF THE PARCEL MAP.

Planning Division:

1. Prior to parcel map approval, submit a numbered list to the Planning Division stating how each condition of approval will be met.
2. Prior to parcel map approval, submit elevations and a revised site plan showing the location of the carport for Parcel 2 meets the setback and maximum height requirements for the Low Density Residential zone to the Planning Division for review and approval.
3. Prior to parcel map approval, install two-car carport on Parcel 2.
4. Prior to parcel map approval, install two City approved street trees (one per lot) fronting Ohio Street tree shall be selected from City's approved street tree list or as otherwise determined by the Planning Division.

Building Division:

1. Prior to parcel map recording, the building official shall inspect the property to insure the homes are not considered in "Substandard Condition" according to any conditions enumerated in the Uniform Housing Code under Substandard Buildings.
2. The applicant shall provide information that the homes have R-30 ceiling insulation, hot water heater blankets or energy efficient water heaters, caulking throughout, gasketing of windows, exterior doors, and water conservation devices.

3. All windows on 0 foot lot line side must be closed and provide 1-hour fire rated wall construction or as otherwise determined by the Building Division.
4. If condition number 3 can not be met, then the subdivision can not be approved.
5. Prior to approval of condition number 3, submit a floor plan showing the sizes of all windows for review and approval.

Public Works Department:

1. Submit a parcel map prepared by a qualified registered civil engineer or land surveyor for review and approval. The submitted map shall include all documents necessary for map review (title report, closure calculations, monument security, fees, etc.).
2. Pay the City fees required by Solano County for providing copies of the recorded map to the City (\$15.00/sheet). Plus \$1,029.00 + \$102.00/lot for cost of map checking.
3. Prior to approval of parcel map, applicant shall provide a letter from all utility companies that each parcel has its own connection to the utility services.
4. Prior to approval of the parcel map proof or improve that surface drainage of one lot does not affect the other lot. Each lot shall handle its own drainage.
5. Prior to approval of the parcel map, remove and replace broken sidewalk and gutter fronting the property as determined in the field by the City Engineer.

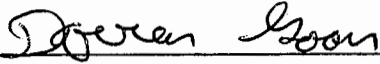
GENERAL CONDITIONS

1. The conditions herein contained shall run with the property and shall be binding on the applicant and all heirs, executors, administrators, and successors-in-interest to the real property that is the subject of this approval.
2. The applicant shall defend, indemnify, and hold harmless the City of Vallejo and its agents, officers, and employees from any claim, action, or proceeding against the City and its agents, officers, and employees to attack, set aside, void, or annul this approval by the City. The City may elect, at its discretion, to participate in the defense of any action.

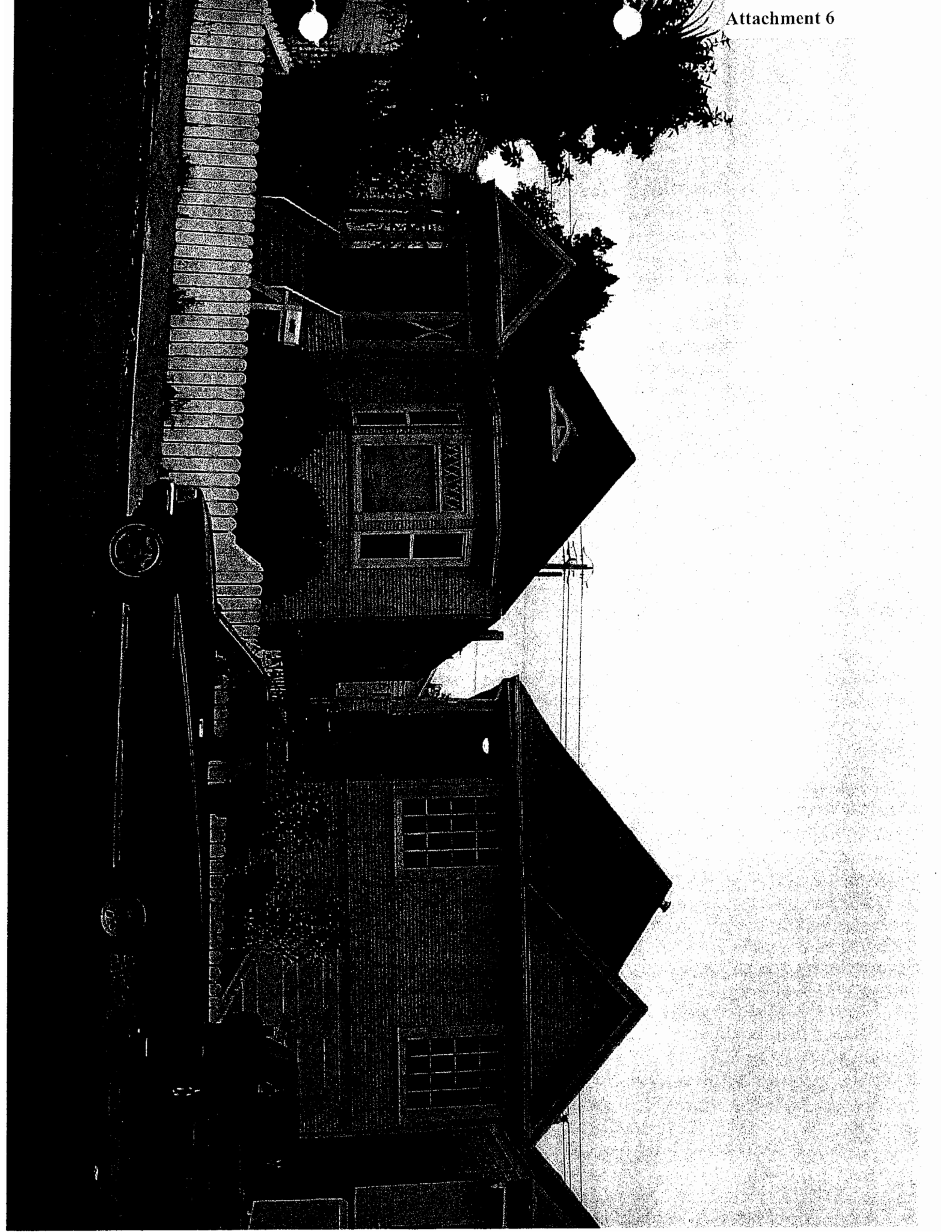
15. EXPIRATION

This parcel map shall expire in 18 months unless the map has been recorded or is extended by the City prior to the expiration date.

The applicant or any party adversely affected by a decision of the Planning Division may appeal the decision by filing an appeal to the Planning Commission. Such appeal must be filed in writing with the Secretary of the Planning Division within ten calendar days after the Planning Division's action. The Commission may affirm, reverse, or modify any decision of the Planning Division that is appealed.

Prepared by: 
Darren Goon, Assistant Planner

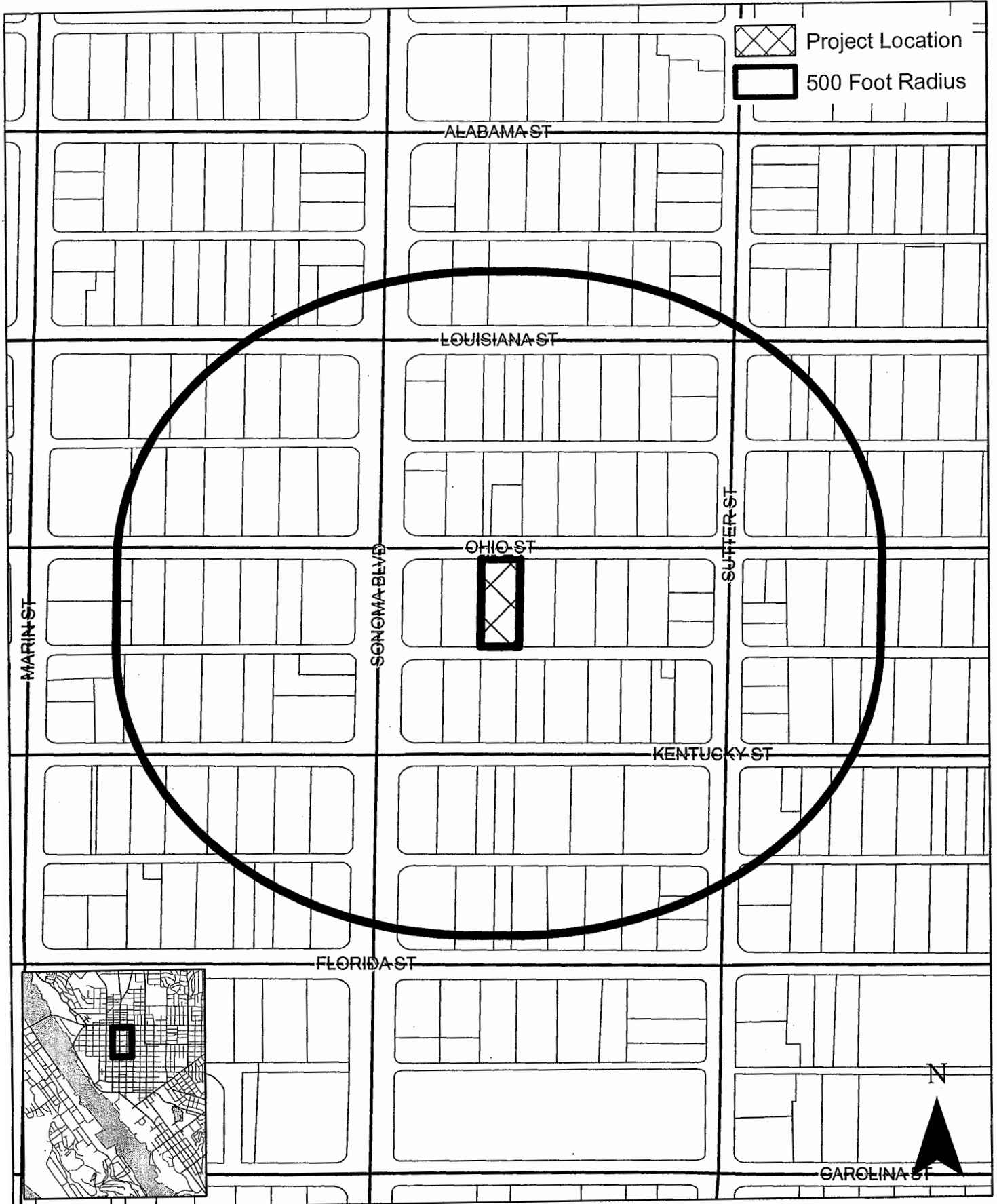
Reviewed by: 
Brian Dolan, Planning Manager



Conflict of Interest Map

Parcel Map - Extension #04-0006

715 & 717 Ohio Street - (APN# 0056-106-030)



**STAFF REPORT – PLANNING
CITY OF VALLEJO
PLANNING COMMISSION**

DATE OF MEETING: December 3, 2007
PREPARED BY: Katherine Donovan *KD*
PROJECT NUMBER: SD 05-0005
PROJECT LOCATION: 145 B Street; APN 0051-132-090

1. PROJECT DESCRIPTION:

The project consists of a two-story addition to the rear of an existing house in the Residential View District, as well as a deck extending along the north side. The addition would add 467 square feet of conditioned space and 152 square feet of unconditioned space to the existing 1,060-square-foot residence. An approximately fifteen by thirty-two foot deck would be constructed on the north side of the first floor.

The application was originally submitted in January 2005 as a much different project including a two-story addition on the north side, where the deck is currently proposed. Staff recommended modifications to the project, which was revised several times. The plans currently under review are the final revisions submitted by the project applicant.

2. RECOMMENDATION: Recommend Approval with Conditions

3. CEQA: Categorically Exempt 15331, Historical Resource Restoration/Rehabilitation

4. PROJECT DATA SUMMARY

Name of Applicant: Charlie Aquilina and Barbara Luck
General Plan Designation: Low Density Residential
Zoning Designation: Low Density Residential; Residential View District
Site/Surrounding Land Use:
Site: Single-family residential

North: Single- and two-family residential
South: Single- and two-family residential
East: Single- and two-family residential
West: Multi-family residential

Lot Area: 4,815 square feet

Total Floor Area: Existing – 1,060 square feet; Proposed – 1,527 square feet

Lot Coverage: 26%

Parking Required/Provided: 2 spaces are required; no spaces are provided

5. BACKGROUND SUMMARY

Project History. Staff met with the applicant and his architect to review potential plans for an addition to the property in the fall of 2005. At that preliminary meeting, staff had not had the opportunity to view the site and explained to the architect and property owners that comments would relate only to the architecture and not to the view issues. The architect for the project submitted a site development permit application on January 13, 2005. These plans included a two-story addition to the north side of the building.

The project was originally scheduled for public hearing on April 4, 2005. After meeting with the Vallejo Heights Neighborhood Association, the property owners' architect requested a delay to revise the plans. Staff continued to work with the original project architect through the spring and summer of 2005. On April 4, 2006, staff met with the property owners and their new architect, Patricia Pollock, to review the new proposal and on March 5, 2006, the new plans were submitted. The new plans provided a two-story addition to the rear of the building, with a substantial deck along the north side.

The project was scheduled for public hearing before the Planning Commission on May 21, 2007. This hearing date was postponed to address staff concerns regarding the project design and to allow the City to hold a Neighborhood Meeting to get input from the neighborhood on potential impacts of the proposal. The Neighborhood Meeting was held on June 14, 2007 and the project was tentatively scheduled to be heard by the Planning Commission on September 17, 2007. This hearing date was delayed several times to allow the architect sufficient time to submit the revised plans for the project. The plan revisions addressed concerns raised by staff and by the neighbors at the Neighborhood meeting.

Site Development Standards. The project has been designed to conform to the development standards for Low Density Residential Districts. Although the

existing building does not meet the front setback requirement, the only new construction within the front setback would be the deck, which, under the exceptions allowed by the Zoning Ordinance (16.80.070(B)), can extend into the front setback up to six feet.

The height limitation in LDR districts is two stories or 35 feet, whichever is the lesser. The existing residence is two-story with a basement and approximately 33 feet at the highest point above natural grade. The proposed addition would be approximately 28 feet at the highest point above natural grade, which is consistent with the height limitations in the zoning ordinance. The addition would add approximately 150 square feet of space to the basement. As the height at the perimeter of the basement, as measured from the finished floor of the first floor of the house to the natural grade of the property, is less than six feet for more than fifty percent of the perimeter, the basement level is not considered a story for purposes of height.

Historic Review. The project is located in the Bay Terrace subdivision, which is a State Historic District. To qualify for a categorical exemption from CEQA, new construction in historic districts must comply with the Secretary of the Interior's *Standards for the Treatment of Historic Properties (Standards)*. The *Standards* also recommend that new additions be designed "in a manner that makes it clear what is historic and what is new" and list as Not Recommended "duplicating the exact form, material style, and detailing of the historic building in a new addition so that the new work appears to be part of the historic building." The *Standards* also recommend that the "design for the new work may be contemporary or may reference design motifs from the historic building. In either case, it should always be clearly differentiated from the historic building and be compatible in terms of mass, materials, relationship of solids to voids, and color." The project design is contemporary and clearly differentiated from the historic building while being compatible with the existing residence in terms of mass, materials, scale and proportion, and color.

Public Comments. The property owners and their first architect met with the Vallejo Heights Neighborhood Association on March 29, 2005 and held another neighborhood meeting on July 15, 2005, as well as meeting privately with one neighbor to review and discuss the proposed plans. After these meetings, the applicants indicated that they wanted to revise their plans before taking the project to the Planning Commission. New plans were submitted to the Planning Division in March 2007. A public hearing was scheduled for May 21, 2007 but was delayed to allow the Planning Division to hold a neighborhood meeting on the new plans. The applicants determined after this meeting to submit revised plans. The plans were received November 9, 2007 and notice of the public hearing was sent out November 8, 2007 in anticipation of receiving the revised plans.

After each meeting and throughout the project development period, staff received numerous telephone calls, emails, and letters regarding the project. All the letters have been included as attachments to this staff report; however, please keep in mind that some of the comments refer to previous plans or unrevised versions of the current plans. Also included as an attachment is a statement by the property owners' presenting their account of the project history.

6. ANALYSIS

View District Findings. In order to approve the application, the project must meet the following findings:

1. *Will maximize open space preservation.*

The proposed project, including the decks, would cover approximately 26 percent of the lot. The impact on open space would be minimal.

2. *Protects view corridors, natural vegetation, land forms and other features.*

The project would not affect natural vegetation, land forms, or other features such as the mature Deodar cedar trees. As proposed, the project would minimize impacts to view corridors by placing the addition to the rear. The project would block a portion of the view to pedestrians walking on the sidewalk in front of the house; however, the view blockage would be largely affected by the pedestrian's exact position along the sidewalk. Views from the street would also be slightly affected, but as the street is several feet higher than the sidewalk, the impacts would be even less significant. There would be very little impact on views from the public walkway adjacent to the property.

3. *Minimizes the appearance of visually intrusive structures.*

The project has been designed to minimize the appearance of visually intrusive structures by situating the addition to the rear and providing a very open design to the deck railing.

4. *Prevents obstruction of property owners' views by requiring appropriate construction of new structures or additions to existing buildings or adjacent parcels.*

There would be minor view impacts from the six foot high privacy screen on the deck to the property owner of 156 B Street; however, as the design of the privacy screen is quite open, with one inch pickets separated by four inch spaces, and the impact would be limited to a relatively small portion of the view, this impact would not be considered significant.

The property owner of 142 B Street is also concerned with view impacts to her property. There is a small view corridor between 145 B Street and 141 B Street that is visible from 142 B Street, although this view corridor is largely obscured by a redwood tree growing between the properties. From the ground floor living room window of 142 B Street, the project would fill in the remaining glimpse of the Mare Island Strait that can be seen from this window. As this is not a major view window, this would not be considered a significant impact.

The main views from this property are from the upstairs office window, which has a substantial panoramic view of the Strait and the horizon beyond. The project would be visible from this window and would block a small portion of the view of River Park; however, the amount of view blockage from this window is extremely small and would not be considered significant.

5. *Minimizes potential view loss from public areas.*

As noted above, the project, as proposed, would minimize impacts on views from the street, sidewalk and the public walkway along the north side of the property.

6. *Incorporates reasonably available design options that eliminate or lessen view obstructions.*

The project has gone through a number of iterations to reach the final project design. These iterations have been informed by comments from the neighbors and from staff, as well as utilizing the design expertise of the project architect. This final design has eliminated the features in the previous designs that obstructed views to provide a project that meets the needs of the property owners for additional living space while minimizing any potential view impacts to the public and adjacent property owners.

Staff has included one change to the project as a condition of approval: that the portion of the six foot "privacy screen" proposed across the deck parallel with B Street be removed from the project. The applicants have agreed to this condition but would like the opportunity to discuss the issues at the public hearing. The applicants have also indicated that they are considering a change to the design of the deck railing, making the railing horizontal rather than vertical as shown. As long as the proportions of open to covered space remains the same (5:1), staff recommends that this design change be approved with the project.

7. ENVIRONMENTAL DETERMINATION

The project has been reviewed for compliance with the California Environmental Quality Act (CEQA). As the project is located within the Bay Terrace District, a State Historic District, the project has been reviewed for compliance with the Secretary of the Interior's *Standards of the Treatment of Historic Properties (Standards)* per Section 15331 "Historical Resource Restoration/Rehabilitation"

of Title 14 of the California Code of Regulations. The project has been found to comply with Section 15331 as it consists of the rehabilitation of an historical resource in a manner consistent with the Secretary of the Interior's *Standards*.

8. CONCLUSION/RECOMMENDATION

Staff has determined that the proposed project, as conditioned, is consistent with the City's General Plan and Municipal Code, and all applicable ordinances, standards, guidelines, and policies. Therefore, staff recommends that the Planning Commission approve Site Development 05-0007, based on the findings and subject to the attached Conditions of Approval.

FINDINGS

1. The project qualifies for a categorical exemption per Section 15331 "Historical Resource Restoration/Rehabilitation" of Title 14 of the California Code of Regulations because the project consists of the rehabilitation of an historical resource in a manner consistent with the Secretary of the Interior's *Standards*.
2. The Planning Commission finds that applicant submitted a Site Development Permit application, which is required for the construction of an addition in the Residential View District that has the potential of view impacts pursuant to the City of Vallejo Municipal Code section 16.36.030. Site Development approval is governed by Chapter 16.90 of City of Vallejo Municipal Code.
3. Planning Commission finds, based on the facts contained in the staff report incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that:
 - a. The proposed project, including the decks, will maximize open space by limiting lot coverage to approximately 26 percent of the lot.
 - b. The project would not affect natural vegetation, land forms, or other features such as the mature Deodar cedar trees. As proposed, the project minimizes impacts to view corridors by placing the addition to the rear per Section 6 of this staff report.
 - c. The project has been designed to minimize the appearance of visually intrusive structures by situating the addition to the rear and providing a very open design to the deck railing.

- d. Prevents obstruction of property owners' views by requiring appropriate construction of new structures or additions to existing buildings or adjacent parcels per Section 6 of this staff report.
- e. The project, as proposed, minimizes impacts on views from the street, sidewalk and the public walkway along the north side of the property per Section 6 of this staff report.
- a. The project has gone through a number of iterations and has been revised to address concerns of neighbors and staff by incorporating reasonably available design options that eliminate or lessen view obstructions per Section 6 of this report.

EXPIRATION

This site development permit and ancillary minor exception shall expire in 18 months unless building permits are secured and construction has begun or unless this permit is extended by the City prior to the expiration date.

The applicant or any party adversely affected by the decision of the Planning Commission may, within ten days after the rendition of the decision of the Planning Commission, appeal in writing to the City Council. Such written appeal shall state the reason or reasons for the appeal and why the appellant believes he or she is adversely affected by the decision of the Planning Commission. Such appeal shall not be timely filed unless it is actually received by the City Clerk or designee no later than the close of business on the tenth calendar day after the rendition of the decision of the Planning Commission. If such date falls on a weekend or City holiday, then the deadline shall be extended until the next regular business day.

Notice of the appeal, including the date and time of the City Council's consideration of the appeal, shall be sent by the City Clerk to all property owners within two hundred or five hundred feet of the project boundary, whichever was the original notification boundary.

The Council may affirm, reverse, or modify any decision of the Planning Commission that is appealed. The Council may summarily reject any appeal upon determination that the appellant is not adversely affected by a decision under appeal.

ATTACHMENTS

1. Resolution
2. Project Plans

3. Photographs of site and views in the vicinity
4. Narrative from the applicants
5. Letter from Daniel and Debra Ryan stamped "Received" Nov. 28, 2007
6. Letter from Gary Heppell dated Nov. 28, 2007
7. Letter regarding objections to project submitted anonymously
8. Letter from Gary Heppell dated May 15, 2007
9. Letter from Sandra Lee, undated
10. Letter from Kim Geddes dated May 15, 2007
11. Letter from Kim Geddes dated March 18, 2005
12. Conflict of Interest Map

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC-07-

**A RESOLUTION OF THE PLANNING COMMISSION
APPROVING A SITE DEVELOPMENT PERMIT APPLICATION
SD 05-0007**

*Addition to an Existing Residence in the Residential View District
145 B Street
0051-132-090*

I. GENERAL FINDINGS

WHEREAS an application was filed by Gregory Klosowki of Ellipsis Architecture and Design, who was later replaced by Patricia Pollock of Sidewalk Studio, seeking approval for a site development permit for an addition to the existing residence, including a two-story addition to the rear and a deck on the north side of the building;

WHEREAS the City of Vallejo Planning Commission conducted a duly noticed public hearing to consider the application for the Site Development Permit on December 3, 2007, at which testimony and evidence, both written and oral, were presented to and considered by the Planning Commission;

WHEREAS, based on evidence received at the public hearing, the Planning Commission makes the following factual findings:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS.

Section 1. The project qualifies for a categorical exemption per Section 15331 "Historical Resource Restoration/Rehabilitation" of Title 14 of the California Code of Regulations because the project consists of the rehabilitation of an historical resource in a manner consistent with the Secretary of the Interior's *Standards*.

III. FINDINGS RELEVANT TO SITE DEVELOPMENT PROJECTS IN THE RESIDENTIAL VEIW DISTRICT AND FINDINGS FOR PROJECT APPROVAL AND FOR DETERMINATION OF PROJECT CONSISTENCY WITH APPLICABLE GENERAL PLAN

Section 2. The Planning Commission finds that applicant submitted a Site Development Permit application, which is required for the construction of an addition in the Residential View District that has the potential of view impacts pursuant to the City of Vallejo

Municipal Code section 16.36.030. Site Development approval is governed by Chapter 16.90 of City of Vallejo Municipal Code.

Section 3. Planning Commission finds, based on the facts contained in the staff report incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that:

1. The project will maximize open space preservation per Sections 5 and 6 of the staff report.
2. The project, as conditioned, protects view corridors, natural vegetation, land forms, and other features per Sections 5 and 6 of the staff report.
3. The project, as conditioned, minimizes the appearance of visually intrusive structures per Sections 5 and 6 of the staff report.
4. The project, as conditioned, prevents the obstruction of property owners' views by requiring appropriate construction of new structures and additions to existing buildings or adjacent parcels per Sections 5 and 9 of the staff report.
5. The project, as conditioned, minimizes potential view loss from public areas per Sections 5 and 9 of the staff report.
6. The project, as conditioned, incorporates reasonably available design options that eliminate or lessen view obstructions per Sections 5 and 9 of the staff report.
7. The project, as conditioned, meets the intent and specific standards and criteria prescribed in pertinent sections of the Zoning Ordinance per Sections 5 and 9 of the staff report.
8. The project, as conditioned, is consistent with the Vallejo General Plan per Section 4 of the staff report.
9. The project, as conditioned, shall serve to achieve groupings of structures that will be well related, one to another, and that, taken together, will result in a well-composed urban design, with consideration given to site, height, arrangement, texture, material, color, and appurtenances, the relation of these factors to other structures in the area, and the relation of the development to the total setting as seen from key points in the surrounding area; only elements of design that have some significant relationship to outside appearance shall be considered per Section 9 of the staff report.
10. The project, as conditioned, shall be of a quality and character that harmonizes with and serves to protect the value of private and public investments in the area per Sections 5 and 6 of the staff report.

11. The design of the project, as conditioned, conforms in all significant respects with the proposals of the Vallejo Heights Neighborhood Plan.

IV. RESOLUTION RECOMMENDING APPROVAL OF THE CONDITIONAL USE PERMIT APPLICATION FOR UP 05-0007, SUBJECT TO CONDITIONS OF APPROVAL

NOW, THEREFORE, LET IT BE RESOLVED that the Planning Commission hereby APPROVES the conditional Site Development Permit application SD 05-0007 for an addition to an existing house in the Residential View District based on the findings contained in this resolution and subject to the Conditions of Approval attached hereto as Exhibit A and incorporated herein by reference.

V. VOTE

PASSED AND ADOPTED at a regular meeting of the Planning Commission of the City of Vallejo, State of California, on the 3rd day of December, 2007, by the following vote to-wit:

AYES:

NOES:

ABSENT:

Charles Legalos, Chairperson
City of Vallejo Planning Commission

Attest:

Don Hazen
Planning Commission Secretary

Site Development 05-0007

145 B Street; APN 0051-132-090

CONDITIONS OF APPROVAL

A. Planning Division

1. Prior to building permit submittal, submit a numbered list to the Planning Division stating how each condition of project approval contained in this report will be satisfied. The list should be submitted to the project planner who will coordinate development of the project.
2. Prior to building permit submittal, submit revised plans to the Planning Division for review and approval that contain the following revisions:
 - a. Remove the portion of the six foot high “privacy screen” on the deck parallel to B Street.
 - b. Revise the proposed fence along the property line adjacent to the public walkway to provide greater visibility, either by using 1” wood balustrades or a wire mesh fencing (or some other view-type fencing).

B. Fire Prevention

1. Submit a numbered list to the Fire Prevention Division stating how each condition of project approval will be satisfied.

PROJECT REQUIREMENTS

A. Building Division

1. As of January 1, 2007, all plans shall be reviewed under the 2007 International Building Code. The project shall be designed to meet the standards and requirements in this code.

B. Public Works

1. The runoff from this property shall not adversely impact the neighboring properties.

C. Vallejo Sanitation and Flood Control District

1. Prior to building permit issuance, a VSFCD Connection Permit is required. Pay all applicable review and connection fees.

2. The project, as submitted, was incomplete. Provide site utility plan showing all existing and proposed sanitary sewer and storm drain facilities to serve project, including mains, laterals, connections, etc. In addition, please show location of sanitary sewer and cleanout on site utility plan.
3. Prior to occupancy/final building permit issuance, provide a standard VSFCD cleanout at the right-of-way/easement line per District standards and a two-way cleanout at the building per the U.P.C.
4. Direct roof drainage across non-paved areas prior to entering storm inlets and gutter, when feasible.
5. It is not clear how the new plumbing is proposed to connect to the District's sanitary system. Provide plans of proposed connection prior to building permit approval.

D. Fire Prevention

1. The project shall conform to all applicable requirements of Title 10 – Public Safety, the applicable California Fire Code, and all Vallejo Municipal Code amendments.
2. Prior to occupancy/final building inspection, install approved numbers or address on the building in a position clearly visible and legible from the street. Residential buildings shall have numerals or letters not less than 3 inches in height and of an approved color that contrasts with the background.
3. Prior to occupancy/final building inspection, all applicable fees shall be paid and a final Fire Prevention inspection shall be conducted. All meetings and inspections require a minimum 24-hour advance request.
4. Additional fire hydrants may be required in the street right-of-way. Submit a complete set of plans for review and approval. All fire hydrants are to have “blue dot” highway reflectors installed on the street adjacent to the fire hydrant to clearly identify the hydrant locations.
5. In Residential (Group R) Occupancies, single station smoke detectors shall be installed prior to occupancy/final building inspection in each sleeping area. When the dwelling unit is of more than one story (including basements), there shall be a smoke detector on each story. When a story is split into more than one level, the smoke detector shall be installed on the upper level.
6. Every sleeping room below the fourth story shall have at least one exterior opening for rescue purposes. The opening shall be a minimum of 5.7 square feet and 20 inches wide by 24 inches high. The finished sill height of the opening shall be no higher than 24 inches from the floor. Ladder access shall be provided for buildings over the first floor.

E. Water Superintendent

1. Submit a numbered list to the Water Division stating how each condition of project approval will be satisfied.
2. All water system improvements shall be consistent with the *Vallejo Water System Master Plan*, 1985, prepared by Kennedy/Jenks Engineers as updated by Brown & Caldwell, 1996. Prior to building permit submittal, water system improvement plans shall be submitted to the Water Division for review and approval, and shall contain at least:
 - a. Location and size of domestic service connection(s).
 - b. Location of fire hydrants.
 - c. Location of structures with respect to existing public water system improvements such as mains, meters, etc.
3. Comply with all fire flow requirements of the Fire Department. Fire flow at no less than 25 psig residual pressure shall be available within 1,000 feet of any structure. One half of the fire flow shall be available within 300 feet of any structure. For single-family residential units, the fire flow is 1,500 gpm.
4. Prior to building permit issuance, hydraulic calculations shall be submitted to the Water Superintendent demonstrating compliance with the fire flow requirements.
5. Fire hydrant placement and fire sprinkler system installation, if any, shall meet the requirements of the Fire Department. For combined water and fire services, the requirements of both the Fire Department and the Vallejo Water System Master Plan, with latest revisions, shall be satisfied.
6. Easements shall be granted for all water system improvements installed outside the public right-of-way in the City's Standard Form for Grant of Water Line Easement with the following widths:
 - a. 15 feet wide (minimum) for water mains.
 - b. 10 feet wide (minimum) for fire hydrants, water meters, backflow preventers, double detector check valves, etc.
7. Each unit or structure shall be metered separately.
8. Water service shall be provided by the City of Vallejo following completion of the required water system improvements and payment of applicable fees. Performance and payment bonds shall be provided to the City of Vallejo prior to construction of water system improvements. Fees include those fees specified in the Vallejo Municipal Code, including connection and elevated storage fees, etc., and fees for tapping, tie-ins, inspections, disinfection, construction water, and other services provided by the City with respect to the water system improvements. The Water Division may be contacted for a description of applicable fees.

9. Prior to occupancy or final building inspection, install water system improvements as required. Backflow devices, where required, shall be installed in areas hidden from public view and/or shall be mitigated by landscaping.
10. Submit plumbing calculations that the existing water service and/or meter size meets the current Plumbing Code requirements. If it does not, upsize the water service and meter size to recommended size. Application for the water service changes should be directed to Water Engineering at 202 Fleming Hill Road, Vallejo, CA 94589.
11. Pay applicable water service installation, meter set, and water connection fees if necessary.

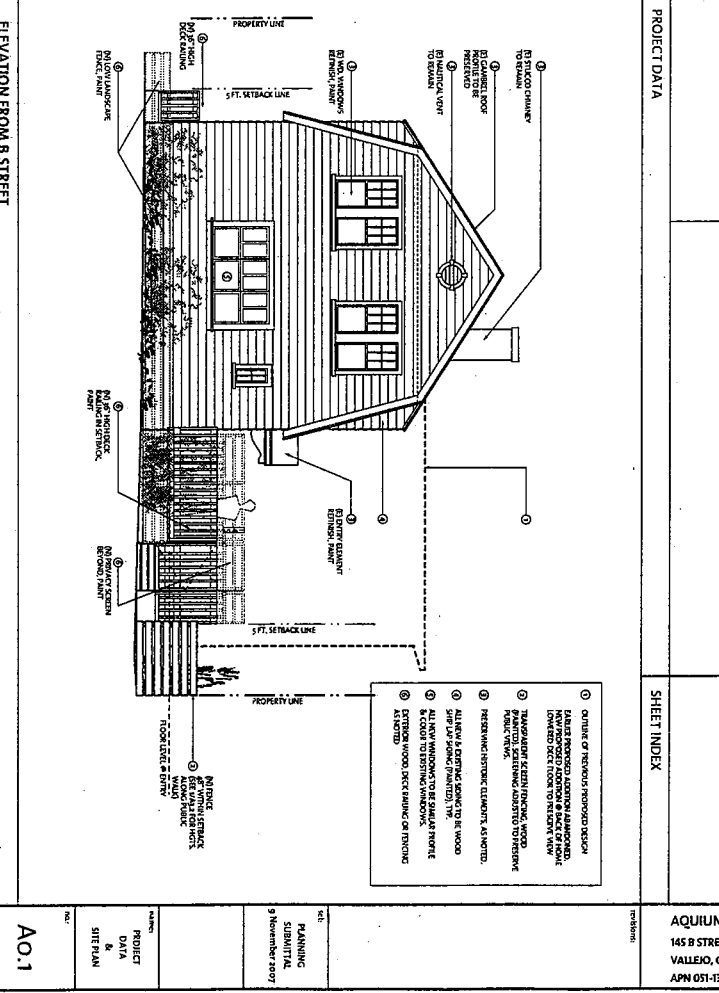
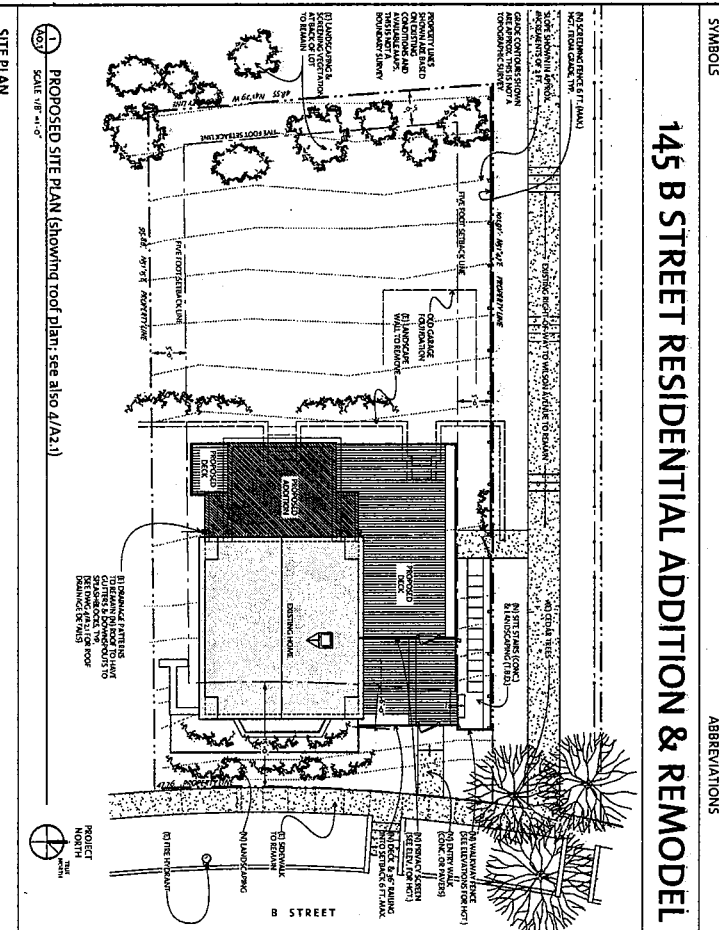
GENERAL CONDITIONS:

1. Construction-related activities shall be limited to between the hours of 7 a.m. and 6 p.m., Monday through Saturday. No construction is to occur on Sunday or federal holidays. Construction equipment noise levels shall not exceed the City's maximum allowable noise levels.
2. All contractors and subcontractors for the project must obtain a City of Vallejo Business License.
3. The conditions herein contained shall run with the property and shall be binding on the applicant and all heirs, executors, administrators, and successors in interest to the real property that is the subject of this approval.
4. The applicant shall defend, indemnify, and hold harmless the City of Vallejo and its agents, officers, and employees from any claim, action, or proceeding against the City and its agents, officers, and employees to attack, set aside, void, or annul this approval by the City. The City may elect, at its discretion, to participate in the defense of any action.

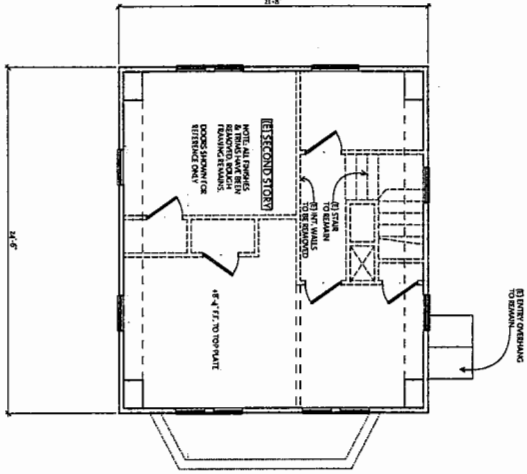
GENERAL NOTES	
1. CONTRACTOR TO VERIFY CONDITIONS AND DIMENSIONS AT THE SITE BEFORE ANY CONSTRUCTION. THE ATTENTION OF THE ARCHITECT BEFORE PROCEEDING WITH WORK.	4. COORDINATE EXACT LOCATION OF MECHANICAL EQUIPMENT, DUCTS, CHILLES, REGISTER, FIRES AND VENTS WITH DRAWINGS. ANY QUESTIONS REGARDING THE INTER-RELATIONSHIP TO THE LAYOUT OF THE NEW WORK SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO PROCEEDING WITH ANY WORK.
2. DO NOT SCALE DRAWINGS. WRITING SHALL BE CLEAR AND READABLE. LARGE SCALE DRAWINGS SHALL GOVERN OVER SMALL SCALE DRAWINGS. CONTRACTOR SHALL NOTIFY ARCHITECT OF ANY CONFLICTS IN WRITING PRIOR TO COMMENCEMENT OF WORK.	5. INSTALL ALL MECHANICAL EQUIPMENT AND PIPING IN CONFORMANCE WITH THE REQUIREMENTS AND RECOMMENDATIONS OF THE MANUFACTURER.
3. ALL DIMENSIONS TO FACE OF ROUGH UNLESS OTHERWISE NOTED.	6. PROVIDE ALL NECESSARY BLOCKING, BRACING, AND FRAMING FOR LIGHT FIXTURES, ELECTRICAL UNITS, PLUMBING FIXTURES, HEATING EQUIPMENT.
	7. CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND PROVIDING ALL PERMITS ON SITE UNLESS OTHERWISE NOTED.
	8. LAYOUT OF NEW WORK IS SUBJECT TO BASIS UPON RELATIONSHIPS TO EXISTING CONDITIONS. ANY QUESTIONS REGARDING THE INTER-RELATIONSHIP TO THE LAYOUT OF THE NEW WORK SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO PROCEEDING WITH ANY WORK.
	9. ALL WORK SHALL BE DONE IN STRICT ACCORDANCE TO THE BOO, CALIFORNIA BUILDING CODE, AND THE CITY OF VALLEJO JUDICIAL REQUIREMENTS.

PROJECT DATA			
PROJECT LOCATION	145 B STREET VALLEJO, CA		
OWNER	CHARLES AQUINA BARBARA LUCK		
ARCHITECT	side walk ARCHITECTS PATRICIA POLLOCK PO BOX 6598 SCOTT VALLEY, CA 95067 T. (925) 398-4968		
PROJECT DESCRIPTION	TO ADDITION OF 1065 SF HOUSE AND ADDITION OF 465 SF IS PROPOSED AT THE FIRST & SECOND FLOORS. AN ADDITION OF 152 SF IS PROPOSED AT THE BASEMENT. THE EXISTING INTERIOR WILL BE RENOVATED TO INCLUDE A REMOVED STAIR, KITCHEN, LIVING ROOM, 2 BEDROOMS AND 2 BATHROOMS. 693 SF OF DECK SPACE IS PROPOSED TO BE ADDED TO THE EXTERIOR.		
BUILDING CLASSIFICATION	EXISTING		
STORIES	3 STOREYS 1 BASEMENT		
ZONE	LDV100 OVERLAY		
OCCUPANCY TYPE	R-3 (SINGLE-FAMILY)		
TYPE OF CONSTRUCTION	TYPE V		
DOOR COMPLIANCE			
	ALLOWED	ACTUAL	
MANUFACTURE	VIA	485 STEEL AC	
BUILDING AREA	20K	1205 (INCL. DECK)	
MAX. COVERAGE	20K	20K	
STAIRS			
MAX. BUILDING HT.	15' 0" MIN	31' 2"	
STAIRS	5' 0" TYP		
SQUARE FOOTAGE	EXISTING	PROPOSED ADDITION	TOTAL
BASEMENT (UNFINISHED)	390 SF	152 SF	542 SF
FIRST FLOOR	1065 SF	467 SF	1532 SF
HOUSE (ESTIMATED FLOOR)	1455 SF	619 SF	2074 SF
DECK		693 SF	693 SF

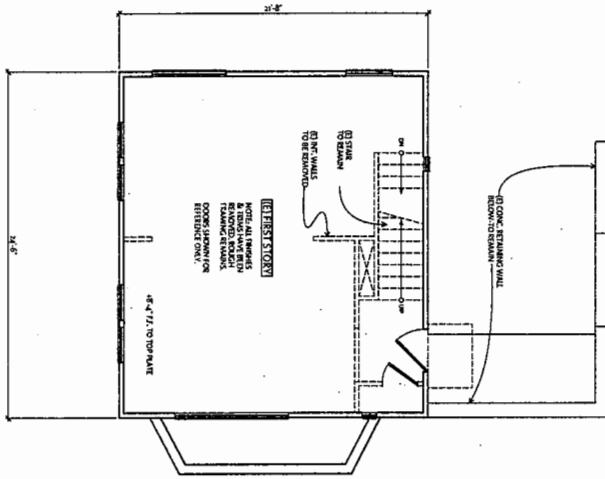
SHEET INDEX	
PROJECT DATA & SITE PLAN	A0.1
EXISTING FLOOR PLANS	A1.1
EXISTING ELEVATIONS	A2
PROPOSED FLOOR PLANS	A3.1
PROPOSED ELEVATIONS	A3.2
PROPOSED ELEVATIONS	A3.3



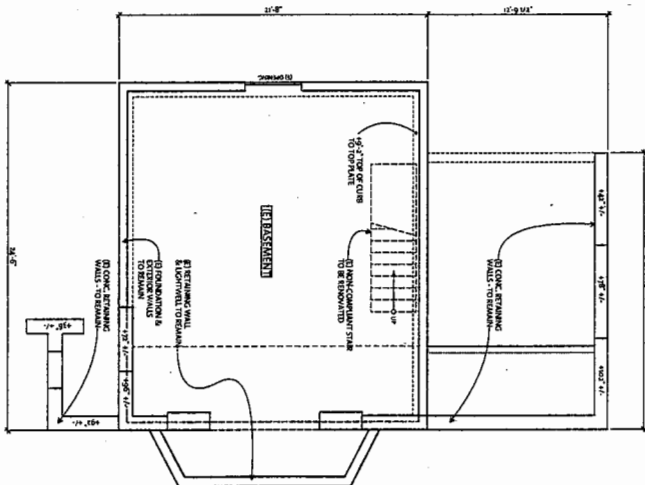
PROJECT DATA	
PROJECT DATA & SITE PLAN	A0.1
EXISTING FLOOR PLANS	A1.1
EXISTING ELEVATIONS	A2
PROPOSED FLOOR PLANS	A3.1
PROPOSED ELEVATIONS	A3.2
PROPOSED ELEVATIONS	A3.3



3 EXISTING SECOND FLOOR PLAN
Scale: 1/4" = 1'-0"



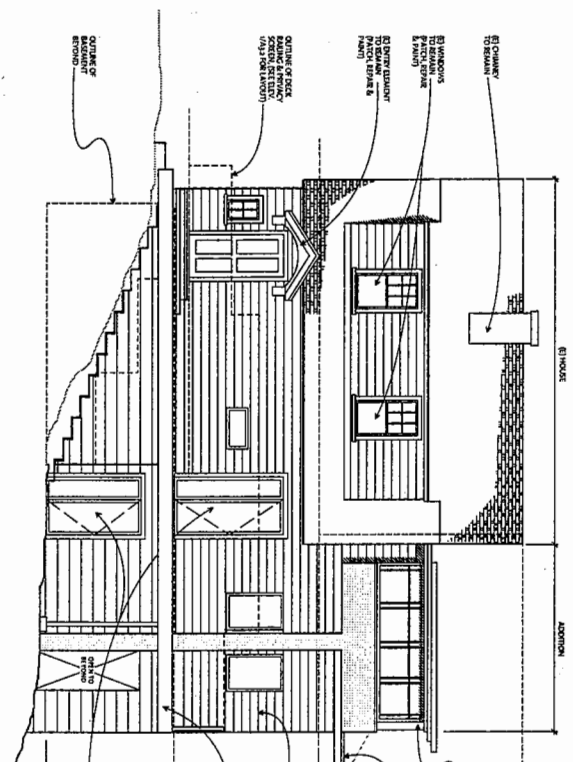
2 EXISTING FIRST FLOOR PLAN
Scale: 1/4" = 1'-0"



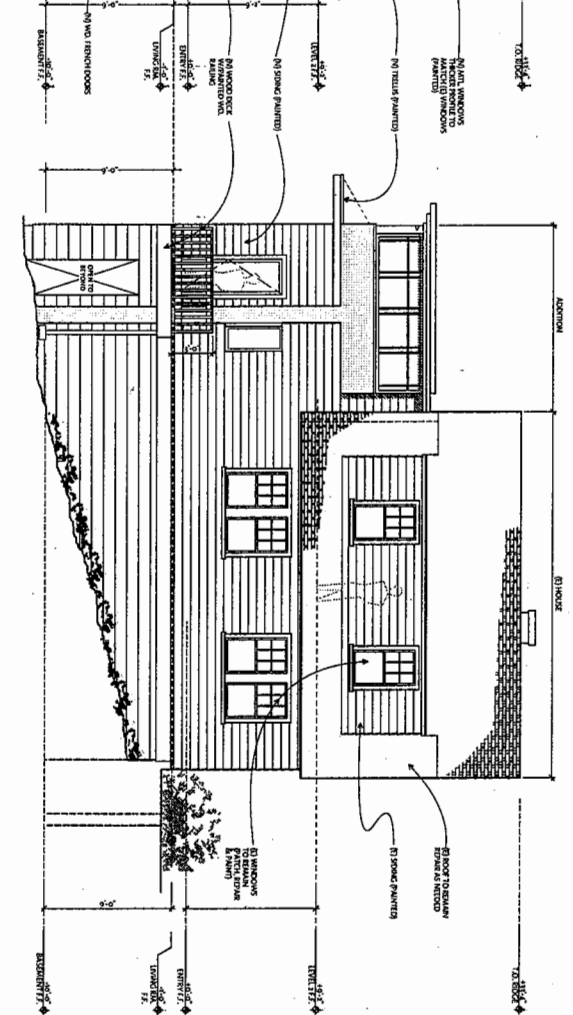
1 EXISTING BASEMENT PLAN
Scale: 1/4" = 1'-0"



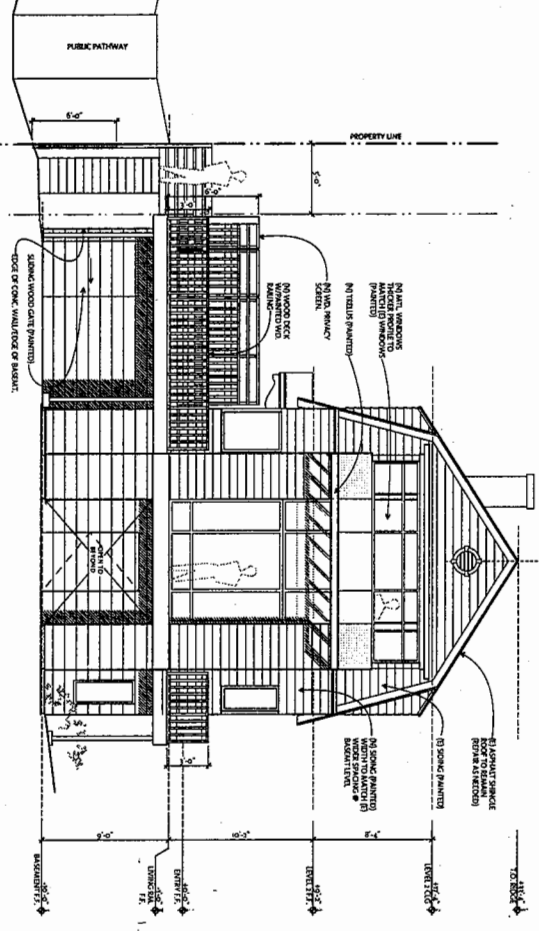
2.3 PROPOSED SIDE ELEVATION (NORTH)
SCALE: 1/4" = 1'-0"



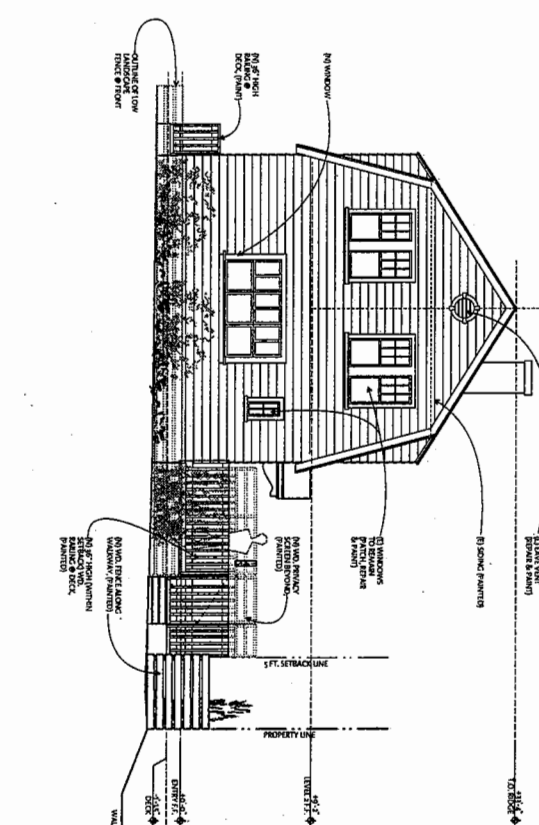
2.3 PROPOSED SIDE ELEVATION (SOUTH)
SCALE: 1/4" = 1'-0"



2.4 PROPOSED REAR ELEVATION (WEST)
SCALE: 1/4" = 1'-0"



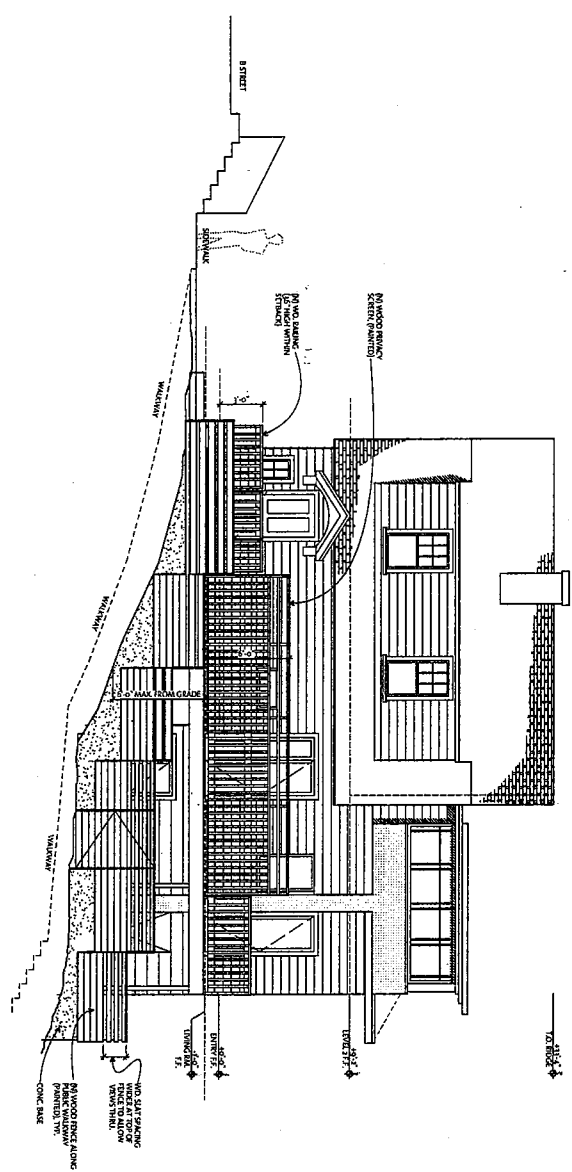
2.1 PROPOSED FRONT ELEVATION (EAST)
SCALE: 1/4" = 1'-0"



1
A3.2

VIEW FROM PUBLIC WALKWAY LOOKING SOUTH

SCALE: 1/4" = 1'-0"



A3.2

DATE: 9 November 2009
 PROJECT: AQUILINA/LUCK RESIDENCE
 DRAWING: PLANNING SUBMITTAL
 SHEET: ADDITIONAL ELEVATIONS

DESIGNER: side walk
 ARCHITECTURE

145 B STREET
 VALLEJO, CA
 APN 051-132-090

side walk
 design build
 architecture

side walk
 architecture

View from Sandra Lee's living room

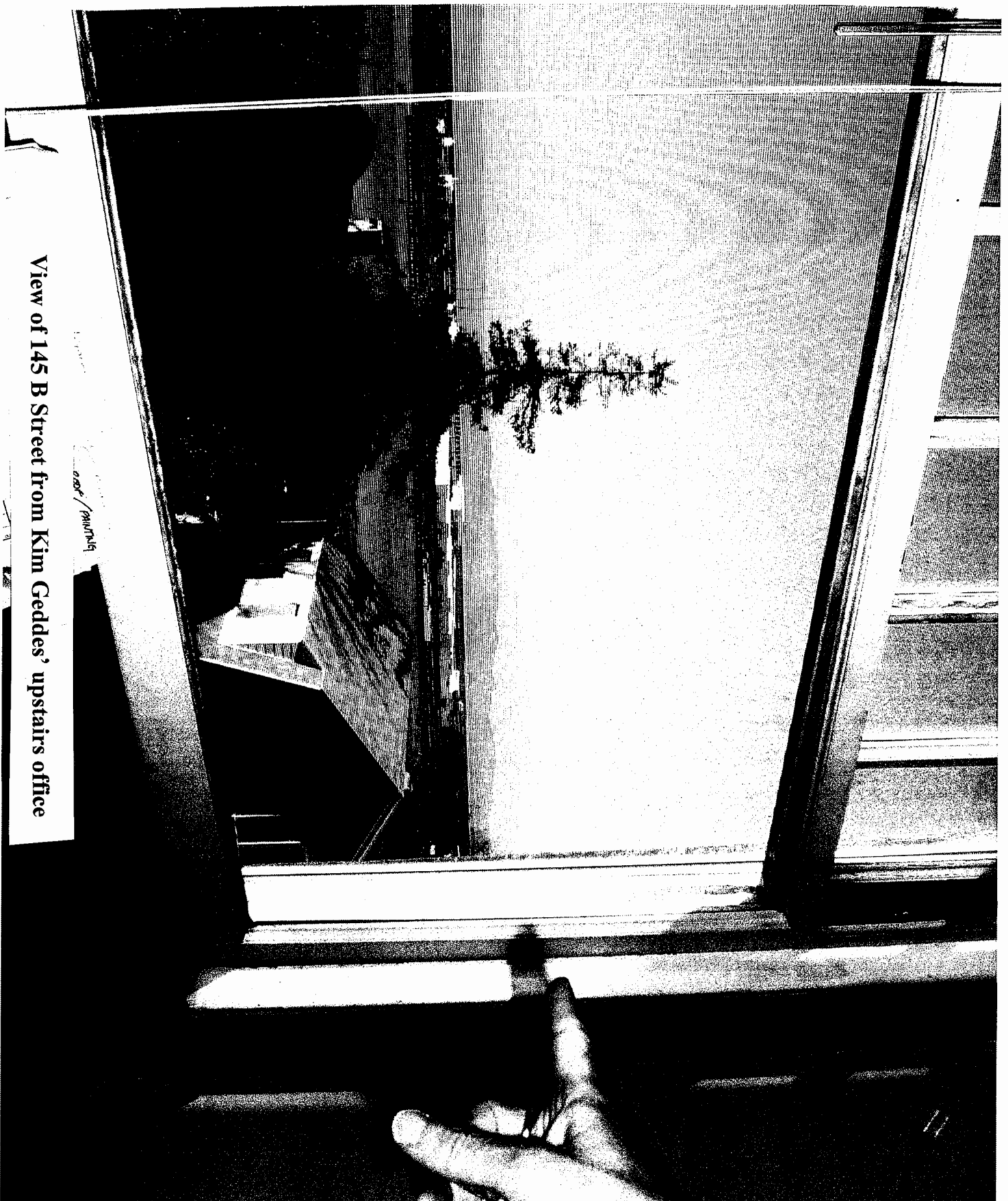


View from Kim Geddes' living room



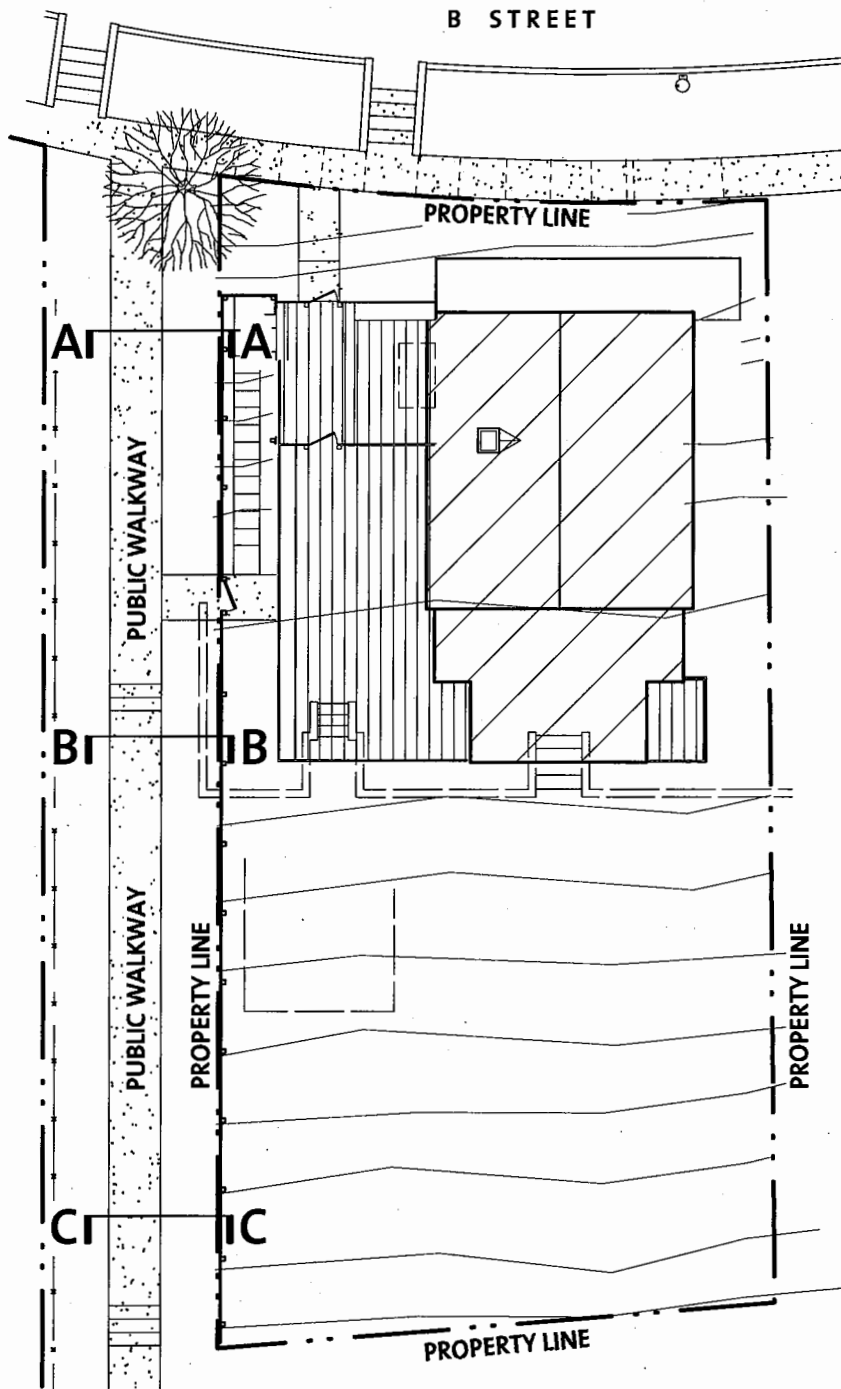
View from Kim Geddes' living room





View of 145 B Street from Kim Geddes' upstairs office

0208 / 0208/14

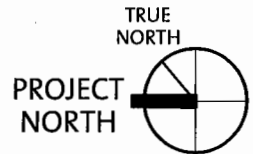


LEGEND ONLY
FOR SITE PLAN INFORMATION
SEE SHEET AO.1.

1
Ad1

SITE LEGEND

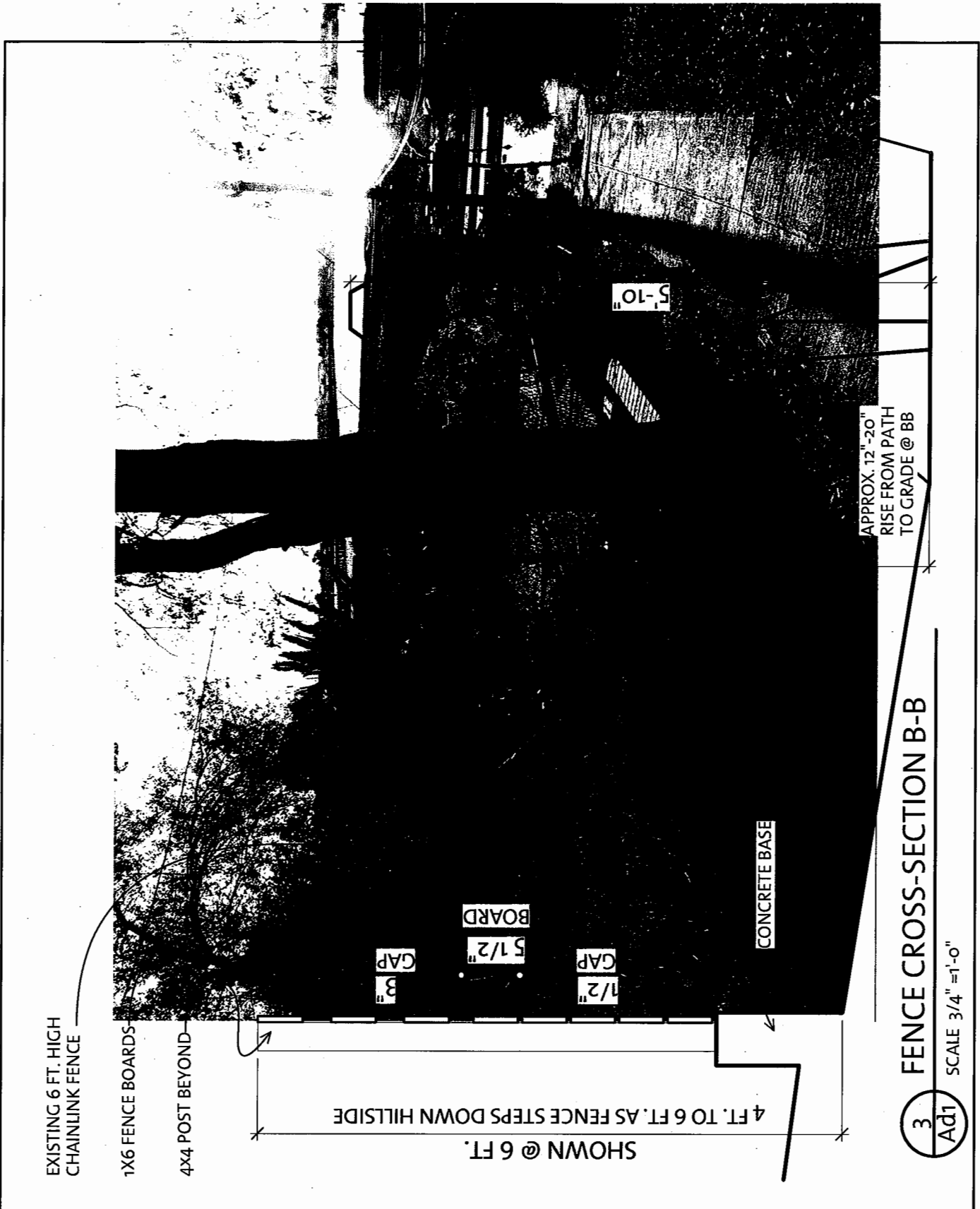
SCALE 1/16" = 1'-0"



AQUILINA/LUCK RESIDENCE
145 B STREET
VALLEJO, CA
APN 051-132-090

**PUBLIC WALKWAY
PROPERTY LINE FENCE**
(addendum to planning submittal)

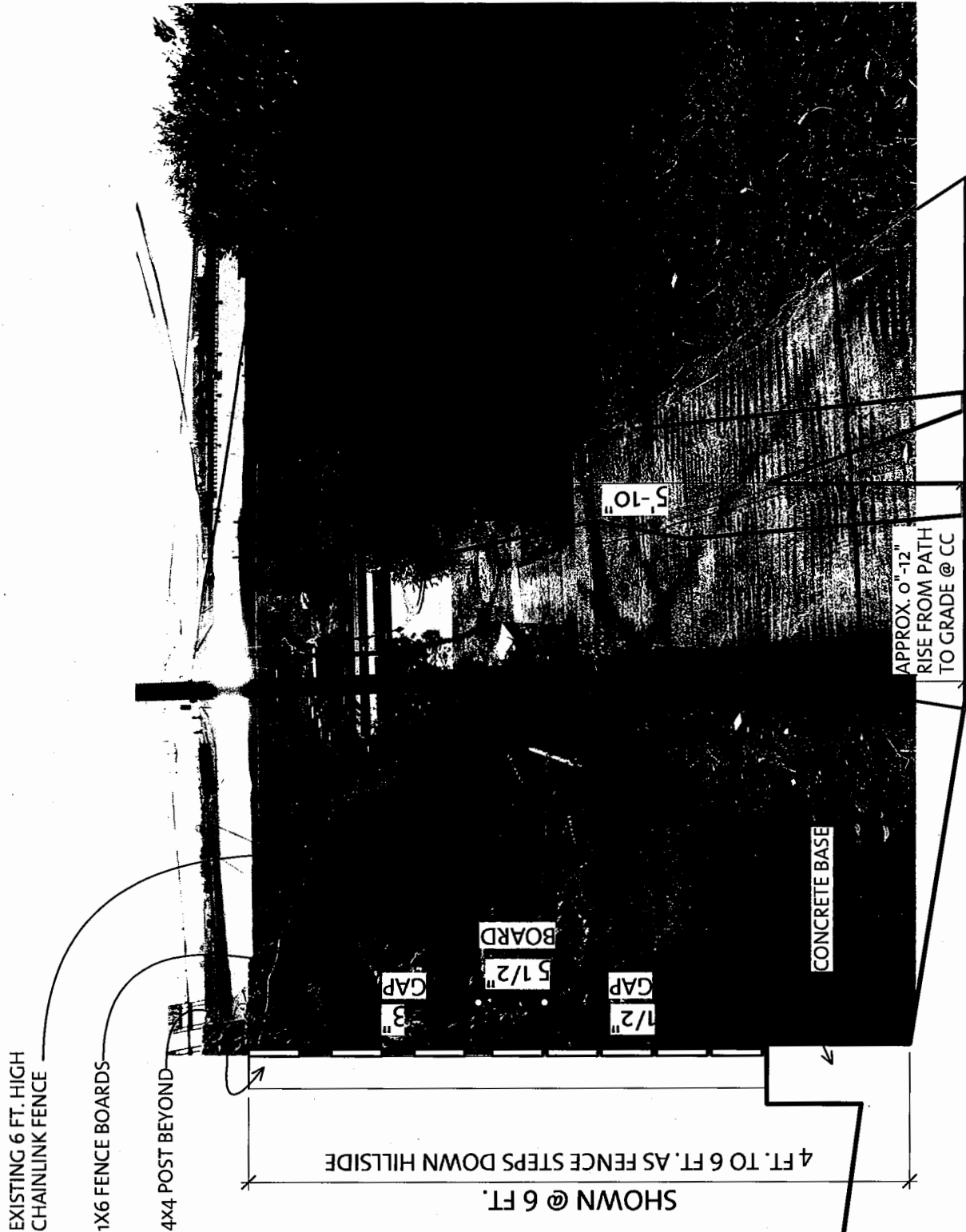
PAGE 1 OF 4
Ad-1
20 November 07
sidewalk studio



AQUILINA/LUCK RESIDENCE
 145 B STREET
 VALLEJO, CA
 APN 051-132-090

**PUBLIC WALKWAY
 PROPERTY LINE FENCE**
 (addendum to planning submittal)

PAGE 3 OF 4
Ad-1
 20 November 07
 sidewalk studio



EXISTING 6 FT. HIGH CHAINLINK FENCE

1X6 FENCE BOARDS

4X4 POST BEYOND

4 FT. TO 6 FT. AS FENCE STEPS DOWN HILLSIDE
 SHOWN @ 6 FT.

GAP 3"

BOARD 5 1/2"

GAP 1/2"

CONCRETE BASE

5'-10"

APPROX. 0"-12" RISE FROM PATH TO GRADE @ CC

FENCE CROSS-SECTION C-C

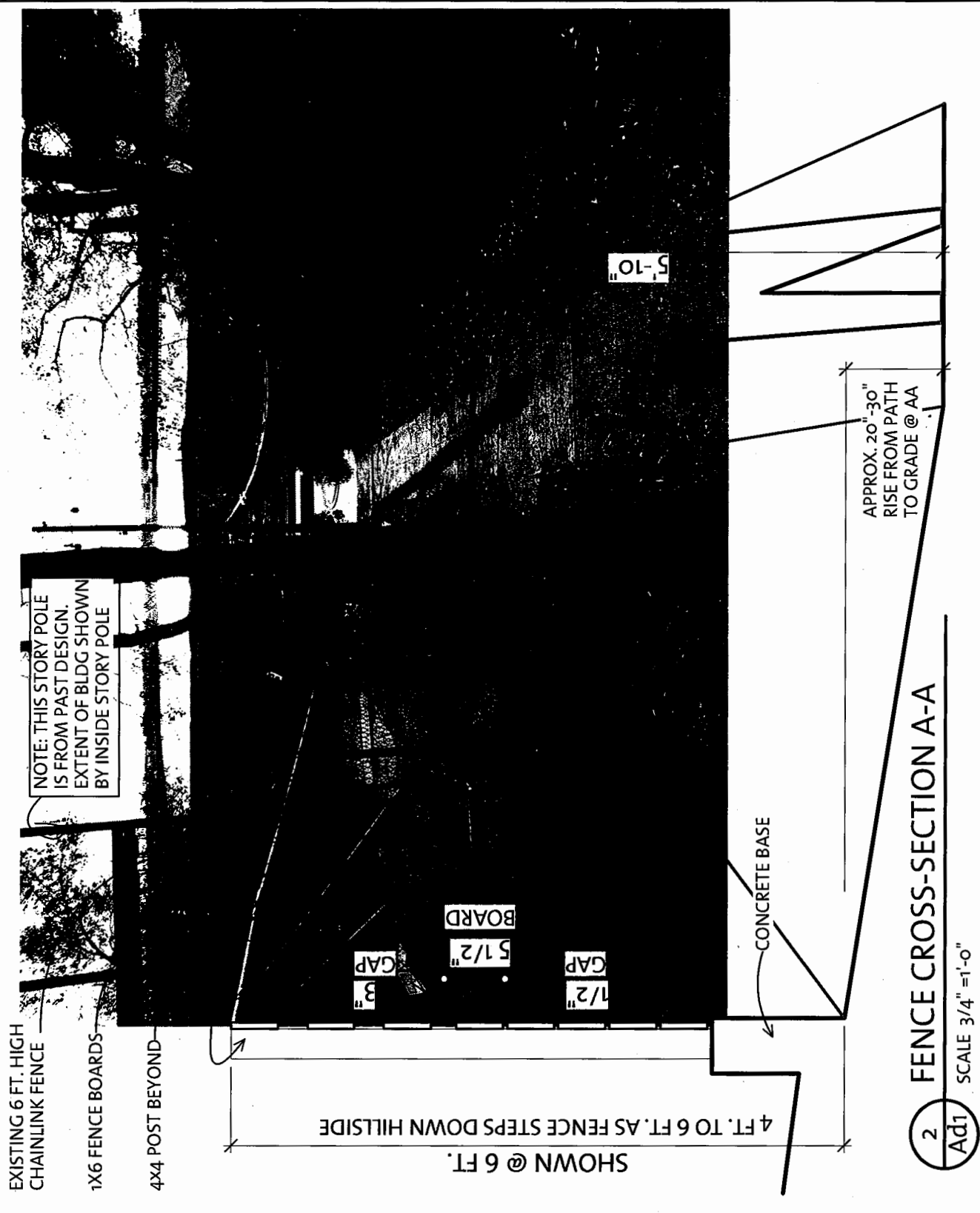
SCALE 3/4" = 1'-0"

4 Ad-1

AQUILINA/LUCK RESIDENCE
 145 B STREET
 VALLEJO, CA
 APN 051-132-090

**PUBLIC WALKWAY
 PROPERTY LINE FENCE**
 (addendum to planning submittal)

PAGE 4 OF 4
Ad-1
 20 November 07
 sidewalk studio



AQUILINA/LUCK RESIDENCE
 145 B STREET
 VALLEJO, CA
 APN 051-132-090

**PUBLIC WALKWAY
 PROPERTY LINE FENCE**
 (addendum to planning submittal)

PAGE 2 OF 4
Ad-1
 20 November 07
 sidewalk studio

Rec'd. 6/7/07

DH

Our subject is the unfinished house at 145 B St.

We have had a hard time completing this house—initially because of some choices we made. But now, an obstacle to our continued progress seems to be communication with the City, and until we address and rectify that, all our time, money and continued effort are fruitless.

Some history

We bought the house hoping to refurbish it and expand it so that we would live there. When we'd first taken possession of the house, we'd thought we could save the old foundation. But several engineers and contractors recommended that we completely replace the foundation. Besides having no rebar at all, it had no drainage and had failed because it was sitting on very loose soil that had probably been dumped all along the west side of the block during the original excavation of B St. That dumped soil had never been properly tamped.

We had the house lifted to make space to work on the foundation. This made the work easier. Phil Joy, house mover, put the house on cribs. We had been trying to reach him by phone for months and one December day, out of the blue, he not only called back, but said he was already on site and starting to work on the house. In retrospect, lifting a house with no notice in the middle of winter (when foundation work should follow) is not a good idea. Lesson learned #1.

We had an architect render drawings, took out permits, and proceeded with the foundation work. We were stalled by events we soon learned are common in construction (concrete worker's debilitating car accident, his father's hospital treatments, his practice of taking and working on new jobs before completing ours). Lesson #2. Nonetheless, we finished the foundation and got Phil Joy back to lower the house. During that process, the contractor called Charlie at work: the I-beams that had been holding the house up temporarily had been placed such that it would now be necessary to cut the plate on the new basement walls if we wanted to keep the original basement height of 8' 6" after lowering the house. The contractor recommended creating a new and higher plate to accommodate the height and easy removal of the I-beams. Charlie called the Building Department at the City and was told that as long as the house stayed below 35', it was fine. So, though we could have followed the contractor's suggestion to raise the house, saving money with the easier solution and still complying with the height restriction the City had just given us, Charlie decided that the house would be too high and instructed the crew to cut the plate on the new basement walls and keep the basement of the lowered house as close as possible to its original height of 8' 6". The present height of the basement is just under 9'. In addition, there is an existing sidewalk on the property that led to and was flush with the basement floor. When we measure from that sidewalk to just beneath the floor joists, we get 9'. Another fact to note: dirt that we dug out for front foundation footings and French drains has not yet been replaced, temporarily exposing more house. This visually exaggerates the height of the house. The house now sits, at most, 14 inches higher than it used to.

We then sought out an architect whose work we had admired. Unfortunately, we were probably too patient as we tried to work with him for a year while he underwent skin cancer treatment. We finally understood that he could not produce work on our project, and we moved on. Another lesson learned. We carefully interviewed more architects and selected Greg Klosowski, an Oakland-based architect, to work with. By now, the neighbors were understandably running out of patience. Their complaints to the City instigated a process whereby the City would monitor

our progress, and we would adhere to an agreed-upon schedule. Our hope was that we would move steadily and efficiently to submittal to the Planning Commission. We appreciated Leon McNeil's guidance, as he was familiar with the history of this project.

Our architect, Greg Kłowsowski, soon after met with planner Katherine Donovan, who was now assigned to our project. Greg was told of the coming deadline for a plan set to submit to the Planning Commission. The timeline was tight, but he felt that with extra work, including work done over the Christmas holidays, he could produce the required plan set on time. Katherine looked at initial sketches that clearly indicated our direction and applauded them. She said, in person and in several emails to Greg over the course of his months of work, "looks good—keep going." We kept going. Greg worked through the holidays and met the deadline. Yet as soon as the plan set was delivered, we were informed that it would not work. All of a sudden we had view issues. We had tried to keep the spirit of the view ordinance in mind as we'd designed, and had not changed direction since receiving the initial go-ahead from the City. Though we had previously been given the view ordinance, all concerned parties—we, Greg, and the City—found it to be brief and vague. Charlie had talked to several planning commissioners who had indicated that the view ordinance was very weak, and its lack of direction had caused problems in the past. They had even suggested that if we found better ordinances from other cities we should bring them in. But we needed to stay focused on our project and not work on recrafting the view ordinance.

Katherine cited neighborhood opposition based on views. Upon investigation, we found that only two letters had been written by dissenting neighbors. One was from the woman whose view would most directly be impacted by our proposed addition. But note:

- The view she wishes to preserve did not exist when she bought her house. It only opened up when we had our trees trimmed.
- When she told us about the importance of seeing what she calls "my Golden Gate," we fretted for weeks about that and how to reconfigure our plans to accommodate it. She later explained that the blue Mare Island gate is what she calls her Golden Gate.
- A large, mature, "view-blocking" tree on our lot mysteriously and rather suddenly died. We've wondered if that one tree may have been tampered with.
- The second letter, from a different neighbor, complains that we raised our house substantially. In talking to the letter's writer, we learned that she based her conclusion on the current height of our roof relative to the house next door. There is no reason to believe that the two roofs ever lined up. The area's hilliness guarantees no such standardization. Furthermore, that neighbor alleged that we built the foundation without permits, which is patently untrue. Her letter also mentions that "we are not necessarily looking to have the house lowered to the original height," but only to preserve the view corridor.
- Though the house was raised, at most, 14 inches, the City has talked to us about it having gone up 4–6 feet. We have also heard those figures bandied about at a neighborhood meeting. We don't know where those numbers originated.

Katherine Donovan had recommended that we arrange a meeting with B St neighbors to keep them informed and try to gain their support. Especially since we wanted to live in the house and knew a few of the neighbors already, we took the suggestion to heart. Unfortunately, the

“meeting” turned into three meetings, all at our expense, for the following reasons. We set up our initial meeting through the neighborhood association, as was recommended by the City. On a rainy weekday Charlie skipped work, and we and the architect attended the meeting. We thought the low attendance—two people—was due to the weather. We later learned from neighbors that the association had never notified neighbors of the meeting. The association president then confirmed that she had made it a “board meeting” to prevent the gathering from turning into a “bitch session.” To then address the concerns of the neighbor with the most relevant view issues, we brought the architect up again, at our expense, to meet with her privately in her living room where views could be accurately assessed. We then, with great effort to accommodate neighbors’ scheduling conflicts, arranged a third meeting, which would finally include all interested neighbors. Our architect had worked to present a new placement for the addition, one responsive to the view issues. Despite valiant efforts by the neighbor facilitating the meeting to move us all forward and look at the new direction, the meeting pretty well degenerated into complaints and false accusations. No, we had not dumped abandoned vehicles at our property or housed drug dealers. City employees, who had promised more than once to attend these meetings, had made it to none of them.

Where to go from here?

Talking with both Building and Planning officials after the last neighborhood meeting, we received conflicting messages. Building suggested we just submit our plan to the Planning Commission, since it existed and might pass. Everyone has agreed it’s a beautiful and exceptionally well-drawn proposal. We no longer wanted to submit it, because we didn’t want to alienate the neighbors. But urged once again by Building to submit, Barbara went across the counter to Planning and told Katherine that we might like to try that. She told Barbara flatly that it would never pass. When asked why not, she cited view issues. When asked why she hadn’t called those out earlier in the process, she said that the neighbors’ complaints were what had alerted her. Why? The City had been overseeing the plans for many months and had told us to keep going. We always assumed that submission to the Planning Commission would offer us an objective review. Yet now, we would not have that opportunity. Confused about how to proceed, we contacted John Bunch, who put us in touch with Brian Dolan. Brian responded quickly. He was the first Planning official to meet with us on site, and once there, gave us clear direction about where we should and should not consider building an addition. Within two hours, we had clear information that could have saved us nearly a year of wasted work, many thousands of dollars, extreme emotional anguish, and further alienation from the neighbors.

We set out to start again. Greg, our architect, had wearied of the process, largely we believe because of what the City had put us all through. We were and still are on good terms, but he resigned, needing to work on projects he could complete. We conducted another search and hired another architect, Patricia Pollock. She had worked for a prestigious firm in San Francisco and had a background in construction management, an important qualification to us.

In the meantime, our budget, which has remained the same, was in practical terms shrinking, as material costs soared. We were losing our enthusiasm for our plan to move to B St anyway, so we decided to pare down the project to include only a minimal addition. We would try to build where Brian Dolan had suggested.

We have worked well and efficiently with Patricia, who was well-informed of this difficult history. Now, several months and more thousands of dollars into our project, she contacted Brian Dolan for information. We have been keeping Brian apprised of our efforts. He knew we were working diligently with a new architect, scaling back and hoping to submit this spring. When Patricia was passed to Katherine Donovan, imagine our surprise when she was told that nothing could move forward until we address the fact that we'd raised our house more than 4 feet. Assuming it was important to get to the bottom of that issue, couldn't someone have notified us eight months ago, after our last "neighborhood meeting"? Or a year ago, when the letter that complains of this issue was submitted? Or at any of the meetings and emails between Greg Klosowski and Planning? Or on any of the eight or more visits we made to the Planning Department in the summer of 2005 when we worked to unravel and clarify all past history?

We simply cannot endure another serious setback that seems to result from poor communication. When our previous plan was effectively dead, we wondered about the fee we'd paid the City. We were told it could be applied to the new submittal, but that some money would have to go to the time spent serving us. We hate to come to this conclusion, but we don't yet feel we have been well served.

The history stated here is our own account, but we've worked hard to make it accurate and complete. Our intention is not to point fingers at anyone or any department. But we have at times truly felt at our wits' end, even as we work sincerely and diligently to move this project forward. It is in this spirit that we hope we may now move on with the work.

Sincerely,

Charlie Aquilina and Barbara Luck

Daniel J. Ryan O.D., Douglas H. Lanning, O.D.
2437 Buhne Street
Eureka, Ca 95501
Office: 707-443-4581 Fax: 707-269-7137

To: City of Vallejo – Planning Division
C.O. Dan Hazen – Planning Manager

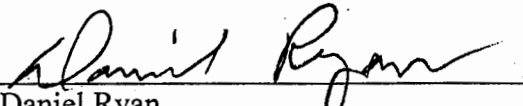
RE: 145 B street Vallejo, CA 94590
Site development - 05-0007




This letter will inform you of an opinion on 145 B Street application. Dan and Debra Ryan own 148 B Street – directly across the street from 145 B Street. We find no view issues; this project will not interfere with our view. From 1955, Debra Ryan played in the houses to each side (142 B Street and 156 B Street). These two properties assert view hindrance. This is NOT so or true. Let the city enter these properties and even ours at 148 B Street to view the situation. This would be fair and unbiased. We were approached by one of the opposing parties to act negative on this project on 4/07. These two parties have a reputation of unclean hands when dealing with their neighbors. Their motive can only be assumed at this time.

The completion of this project will not only increase neighborhood housing value but also help a neighborhood in need of a face lift. Dan and Debra Ryan have been owners of 148 B Street. Since 1978 – we know what's going on!! Please pass the project. It is much needed for Vallejo Heights residential development.

Sincerely,


Daniel Ryan


Debra Ryan

LAW OFFICES

FAVARO, LAVEZZO, GILL, CARETTI & HEPELL

ALBERT M. LAVEZZO
THOMAS L. GILL
LOUIS S. CARETTI
GARY K. HEPELL
JUDITH L. CARLISLE
DEBORAH DURR FERRAS
CHARLES B. WOOD, III
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MICHAEL E. COAN,
Law Firm Manager, Law Clerk
DEBORAH L. FLORES,
Office Administrator

FAIRFIELD OFFICE
724 TEXAS STREET
FAIRFIELD, CA 94533
(707) 425-3977

NAPA TELEPHONE
(707) 224-4737

November 28, 2007

REPLY TO VALLEJO

City of Vallejo Planning Commissioners
Charles Legalos
Kent Peterman
Robert McConnell
Norm Turley
Bruce P. Gourley
Suzanne Harrington-Cole

RE: 145 B Street, Vallejo, California
Vallejo Planning Commission Agenda, December 3, 2007

Ladies and Gentlemen:

We represent Sandra Lee, who lives at 156 B Street, with regard to the development application of Charles Aquilina and Barbara Luck, 145 B Street. We are concerned that the proposed addition would degrade the view enjoyed by Ms. Lee of the Napa River and the Mare Island Causeway. The properties are located in a residential view district and the historic Bay Terrace neighborhood.

Ms. Lee is a fine art painter for whom light and landscape are very important. Ms. Lee moved to Vallejo in 2000 in large part because of the unique neighborhood of Bay Terrace and the view of the Napa River. Ms. Lee has made significant contributions to the arts in Vallejo.

Ms. Lee's view is not panoramic by any means, but from her living room and front porch she can see the water, part of River Park, and part of Mare Island, including the causeway.

The addition proposed by Mr. Aquilina and Ms. Luck wraps around the back of the house (west elevation) and the entire north elevation. It is primarily the addition on the north side of 145 B Street that so concerns Ms. Lee. The proposed addition on the north side consists of a large deck, approximately 40' x 14', surrounded by a wooden railing. Most of the deck, at least on the north side, will be constructed on a foundation for which we understand the owner had no City approval when he constructed the same in late 2000 or early 2001.

Rec'd 6/7/07
[Signature]

OBJECTIONS MADE RE: AQUILINA PROPERTY

From the silly to the serious

- It scares children because it's a spooky, empty property (RESPONSE: if that is true, it also inspires creativity; at least one child has drawn pictures and written a whole storybook on the property; besides, who decided that it was "spooky"—some adult, as a way to control a child?)
- It attracts pests such as rats, snakes, termites, and weeds, which, apparently will move onto nearby properties (RESPONSE: this is a supposition. Where is the evidence?)
- It attracts crime by being empty; one B Streeter stated, "I've seen people jumping around inside, laughing and playing." (RESPONSE: The windows of the property have been boarded up solidly and a new door has been installed for the past year (?). If "people" have been inside, do others have x-ray vision to see through and into the house? Is jumping, laughing, and playing a crime? What is the crime the house has attracted? What police reports substantiate this supposed crime?)
- It deprives us of a potentially great neighbor. (RESPONSE: One cannot determine that a potential neighbor will be "great," let alone friendly; B Street has had—and perhaps still does have—other neighbors that have not been friendly or "great." The quality of neighbors is not a guarantee.)
- It has raised refinancing rates "of anyone nearby." (RESPONSE: If that is true, what is the evidence that the property—and not the market itself—was the cause of this raise? In fact, at least a couple of neighbors can attest to having solicitations of lower refinancing rates.)
- It caused the sisters Kathleen and Barbara to uproot themselves and move to the East, leaving their friends and "lifelong" hometown. (RESPONSE: First, the sisters did not live in Vallejo all their lives. Second, how did the property itself cause them to move if their last house was not in sight of it? Third, their decision to leave the original duplex was based on only one appraisal by a friend of a neighboring couple who promptly traded properties with the sisters who did not get a second appraisal. The Aquilina property certainly did not prevent that couple of attaining that duplex or from buying yet another house on that street.)
- It caused property value to go down. (RESPONSE: a new neighbor about two years ago bought a house diagonally across and down the street within view of the empty property for over a half million dollars.)
- It creates ill-will and anger because of the owners disdain for the neighborhood, shown by their inaction and empty promises. (RESPONSE: The owners have not disdained the neighborhood but have tried over and

over to work with them. The owners have spent thousands of dollars to have plans drafted and redrafted, each time to be thwarted by one or two neighbors' objections. The owners have not been inactive, but have called for meetings, met with city officials, re-designed the house several times to please one or two people, etc.

- (RESPONSE: Finally, one also wonders if some resentment by two other property owners in the immediate area isn't behind some of the anger and ill-will. Those two separate entities each offered absurdly low prices to buy the house but were refused. These two owners are known to be among the most aggressive in sending e-mails throughout the neighborhood re: the empty property, e-mails that continue to keep at least ten or twelve of the B Streeters riled.)

LAW OFFICES

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REPLY TO VALLEJO

ALBERT M. LAVEZZO
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JUDITH L. CARLISLE
DEBORAH DURR FERRAS
CHARLES B. WOOD, III
DANIEL B. NEWBOLD
TRAVIS J. DILLON
OF COUNSEL
BERNARD J. FAVARO

May 15, 2007

City of Vallejo Planning Commissioners
Kent Peterman
Linda Engelman
Charles Legalos
Gail Manning
Norm Turley
Robert McConnell
Gary Salvadori

RE: 145 B Street, Vallejo, California
Vallejo Planning Commission Agenda, May 21, 2007

Ladies and Gentlemen:

We represent Sandra Lee, who lives at 156 B Street, with regard to the development application of Charlie Aquilina and Barbara Luck, 145 B Street. The proposed addition would seriously degrade the view enjoyed by Ms. Lee of the Napa River and the Mare Island Causeway. The properties are located in a residential view district and the historic Bay Terrace neighborhood.

Ms. Lee is a fine art painter for whom light and landscape are very important. Ms. Lee moved to Vallejo in 2000 in large part because of the unique neighborhood of Bay Terrace and the view of the Napa River. Ms. Lee has made significant contributions to the arts in Vallejo.

Ms. Lee's view is not panoramic by any means, but from her living room and front porch she can see the water, part of River Park, and part of Mare Island, including the causeway.

The addition proposed by Mr. Aquilina and Ms. Luck wraps around the back of the house (west elevation), the entire north elevation, and part of the south elevation. It is primarily the addition on the north side of 145 B Street that so concerns Ms. Lee. The proposed addition on the north side consists of a large deck and freestanding bathroom structure that would sit at the outer edge of the deck. Most of the deck, at least on the north side, will be constructed on a foundation for which we understand

City of Vallejo Planning Commissioners
May 15, 2007
Page 2

the owner had no City approval when he constructed the same in late 2000 or early 2001.

The proposed bathroom structure, inexplicably situated away from the house and on the outer edge of the deck, would completely block Ms. Lee's view of the causeway and much of her water view. The bathroom is not the entire problem, however. The deck is proposed to be surrounded by a "privacy fence" (see, e.g., sheet A2.1 of the plans). That is exactly what the plans show. There is no seeing through a privacy fence. Even if the bathroom were removed, a railing of standard size and construction around the deck would block Ms. Lee's view of the water. We submit that if a deck is to be built on the north side of 145 B Street, it must be scaled back in size so as not to extend so far from the house on the north side. Further, the railing must be transparent (e.g., plexiglass).

We understand the deck extends into the setback zones at the front of the house and on the north side.

Although the Planning Department has been unable to find a permit for this work, the neighbors contend the house at 145 B Street was raised several feet in 200-2001 when a new foundation was installed. Subsequent plans submitted by the owners already show the basement area is constructed to accommodate one or more rooms. We submit that if the owners need an additional bathroom, one can be installed in the basement.

In addition to the obstruction of a substantial part of Ms. Lee's view, the deck/bathroom addition would obstruct the public's view of the Napa River from the sidewalk on B Street and from the public path that runs from Wilson Avenue to B Street along the north side of the Aquilina/Luck house.

The plans submitted by the applicant make it impossible for the Planning Commission to make the required findings set forth in the Vallejo Municipal Code, section 16.36.030; namely, that the project

- protects view corridors
- minimizes the appearance of visually intrusive structures
- prevents the obstruction of property owners' views by requiring appropriate construction of new structures or additions to existing buildings...
- Minimizes potential view loss from public access

City of Vallejo Planning Commissioners

May 15, 2007

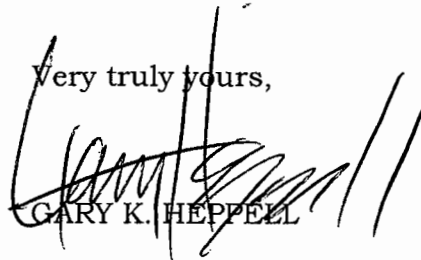
Page 3

- Incorporates reasonably available design options that eliminate or lessen view obstructions

Ms. Lee and I will attend the hearing on May 21 and present photographs showing the view obstruction. Meanwhile, we invite and encourage the Commissioners to come to Ms. Lee's home at 156 B Street and look at the storey poles, visualize the areas that would be obstructed and observe the effect the proposed addition would have on Ms. Lee's enjoyment of her property.

Thank you for your consideration.

Very truly yours,



GARY K. HEPPELL

GKH:k

cc: Sandra Lee
Katherine Donovan, Associate Planner

Planning Department
Katherine Donovan
555 Santa Clara St.
Vallejo, CA 94590

Dear Katherine,

I moved here to B street in Vallejo in the year of 2000, after being a resident of San Francisco for 17 years. I was fascinated with the beautiful community and the beauty of the water view looking out to the Mare Island bridge from my living room window. As a 20 year professional artist, this little treat inspires a great deal of my creativity.

When I saw the building plan that Charlie and his architect brought to my home, I was shocked because one third of my favorite view would be blocked. Charlie told me he might change the plan after he experienced my sympathetic emotional shaking. I inspected his plan and his idea of how he wants to expand his sq. footage of the building. There is no way to avoid blocking the view one way or another with his proposed plan. I am terribly concerned and worried about this project. I hope you will take into consideration my heart felt thoughts about my view and my concerns about this project.

With many thanks,
Sandra Lee



5/14/07

Planning Department
Katherine Donovan
555 Santa Clara St.
Vallejo, CA 94590

Dear Katherine,

I am writing another letter regarding 145 B Street per your request. However, I would like my original letter regarding this matter dated 3/18/05 included with this letter. This project has stalled for several years for reasons unknown to me. Many folks in my neighborhood have talked with the project applicants about their plans, and concerns about view obstruction. We have been assured that our views would not be obstructed, especially as our view has already been altered with the elevation change of the house. Now plans have stalled another two years and yet another proposal that disregards our concerns for view preservation has been submitted for your review.

The first renovation plans made renovation to the building footprint with a modest bump out out the back of the home only. The second proposal for renovation that was submitted two years ago adds an addition to the basement floor which ceiling height has increased due to the elevation change/new foundation. We would like to see the house renovated using the existing building footprint. This new proposal continues to add square footage to a building that has already been increased in size with the raising of the house. As I have said earlier we are not looking to have the house lowered to the original height, but we don't want to see any of the views taken away to increase building space when the building space has already been increased. We believe that an addition to the back of the house on the basement and 1st floors would not impact anyone's view.

The current proposal seeks to build out the back all levels and wrap around each side of the house. The top floor protrusion and both side protrusions take views away from my home, thus the applicants for 145 B should not be allowed to build on either side, and off the top floor of the home. The homeowners of 133 B altered their plans to only protrude from behind the residence on the basement and 1st floor level, when they became aware that their plans affected views. They were very mindful of the effect of their renovation on views from the street side! I feel that the raising/change of elevation of 145 B has created all of the difficulties in the scale of new construction on the home, thus making any addition to the home seem extremely large in scale and very tricky.

Please do consider the history of this project and how the current plans impact on our views. This particular plan also effects a broad range of people due to the fact that it is next to a public pathway and a grove of historic Deadora Cedar trees on the NE side of the home. A renovation with bump out as proposed earlier on basement and 1st floor would make for a very spacious addition now that area has extra ceiling height due to elevation change. Thank you for your kind consideration in this matter

Sincerely,

Kim Geddes

142 B Street
Vallejo, CA 94590

3/18/05

Planning Department
Katherine Donovan
555 Santa Clara St.
Vallejo, CA 94590



Dear Katherine,

We are happy that steps are finally being taken to remodel 145 B Street. However, we are opposed to the addition of a two story section on the side of the house. This project has morphed from what began as repairs to a foundation, to a three story house. In either case we were never notified. Please inform us of the procedures that took place to approve the foundation plans, and how it is that without any notice, we now are facing a house that has been raised? I have been told by the building/planning departments that the plans were never finalized and there was actually a stop order in place when the foundation was completed.

From street level, the roof height of the home is now taller than any existing home in Bay Terrace. Our view has been altered with the elevation change of the house. The proposed addition will affect both public and private views of Mt. Tamalpais, the bridge and the Napa River. It also affects the general public view corridor for people walking by or using it to access River Park. This area has been designated a view corridor in the Wilson Avenue improvement project. There is also a grove of historic Deadora Cedar trees right next to proposed site that would be gravely impacted. Preserving these historic trees was of paramount importance in our block renovation and numerous steps were taken to preserve this special place. Arborist John Britton was retained by the city to insure the health of the trees during this project.

Allowing a project like this to go forward could set a precedent for similar projects to go forward with no regard to the stunning views or the unique character of this historic neighborhood.

We don't want to see views impacted or obstructed. We would like to see the house renovated using the existing building footprint. This proposal adds square footage to a building that has already been increased in size with the raising of the house. We are not necessarily looking to have the house lowered to the original height, but we don't want to see any of the view corridor taken away to increase building space when the building space has already been increased.

Sincerely,

Kim Geddes
Kim Geddes

145 B Street



CONFLICT OF INTEREST MAP
(500-foot radius)

SD 05-0007
145 B Street; APN: 0051-132-090

**STAFF REPORT – PLANNING
CITY OF VALLEJO
PLANNING COMMISSION**

DATE OF MEETING: December 3, 2007
PREPARED BY: Geoff I. Bradley, Planning Consultant
PROJECT NUMBER: GPA07-0001, ZMA07-0002, TM07-0008, PD07-0007

PROJECT DESCRIPTION: The project consists of a 214-unit subdivision and a 4.1-acre neighborhood park on property previously developed with ball fields and drive-in theatre. The subdivision would have two housing types: 104 single-family homes on minimum 3,375 square foot lots and 110 paseo homes on minimum 2,359 square foot lots. The paseo homes would have garage access off alleys located at the rear of the lots, with landscaped pedestrian pathways (paseos) to the property frontages. The project requires approval of a General Plan Amendment, a Zoning Map Amendment, a Master Plan/Unit Plan, and a Vesting Tentative Map.

The General Plan Amendment, Zoning Map Amendment, and Master Plan/Unit Plan are reviewed by the Planning Commission and approved by City Council. The Vesting Tentative Map is approved by the Planning Commission.

RECOMMENDATION: Forward a recommendation of approval of the General Plan Amendment, the Zoning Map Amendment, and the Master Plan/Unit Plan to the City Council and approve Vesting Tentative Map with Conditions.

CEQA: Mitigated Negative Declaration with Monitoring Plan.

PROJECT DATA SUMMARY

Name of Applicant: KB Home South Bay, Inc.
Date of Application Completion: October 11, 2007
General Plan Designation: Existing: Retail/ Low Density Residential
Proposed: Low Density Residential
Zoning Designation: Existing: Public Facility & Pedestrian Commercial
Proposed: Planned Development Residential

Site: Northwest corner of Benicia Road and Rollingwood Avenue

Surrounding Land Uses:

North: Single-family residential
South: Cemetary
East: Cemetary, with single-family residential uses beyond.
West: Unincorporated (Solano County) Rural Residential

Lot Area:	31.8 Acres
Total Floor Area/Ratio:	471,781 S.F./34% Floor Area Ratio
Landscape Area/Coverage:	.3 acres in single-family portion/2.8 acres in paseo portion
Parking Required/Provided:	Single Family Homes – 208 required/416 proposed Paseo Homes – 242 required/284 provided

BACKGROUND SUMMARY

The project site is primarily vacant with the exception of the little league baseball fields that are owned by the Vallejo City Unified School District (VCUSD) and operated by East Vallejo Little League (EVLL). A former drive-in theatre is located in the area near the intersection of Rollingwood Drive and Benicia Road. A vacant residential structure is located near Benicia Road that was once the residence of the drive-in theatre caretaker. The structure is now abandoned and dilapidated. The remaining property is vacant. The project site contains four separate parcels owned by the VCUSD, City of Vallejo and Syufy Enterprises. KB Home is under contract to purchase the lands owned by the VCUSD and Syufy Enterprises. The City of Vallejo would trade the 4.3 city owned parcel near Rollingwood Drive for the proposed fully improved 4.1 public park.

ANALYSIS

Proposed Project

The applicant is requesting approval of an application for 214 housing units and a 4.1-acre park. The actions proposed include a General Plan Amendment for a portion of the property from Retail to Low Density Residential, Zoning Map Amendment from Public Facility and Pedestrian Commercial to Planned Development Residential, Master Plan/Unit Plan and Tentative Map for a 214 lot subdivision including a 4.1 acre public park.

The proposed southerly entrance road (Street 'G') from Rollingwood Avenue separates the project into two distinct neighborhoods: single-family homes and Paseo homes. The lower density (6.7 du/ac) single-family homes are located on 45 foot by 75 foot lots and home sizes range from 2,114 to 2,424 square feet. The proposed single-family neighborhood is located along the site's northern end, adjacent to existing single-family residences.

Paseo homes are two story homes with garages to the rear of the homes that are accessed by private alleys. The paseo home sizes range from 1,942 to 2,248 square feet. The higher density (9.1 du/ac) paseo homes are located adjacent to Benicia Road. Each neighborhood has its own unique design and character and unit types. The proposed park is centrally located towards the center of the site. The proposed land use summary is presented below.

Land Use Summary				
Land Use	Units	Acres	Density (du/ac)	% of Acreage
Single-Family (45'x75' lots)	104	15.6	6.7	49.1
Paseo Homes (with alleys)	110	12.1	9.1	38.0
Park	--	4.1	--	12.9
Project Total	214	31.8	6.7	100%

These modified development standards may be considered through the Planned Development Residential (PDR) zoning district. The purpose of the Planned Development Residential zoning is to provide for flexible development standards in consideration of public benefits. The provision of 214 units on 31.4 acres yields a density of 6.7 units per acre. Exclusive of the park, the density is 7.7 units per acre. The provision of for-sale housing at this density results in smaller individual parcels, more common open space and generally more affordable "ownership units" that allow homeownership and residents to build equity and be a part of the community. These public benefits may be considered when evaluating the proposed PDR zoning.

Existing Setting

The project site is located at the northwest corner of Benicia Road and Rollingwood Avenue in Vallejo. The 31.8 acre site contains four little league baseball fields and ancillary buildings at the north end of the property. The southern portion of the property is primarily vacant with the exception of the one-story structure that used to be the residence of the drive-in theatre caretaker. The existing terrain is varied and covered with patchy vegetation. Elevations range from 140' above sea level at the northwest corner of the site to a high point of 200' above sea level along Rollingwood Avenue. The site generally drains to the west. Benicia Road and Rollingwood Avenue are bordered by a row of trees.

Surrounding Setting

Existing land uses surrounding the property include a single-family housing development to the north, rural homes and businesses in the unincorporated area to the west and cemeteries to the south and east. The zoning districts for the surrounding properties are:

North: Low Density Residential
 South: Public and Quasi-Public Facilities
 East: Low Density Residential and Public and Quasi-Public Facilities
 West: Rural Residential (unincorporated Solano County)

Proposed Land Use

The current General Plan land use designation for a portion of the site is Retail (the remainder is designated Low Density Residential). The applicant is requesting a

change to Low Density Residential. The existing Zoning designation is Public Facilities (PF) and Pedestrian Commercial (CP) with a requested change to Planned Development Residential (PDR). The proposed General Plan and Zoning Amendments would result in a compatible land use pattern in this area due to the existing surrounding land uses. The area is developed primarily with residential and cemetery uses. The existing General Plan and Zoning Designations for retail uses on a portion of the site could result in potentially incompatible uses. The PDR zone would allow for maximum flexibility to provide for multiple unit types and site specific site planning.

Development Standards

Residential development regulations have been established to ensure that the proposed buildings and site plan achieve the desired character and development quality. The regulations set forth the minimum requirements necessary and are consistent with the proposed project. Refer to the City of Vallejo Zoning Ordinance for any regulations that are not specified below.

	Single-Family (45'x75')	Paseo Homes
Minimum Lot Area	3,375 s.f.	2,359 s.f.
Setbacks (minimum)		
Front	15' to building 10' to porch	9.5' to building 4' to porch
Rear	10'	4' to face of garage
Side	5'	4' to property line
		8' between buildings
Minimum Building Separation	10'	8'
Maximum Height	32'-1.5"	25'-7.5"
Maximum Lot Coverage	46.5%	65.2%
Accessory Structures	Allowed per requirements of the Low Density Residential District	Not Allowed
Permitted Uses	Allowed per requirements of the Low Density Residential District	Allowed per requirements of the Low Density Residential District
Parking Required	Two spaces per unit	Two spaces per unit Plus one guest space per 5 units (22 guest)
Parking Provided	Two garage spaces plus two space in the driveway per unit plus 166 total on-street spaces	Two garage spaces plus one guest space per 1.7 units (64 guest spaces)

These modified development standards may be considered through the Planned Development Residential (PDR) zoning district. The purpose of the Planned Development Residential zoning is to provide for flexible development standards in consideration of public benefits. The provision of 214 units on 31.4 acres yields a density of 6.7 units per acre. Exclusive of the park, the density is 7.7 units per acre. The provision of for-sale ownership housing at this density results in smaller individual parcels, more common open space and generally more affordable homes. Ownership units allow more opportunities for homeownership and for residents to build equity and be a more permanent part of the community. These public benefits may be considered when evaluating the proposed PDR zoning.

Project Architecture

The project has been designed in a variety of styles to be compatible with the existing residential development in the vicinity.

For the single-family portion of the project, there are three floor plans and three different elevations. The three proposed architectural styles are contemporary Tudor, contemporary Mission, and Craftsman style.

The level of architectural detailing of the project is similar to and compatible with the existing residential development in the vicinity.

The paseo homes feature three floor plans and three separate elevation designs. The architectural design consists of two different Craftsman style elevations and one contemporary Mission style.

Circulation

The project site is served by Benicia Road and Rollingwood Avenue. Benicia Road is a two-lane arterial road connecting Vallejo and Benicia. Rollingwood Avenue is a collector road that serves the residential areas to the north. The intersection of Benicia Road and Rollingwood Avenue is a signalized intersection. Both roads currently have unimproved frontages. Installation of full curb, gutter and sidewalk public improvements on the property frontage would be required as a condition of project approval.

Vehicle and pedestrian access to the site is available from Benicia Road and Rollingwood Avenue. Access to the project would be provided by two entrances along Rollingwood Avenue and one entrance along Benicia Road. All internal streets would provide convenient access to the public park.

The project as proposed includes a full public street connection from Hargus Avenue to the project site. This connection would provide a fourth point of access to the site and would promote an interconnected community with circulation to promote efficient delivery of public services including police, fire, and trash/recycling collection. A number of residents of the Hargus Avenue area have expressed opposition to this connection. The concerns have been focused on potential increases in vehicular traffic.

The single-family residences are served by public streets. The proposed street section is 36 feet curb-to-curb with parking on both sides of the street. This street section includes a detached 5-foot sidewalk with a 5-foot planter strip between the sidewalk and curb. This design allows for a pedestrian friendly streetscape with street trees placed close to the street for shade and maximum aesthetic value. The paseo homes are served by private 24-foot-wide alleys providing access to the garages at the rear of the homes.

Utilities

Water. The project would be served by the City of Vallejo water system. The current system is inadequate for the amount of development planned. Therefore, the project would be required to contribute to the overall system as well as to construct limited project-specific off-site improvements. The applicant will pay a fee to be used by the City towards the construction of an 18-inch water line from the Benicia/Rollingwood intersection to the Keats/Devlin intersection and/or the demolition of two existing 0.5 million gallon water tanks and the construction of the a new approximately 2.5 million gallon water tank. In addition, the applicant is required to install approximately 1,150 lineal feet of 12-inch water line along the project frontage in Benicia Road to a newly installed 12-inch water line that currently ends near the western property line. The existing 6-inch line in this location will be abandoned in place. Also, roughly 350 lineal feet of 12-inch water line will be in Hargus Avenue connection the existing system and abandoning and existing 6 inch line in place.

On-site improvements include a 12-inch water line from Hargus Avenue along the west and the south side of the park and connections to both Rollingwood Drive and Benicia Road. The remainder of the site will be served with 8-inch mains. The single-family homes will have standard water service and meters. The paseo homes will have a main running through the common driveways. The mains will have master meters and each paseo home will be sub-metered to be read by the homeowner's association.

Sanitary Sewer. Sewage will be collected and conveyed along standard 8 inch lines from the subdivision and directed to the existing system via Hargus Avenue.

Park

As part of the project, the City agreed to exchange a 4.3-acre City-owned parcel that was donated to the City to be used for recreational purposes for a 4.1-acre turnkey park at a different location within the subdivision. The City agreed to this exchange, in part, because the original property was awkwardly located, with site topography that made it difficult to use for recreational purposes. KB Home reimbursed the City for the 0.2-acre difference in the property at the same rate as was paid for the Vallejo Unified School District property.

The design of the proposed 4.1-acre public park has been reviewed by the Greater Vallejo Recreation District, which would lease the park from the City and be responsible for its maintenance. The park would be considered a Neighborhood Park, serving residentents in the immediate area. Amenities and uses within the park would include: baseball diamond with backstop, dug outs and bleacher seating, public restroom, group

picnic area, play area for children (tot lot), open grass area and half basketball court. Other features include a drinking table, picnic tables and covered seating areas. The northwest corner of the park also serves as a storm water detention facility. As a result the park is graded into a depression with depths ranging from five to 12 feet. The sloped portions have been designed as landscaped areas with walkways and also provide seating around the baseball diamond. A condition of approval has been included to continue to attempt to minimize the depth of the park relative to the surrounding streets.

Ballfields

The property owned by the Vallejo Unified School District contains four ballfields that were used through an agreement with by the East Vallejo Little League. As part of its efforts to stabilize its financial situation, the School district is selling off surplus land. One of the parcels that was included in the surplus land sale was the parcel that had previously been used by the East Vallejo Little League. The agreement regarding the use of the parcel expired prior to the sale of the property; however, as it had been used by the Little League for many years, there is strong sentiment in the community that equivalent playing fields should be provided as part of the project.

The Little League agreement with the School District has expired and neither the School District nor the project proponent have any legal obligation to provide property to the Little League. However, the proposed park would provide one regulation ballfield that could be used by the Little League through an agreement with GVRD and the School District has been negotiating with the East Vallejo Little League to provide three additional fields in the vicinity: two at Springstown Junior High School and one at Pennycook Elementary School. The school district properties would be available for two years and KB Home has agreed to build the fields for the Little League on the School District property.

Vesting Tentative Map

The proposed vesting tentative map would provide 214 residential parcels and one common lot for the paseo homes containing the access driveways, pocket parks, and paseos. This common area would be owned and maintained by the Homeowners Association.

The streets within the single-family portion of the project would be public streets, owned and maintained by the City of Vallejo. The parkstrips and roadway landscaping adjacent to Rollingwood Avenue and Benicia Road would be maintained by a proposed Landscape Maintenance District.

The vesting tentative map has been reviewed and found to be in conformance with the Subdivision Map Act and with the requirements of the Vallejo Municipal Code. The Vesting Tentative Map has been designed to include the street linkage of the proposed project and the existing neighborhood to the west (Hargus Avenue).

ENVIRONMENTAL DETERMINATION

Based on the Initial Study prepared by Design, Community and Environment, an environmental planning firm, for the City of Vallejo, it has been determined that with the mitigation measures proposed in the initial study, the proposed project would not have a significant effect on the environment. All recommended mitigation measures would be included in the conditions of approval with a mitigation monitoring plan (during the building permit stage).

An Initial Study and Mitigated Negative Declaration, including Mitigation Monitoring Program, were prepared for the project. Potentially significant impacts were determined in the areas of Air Quality, Biological Resources, Cultural Resources, Geology and Soils, Hazards/Hazardous Materials, Noise and Transportation/Traffic. In all cases, mitigation measures were included that would reduce the potentially significant impacts to a less-than-significant level.

Air Quality. Construction-related air quality impacts are normal for a project of any significant size. Standard mitigation measures provided by the Bay Area Air Quality Management District (BAAQMD) can reduce these impacts to a less-than-significant level. These mitigation measures are generally aimed at reducing particulate matter that is normally released during construction. These measures include wetting down soil and sweeping and removing dirt from paved roadways at regular intervals.

Biological Resources. The biological resources on-site were evaluated including plants, trees, habitat and wildlife. A number of mitigation measures were developed to ensure there would be no impact in the areas of sensitive plant and animal species, wetlands, and raptors. Focused surveys prior to project construction would provide additional information to confirm the absence of the sensitive plant and animal species and raptors, none of which have been identified on the site. The project is required to prepare a Wetland Mitigation Program subject to Army Corps and/or RWQCB jurisdiction.

Cultural Resources. Whenever a project includes grading or digging, there is a chance that unsuspected historical, archaeological, or paleontological resource or human remains may be discovered. In the event that such resources or human remains are discovered during any phase of the project, land alteration work in the vicinity shall be halted and a qualified professional consulted to evaluate the resource and determine an appropriate management plan. If human remains are discovered, the County Coroner shall be notified. If the Coroner determines that the remains are of Native American decent, the Coroner shall contact the Native American Heritage Commission within 24 hours of the determination.

Geology and Soils. The Bay Area is a seismically active area. Construction within seismically active areas has the potential of exposing people and structures to seismic-related hazards. Construction techniques appropriate to the Seismic Zone and project-specific engineering techniques would be required to reduce potential impacts to a less-than-significant level.

Hazards/Hazardous Materials. The Department of Toxic Substances Control (DTSC) database records indicate that the site sampled for lead, arsenic and pesticides. Relatively minimal concentrations of lead and pesticides are indicated, but arsenic levels in excess of residential Environmental Screening Levels are documented. The arsenic is from previous use as a weed control application. Clean up to DTSC standards is required, if elevated levels of hazardous materials are confirmed by additional soil sampling. Before soil can be exported off-site it shall be profiled for landfill disposal requirements.

Noise. Mitigation measures require the installation of sound walls or equivalent in several locations along Rollingwood Drive to ensure that the outside yard areas meet the 60dBA Ldn level. These sound walls need to be six feet high in some locations and eight feet high along the rear of seven parcels along Rollingwood Avenue. This mitigation measure can be satisfied with a specially designed wood fence rather than a concrete wall.

Transportation/Traffic. The traffic analysis for the proposed project considered the project-related traffic as well as cumulative traffic until the year 2020, which anticipates traffic to occur with buildout of the City's General Plan. The traffic analysis concluded that there were no potentially significant off-site traffic impacts from the proposed project. An on-site mitigation measure was identified to include adequate turnaround space for fire trucks and garbage/recycling trucks within the paseo portion of the development, if required by the Fire Department and Vallejo Garbage Service.

Public Comments

The applicant hosted three neighborhood meetings on the project; on March 29, 2007, May 14, 2007, and July 30, 2007 as well as a presentation to the Glen Cove Homeowners Association. The meetings were well attended with 30 to 50 people attending each meeting. Issues of concern included the Little League fields, traffic, and drainage. Staff also received several letters, emails, and telephone calls related to the same issues.

CONCLUSION/RECOMMENDATION

Staff has determined that the proposed project, as conditioned, is consistent with the City's General Plan and Municipal Code, and all applicable ordinances, standards, guidelines, and policies. Therefore, staff recommends that the Planning Commission forward a recommendation of approval to the City Council on the General Plan Amendment, the Zoning Map Amendment, and the Master Plan/Unit Plan (GPA07-0001, ZMA07-0008, and PD 07-0007). Staff also recommends that the Planning Commission approve the Vesting Tentative Map (TM 07-0008) based on the findings and subject to the attached Conditions of Approval.

ATTACHMENTS

1. Zoning Map Amendment Resolution (Petition)
2. General Plan, Tentative Map, and Master Plan/Unit Plan Resolution
3. Project plans
4. Photographs of Existing Site
5. Master Plan/Project Description
6. Initial Study/Mitigated Negative Declaration
7. Emailed letter from Fernando Maldonado
- 7.a Letter from Anthony Intintoli, Mayor dated 10/31/07
8. Letter from Vallejo City Unified School District
9. Conflict of Interest Map
10. Driving directions

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC-07-

**RESOLUTION OF INTENTION
TO AMEND THE ZONING ORDINANCE
Zoning Map Amendment
ZMA 07-0002**

**Rollingwood Project
Benicia Road and Rollingwood Avenue, Vallejo
APN: 0072-170-050, 0072-170-060, 0072-170-200, 0072-170-210**

I. GENERAL FINDINGS

WHEREAS an application was filed by KB Home South Bay, Inc. seeking approval for a General Plan Amendment, zone change, tentative map and planned development permit to allow the residential uses and park space.

WHEREAS the City of Vallejo Planning Commission conducted a duly noticed public hearing to consider the application for the related permits and approvals on December 3, 2007, at which testimony and evidence, both written and oral, was presented to and considered by the Planning Commission.

WHEREAS based on evidence received at the public hearing, the Planning Commission makes the following factual findings:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS.

Section 1. An Initial Study and Notice of Intent to Adopt a Mitigated Negative Declaration were prepared by the City and made available to the public for review on October 11, 2007. The Notice of Intent to Adopt the Mitigated Negative Declaration was duly sent, posted, and available for public review.

Section 2. The Planning Commission finds that the Initial Study identifies potentially significant impacts for which the project’s proponent has made or agrees to make project revisions that clearly mitigate the impacts, and the revised project, with mitigation, will not have a significant environmental impact.

Section 3. The planning commission hereby makes these findings based on the whole record, including the environmental findings contained in Section 2 “*Environmental*

Review” of the staff report attached hereto as "Exhibit 1" and incorporated herein by this reference.

III. FINDINGS RELEVANT TO REZONING, PROJECT APPROVAL AND FOR DETERMINATION OF PROJECT CONSISTENCY WITH APPLICABLE GENERAL PLAN

Section 4. The Planning Commission finds that applicant submitted an application for an Amendment to the City of Vallejo’s Zoning Ordinance, in that the applicant seeks a change to the zoning map to allow for a 214-unit subdivision and a 4.1 acre neighborhood park on property previously developed with ball fields and drive-in theatre. The subdivision would have two housing types: 104 single-family homes on a minimum 3,375 square foot lots and 110 paseo homes on a minimum 2,359 square foot lots. The paseo homes would have garage access off alleys located at the rear of the lots, with landscaped pedestrian pathways (paseos) to the property frontages. A landscape maintenance district would be created for the main frontages at Rollingwood Drive and Benicia Road.

Section 5. The Planning Commission finds that amendments to the City of Vallejo’s Zoning Ordinance are subject to the procedures contained in Chapter 16.86 of the City of Vallejo Municipal Code, and that such amendment may be initiated by a Resolution of Intention of the Planning Commission pursuant to Vallejo Municipal Code section 16.86.030.

Section 6. The Planning Commission finds that the amendment is consistent with the General Plan, municipal code, and all applicable ordinances, standards, guidelines and policies, zoning regulations and subdivision regulations in that,

1. The General Plan Land Use Element designates the property as Low Density Residential. The proposed rezoning of the four parcels to Planned Development Residential is compatible with the General Plan Designation and the existing surrounding land uses.
2. The proposed density of 6.7 dwelling units per acre is consistent with the General Plan allowed density of 0.5 to 8.7 dwelling units per acre.
3. The proposed lot sizes are 2,359 to 3,375 square feet, which are allowed by approval of the related Planned Development Permit.

IV. RESOLUTION RECOMMENDING AMENDMENT TO THE ZONING ORDINANCE FOR ROLLINGWOOD PROJECT

NOW, THEREFORE, LET IT BE RESOLVED that the Planning Commission hereby APPROVES this resolution of intent to amend the zoning ordinance by adopting Zoning Amendment (ZMA 07-0002) for the Rollingwood project, based on the findings contained in this resolution, with reference to the staff report attached hereto and incorporated herein as “Exhibit 1” and subject to the Conditions of Approval contained in

"Exhibit 2" attached hereto and incorporated herein by reference. The monitoring or reporting program included in the Mitigated Negative Declaration is hereby adopted.

V. VOTE

PASSED and APPROVED at a regular meeting of the Planning Commission of the City of Vallejo, State of California, on the 3rd day of December, 2007, by the following vote to-wit:

AYES:

NOES:

ABSENT:

Charles Legalos, Chairperson
City of Vallejo Planning Commission

Attest:

Don Hazen
Planning Commission Secretary

CITY OF VALLEJO PLANNING COMMISSION

RESOLUTION NO. PC-07-_____

**A RESOLUTION OF THE PLANNING COMMISSION
APPROVING A VESTING TENTATIVE MAP APPLICATION
(TM 07-0008)**

and

**RECOMMENDING CITY COUNCIL APPROVE
A GENERAL PLAN AMENDMENT
(GPA 07-0001)**

and

**A MASTER PLAN/UNIT PLAN
(PD 07-0007)**

for

**Rollingwood Project – KB Home
Northwest corner of Rollingwood Drive and Benicia Road
APNs 0072-170-050, -060, -180, -200 and -210**

I. GENERAL FINDINGS

WHEREAS an application was filed by KB Home South Bay, Inc. seeking approval for a Master Plan/Unit Plan and Vesting Tentative Map to allow the development of a 214-unit single family residential project of five existing parcels,

WHEREAS the City of Vallejo Planning Commission conducted a duly noticed public hearing to consider the application for the Master Plan/Unit Plan, Vesting Tentative Map, on November 19, 2007, at which testimony and evidence, both written and oral, was presented to and considered by the Planning Commission,

WHEREAS based on evidence received at the public hearing, the Planning Commission makes the following factual findings:

II. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS.

Section 1. An Initial Study and Notice of Intent to Adopt a Mitigated Negative Declaration were prepared by Design, Community & Environment for the City and made available to the public for review on October 11, 2007. The Notice of Intent to Adopt the Mitigated Negative Declaration was duly sent, posted, and available for public review.

Section 2. The Planning Commission finds that the Initial Study identifies potentially significant effects for which the project's proponent agrees to make project revisions and follow mitigation procedures that clearly mitigate the effects, and the revised project, with mitigation, will not have a significant environmental effect.

Section 3. The Planning Commission hereby makes these findings based on the whole record, including the environmental findings contained in Section 7 "*Environmental Determination*" of the staff report attached hereto and incorporated herein by this reference.

III. FINDINGS RELEVANT TO VESTING TENTATIVE MAP APPROVAL

Section 4. The Planning Commission finds that the applicant submitted an application for a Vesting Tentative Map, which is required for the 214-unit single family development project. Vesting Tentative Map approval is governed by Chapter 15.08 of City of Vallejo Municipal Code.

Section 5. The Planning Commission finds, based on the facts contained in the staff report attached hereto and incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that:

1. As conditioned, the tentative map is consistent with the goals and policies of the Vallejo General Plan.
2. The design and improvements of the proposed subdivision are in conformance with Title 15 "Subdivisions" and Title 16 "Zoning Ordinance" of the Vallejo Municipal Code.
3. The vesting tentative map is in conformance with the Subdivision Map Act.
4. The site is physically suitable for the proposed density of development.
5. The design of the subdivision and proposed improvements would not cause significant environmental damage or substantially injure fish or wildlife or their habitat.
6. The design of the subdivision and improvements will not cause serious public health problems.
7. The design of the subdivision and improvements would not conflict with easements of record, or established by a court of competent jurisdiction acquired by the public at large, for access through or use of property within the proposed subdivision.
8. The subdivision and improvements are no proposed for lands subject to the Williamson Act and are consistent with the requirements of Section 66474.4 of the Government Code.

IV. FINDINGS RELEVANT TO RECOMMENDATION OF APPROVAL OF GENERAL PLAN AMENDMENT

Section 6. The Planning Commission finds that the applicant submitted an application for a General Plan Amendment, which is required for the 214-unit single family development project. General Plan Amendment approval is governed by Chapter 17.04 of City of Vallejo Municipal Code.

Section 7. The Planning Commission finds, based on the facts contained in the staff report attached hereto and incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that the proposed General Plan Amendment is internally consistent with the Vallejo General Plan and that it is in the public interest to approve the proposed amendment.

V. FINDINGS RELEVANT TO RECOMMENDATION OF APPROVAL OF MASTER PLAN/UNIT PLAN

Section 8. The Planning Commission finds that the applicant submitted an application for a Master Plan/Unit Plan, which is required for the 214-unit single family development project. Master Plan/Unit Plan approval is governed by Chapter 16.116 of City of Vallejo Municipal Code.

Section 9. The Planning Commission finds, based on the facts contained in the staff report attached hereto and incorporated herein by this reference, and given the evidence presented at the public hearing, and subject to the conditions attached to this resolution, that:

1. The master plan/unit plan is consistent with the goals and policies of the Vallejo General Plan.
2. The master plan/unit plan furthers the stated purpose of the planned development district.
3. The master plan/unit plan is in conformity with public convenience, the general welfare, and good land use practice.
4. The master plan/unit plan will not be detrimental to health, safety, and general welfare.
5. The master plan/unit plan will not adversely affect the orderly development or the preservation of property values.
6. The unit plan is consistent with the intent, purpose, and development standards of the master plan.
7. The unit plan provides six floor plans with three elevations each that are well-related to each other and, taken together, will result in a well-composed urban design, with consideration given to site, height, arrangement, texture, material, color, and appurtenances, the relation of these factors to other structures in the immediate area, and the relation of the development to the total setting as seen from key points in the surrounding area.
8. The unit plan is of a quality and character that harmonizes with, and serves to protect the values of, private and public investments in the area.

VI. RESOLUTION RECOMMENDING APPROVAL OF THE VESTING TENTATIVE MAP FOR THE ROLLINGWOOD PROJECT – KB HOME, SUBJECT TO THE CONDITIONS OF APPROVAL

NOW, THEREFORE, LET IT BE RESOLVED that the Planning Commission hereby APPROVES the Vesting Tentative Map and (TM 07-0008) for the Rollingwood Project, based on the findings contained in the staff report attached hereto and incorporated herein and subject

to the Conditions of Approval (EXHIBIT A) attached hereto and incorporated herein by reference.

VII. RESOLUTION FORWARDING A RECOMMENATION OF APPROVAL OF THE GENERAL PLAN AMENDMENT FOR THE ROLLINGWOOD PROJECT

LET IT FURTHER BE RESOLVED that the Planning Commission hereby recommends that the City Council APPROVE the General Plan Amendment (GPA 07-0001) for the Rollingwood Project, based on the findings contained in the staff report attached hereto and incorporated herein and subject to the Conditions of Approval attached hereto and incorporated herein by reference.

VIII. RESOLUTION FORWARDING A RECOMMENATION OF APPROVAL OF THE MASTER PLAN/UNIT PLAN FOR THE ROLLINGWOOD PROJECT, SUBJECT TO CONDITIONS OF APPROVAL

LET IT FURTHER BE RESOLVED that the Planning Commission hereby recommends that the City Council APPROVES the Master Plan/Unit Plan application (PD 07-0007) for the Rollingwood Project, based on the findings contained in the staff report attached hereto and incorporated herein and subject to the Conditions of Approval attached hereto and incorporated herein by reference.

VI. VOTE

PASSED and APPROVED at a regular meeting of the Planning Commission of the City of Vallejo, State of California, on the 3rd day of December, 2007, by the following vote, to-wit:

AYES:

NOES:

ABSENT:

Charles Legalos, Chairperson
City of Vallejo Planning Commission

Attest:

Don Hazen
Planning Commission Secretary

PLANNING DIVISION**THE FOLLOWING STANDARD COMMENTS/CONDITIONS APPLICABLE TO THE SUBJECT ARE SO MARKED:**

1. **CONSTRUCTION TRAILER.** Prior to building permit issuance, obtain administrative permit from the **Planning Division** for any temporary office or construction trailer per Chapter 16.58.090 (VMC).
2. **SCHOOL FEES.** Prior to building permit issuance for residential construction, obtain written certification from the School District (or show cause why the mitigation proposed by the District should not be required) that the applicant has mitigated the school-related impacts of this project satisfactory to the District per Chapter 3.14 (VMC). The methods under which the school-related impacts are to be mitigated include, but are not limited to, those methods set forth in the Government Code. This condition shall not be constructed as a limitation on the District's choice of legal mitigation alternatives.
3. **OBTAIN SIGN PERMIT.** Prior to occupancy/final building inspection, obtain a sign permit from the **Planning Division** prior to the erection of any sign, including flags, banners, etc. All signs shall comply with Chapter 16.64 (VMC)
4. **OUTDOOR STORAGE.** There shall be no outdoor storage or display of any kind except as allowed per Chapters 16.70 and 16.77 (VMC)
5. **SCREENING OF UTILITIES.** All mechanical equipment and utility meters shall be screened in a manner approved by the **Planning Division**. Electrical transformers shall be screened or placed underground. Chapter 16.75.030 D (VMC)
6. **SCREENING OF ROOF EQUIPMENT.** All roof-mounted mechanical devices and their components such as air conditioners, heating equipment, exhaust fans, vents or ducts or similar equipment shall be screened from view in a manner approved by the **Planning Division**. All wall-mounted air conditioners shall be flush-mounted. Chapter 16.75.0530 D (VMC).
7. **TRASH ENCLOSURE.** Prior to building permit issuance, submit details of trash enclosure to the **Planning Division** for review and approval. Trash enclosure shall meet the requirements of the Vallejo Garbage Service. Materials and colors shall be similar to those approved for the building. Chapter 16.75.030 C (VMC)
8. **LANDSCAPING.** Landscaping plans shall comply with Chapter 16.70 (VMC) for the zone in which the project is located and shall include the provisions listed in the Landscape and Irrigation condition of the Standard Conditions.
9. **MAINTENANCE OF LANDSCAPING.** Required landscaping shall be maintained in a neat, clean and healthy condition per Chapter 16.70.090 (VMC). This shall include pruning, mowing of lawns, weeding, removal of litter, fertilizing, replacement of plants when necessary, and the regular watering of all plantings.
10. **REVOCATION.** If the Planning Division, either independently or as a result of complaints from the public, becomes aware that this use is being conducted in a manner which violates the conditions of this use permit or other applicable City regulations, and the Planning staff is unable to obtain compliance or abatement, staff will refer the use permit to the Planning Commission for possible suspension or revocation per Section 16.82.110, Vallejo Municipal Code.

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED:

11. **HOW PROJECT CONDITIONS SATISFIED.** Prior to building permit issuance, submit a numbered list to the **Planning Division** stating how each condition of project approval contained in this report will be satisfied. The list should be submitted to the project planner who will coordinate development of the project.
12. **COLORS AND MATERIALS.** Prior to building permit issuance, the applicant shall submit a color board indicating exterior materials and colors to the **Planning Division** for review and approval. Color chips shall be attached to the building elevation drawings. Once installed, all improvements are to be maintained in accordance with the approved plans. Any changes which affect the exterior character shall be resubmitted to the Planning Division for approval.
 - a. Provide details of proposed exterior trim for both the single family homes and paseo homes. Shutters shall be solid material other than stucco over foam.
13. **LANDSCAPING AND IRRIGATION.** Prior to building permit issuance, submit 3 sets of landscaping plans prepared by a registered landscape architect to the Planning Division for review and approval. The requirement for a registered landscape architect may be waived at the discretion of the Planning Manager. Landscape plans shall comply with Chapter 16.70 (VMC), and are to include the following:
 - a. Location, species and size of all mature trees six inches in trunk diameter or greater;
 - b. Replacement of any mature trees to be removed;
 - c. City-approved street trees to be planted at least 6 feet from any sewer line;
 - d. Specification of low growth type species adjacent to doors, windows and walkways;
 - e. Low-water using and drought-resistant plant materials;
 - f. Screening of required backflow preventer;
 - g. All trees to be minimum of 15 gallon, double staked; at least 50% of the proposed shrubs shall be a minimum of 5 gallon;
 - h. Irrigation plan indicating all components of the irrigation system including sprinklers and other outlets, valves, backflow prevention devices, controllers, piping and water usage; and
 - i. 6 inch high curbing around planters
 - j. Prior to occupancy/final building inspection install landscaping and irrigation per approved plans. The landscape architect shall verify in writing that the landscaping and irrigation have been installed in accordance with the approved landscaped plans with respect to size, health, number and species of plants and the overall design concept.
14. **PARK.** Complete Park improvements prior to occupancy or final inspection of the first unit, other than model homes. Consider to explore opportunities to decrease the depth of the public park relative to surrounding street level subject to review and approval of Public Works Director prior to issuance of Building Permits.
15. **FENCING.** Prior to building permit issuance, submit details and location of any proposed fencing to the **Planning Division** for review and approval. Fencing shall comply with Chapter 16.70 (VMC).
16. **LIGHTING PLAN.** Prior to building permit issuance, submit site lighting plan to the Planning Division for review and approval. Exterior illumination shall be provided by lighting fixtures utilizing high pressure sodium vapor (HPS) or metal halide lamps, or their equivalent. All pole mounted or raised fixtures shall be a minimum of 8 feet in height from the ground to the lower base of the light fixture. The fixture housing shall be constructed so

that the light is diffused downward. All light devices shall be protected by weather and vandal resistant covers.

17. **VENTS, GUTTERS, FLASHINGS.** All vents, gutters, downspouts, flashings, electrical conduits, etc., shall be painted to match the color of the adjacent surface.
18. **INSTALL PARKING.** Prior to occupancy/final building inspection, install parking lot per approved plans. Each parking space designated for compact cars shall be identified by a permanent marking reading "compact".
19. **CONFORMITY WITH APPROVED PLANS.** The building techniques, materials, elevations, and appearance of the project as presented for approval at the appropriate City meeting (Planning Commission, City Council, etc.) or staff decision on (date) which are prepared by the firm that prepared the plans, on (date), shall be the same as required for submittal of a building permit. Minor modifications or revisions to the project shall be subject to review and approval by the Planning Division. Modifications not deemed minor by the Planning Manager shall require review and approval by the original decision-making body.
20. **PLANNING DIVISION INSPECTION.** Obtain an inspection form from the Planning Division prior to occupancy/final building inspection. All inspections require a minimum 24 hour notice. Occupancy permits shall not be granted until all construction and landscaping is completed and finalized in accordance with the approved plans and required conditions of approval, or a bond has been posted to cover all costs of the unfinished work as agreed to by the Planning Manager.
21. **CONSTRUCTION HOURS.** Construction-related activities shall be limited to between the hours of 7 a.m. and 6 p.m., Monday through Saturday. No construction is to occur on Sunday or federal holidays. Construction equipment noise levels shall not exceed the City's maximum allowable noise levels.
22. **CONDITIONS RUN WITH PROPERTY.** The conditions herein contained shall run with the property and shall be binding on the applicant, and all heirs, executors, administrators, and successors in interest to the real property that is the subject of this approval.
23. **INDEMNIFICATION.** The applicant shall defend, indemnify, and hold harmless the City of Vallejo or its agents, officers, and employees from any claim, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void, or annul, this approval by the City. The City may elect, in its discretion, to participate in the defense of any action.

FIRE PREVENTION DIVISION**THE FOLLOWING STANDARD COMMENTS/CONDITIONS APPLICABLE TO THE SUBJECT ARE SO MARKED:**

24. **HOW FIRE PREVENTION CONDITIONS SATISFIED.** Submit a numbered list to the Fire Prevention Division stating how each condition of project approval will be satisfied.
25. **FIRE CODE CONFORMANCE.** The project shall conform to all applicable requirements of Title 19-Public Safety, 2001 CFC and all VMC Amendments.
26. **WATER SUPPLY.** Prior to the building permit issuance, the applicant shall install an approved and tested water supply system capable of supplying the required fire flow as determined by the Fire Chief. Water supply systems for staged construction shall provide required fire flows at all stages. (2001 CFC Section 903, Appendix III-A)
27. **ADDRESSING.** Prior to occupancy/final building inspection, install approved numbers or addresses on all buildings in such a position as to be clearly visible and legible from the street. Residential buildings shall have numerals or letters not less than 3 inches in heights, and approved color that contrasts the background. Commercial occupancies shall have numerals or letters not less than 6 inches in height of contrasting background, and illuminated at night. (2001 CFC Section 901.4.4; added VMC Section 12.28.170)
28. **WEED FREE PROPERTY.** Development sites shall be maintained weed free during construction. (2001 CFC Section 1103.2.4)

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED:

29. Additional fire hydrant(s) is/are required. Hydrants are required in the street right-of-way and hydrants are required on-site as shown in the Fire Department copy of the plans. Submit a complete set of plans for review and approval. All fire hydrants are to have "blue dot" highway reflectors installed on the adjacent street of the driveway to clearly identify the fire hydrant locations. (2001 CFC Section 903, Appendix III-B)
30. In residential (Group R) Occupancies, single station smoke detectors shall be installed prior to occupancy/final building inspection in each sleeping area and at a point centrally located in the corridor or area giving access to each separate sleeping area. When the dwelling unit is of more than one story (including basements), there shall be a smoke detector on each story. When a story is split, into more than one level, the smoke detector shall be installed on the upper level. (2001 CBC Section 310.9.1.1)
31. Every sleeping room below the fourth story shall have at least one exterior opening for rescue purposes. The opening shall be a minimum of 5.7 square feet and 20 inches wide by 24 inches high. The finished sill height of the opening shall be no higher than 44 inches from the floor. Ladder access provided for building over the first floor. (2001 CBC Section 310.4)

PUBLIC WORKS DEPARTMENT

32. Submit a comprehensive geotechnical investigation report for this project for review. City of Vallejo may decide that an independent geotechnical consultant to review the soils report and cost of this review will be born by the developer.
33. Submit grading, drainage, erosion control, improvement, utility, joint trench, street light, signing & striping and landscaping plan for review and approval.
34. Surface runoff from the subdivision shall be intercepted, piped and tied into public storm drain system.
35. Rollingwood Drive improvements includes but not limited to pavement widening, grading, drainage, curb, gutter sidewalk, street lights and under-grounding overhead utility wires.
 - a. The submittal does not provide limit of widening and signing & striping along Rollingwood Drive.
 - b. The traffic study does not provide any proposed signal or stop sign or left turn pocket for proposed "A" & "G" street intersections.
 - c. The proposed sidewalk along Rollingwood Drive should have three feet flat bench behind sidewalk.
 - d. The plan does not show "NO Parking" sign along Rollingwood Drive.

To satisfy the above comments there may be a need for re-design of submitted plans.

36. Benicia Road full improvements from intersection of Rollingwood Drive up to west property line of project includes but not limited to widening, grading, drainage, curb, gutter, sidewalk, left turn pocket, street lights, under-grounding over head utility wires, bus bay and bus shelter pad.
 - a. The submittal does not provide limit of widening and signing & striping along Benicia Road.
 - b. The traffic study does not provide any proposed signal and stop sign along Benicia Road.
 - c. The plan does not show location of bus bay and bus shelter pad along Benicia Road.

To satisfy the above comments there may be a need for re-design of submittal plans.

37. Proposed perpendicular parking areas within parcels "H", "W" and "Z" shall have five feet buffer between access driveway and parking stalls. (Traffic Engineer)
38. Provide turnaround template at the end of alleys for review. (Traffic Engineer)
39. Driveway approach location serving lot one shall be on far side of the lot to meet curve sight distance requirements and proximity of driveway to intersection. (Traffic Engineer)
40. Construct 100 feet westbound right turn lane for the project main entrance (Street "D") with appropriate taper along Benicia Road to improve safety. (Traffic Engineer)
41. Construct 75 feet eastbound left turn lane on Benicia Road and modify existing striping to coincide with change. (Traffic Engineer)

42. Submit a detailed signing and striping plan for Rollingwood Drive and Benicia Road for review and approval.
43. Make sure the corner buildings at the intersection of Rollingwood Drive and proposed "A" and "G" streets have appropriate setback to provide adequate sight distance. (Traffic Engineer)
44. Designate required public utility easement area between buildings and back of curb for utility facilities on both sides of alley homes area. Provide a detail showing all utility facilities/boxes will fit within easement area.
45. Establish private reciprocal drainage easement behind lots 58 -64 and 83-111 for the backyard surface drainage terminates into common v-ditch. V-ditch shall be maintained by respective lot owners. This responsibility shall be addressed in the CC&R.
46. Install standard handicap ramp at each curb return.
47. Prior to building permit all existing easements crossing proposed lots shall be quitclaimed.
48. Additional right of way up to back of proposed meandering sidewalk along Rollingwood Drive shall be dedicated to the City of Vallejo.
49. Install "NO Parking" sign along Rollingwood Drive fronting the project.
50. All proposed intersections in the project shall have standard stop sign.
51. The project does not specify who will maintain the landscaping fronting Benicia Road, Rollingwood Drive and five feet interior park strip along sidewalks. The applicant shall provide a mechanism to maintain these areas.
52. Street lights within alleys shall be maintained by Homeowners Association.
53. Proposed cal de sac "C" bulb has seven driveway approaches. It appears too tight to accommodate all utility facilities, street trees and street lights. Provide a detail showing how it works.
54. Surface drainage of this project shall not adversely affect the neighboring properties.
55. Prior to recording the final or parcel map, the owner shall pay the City charges required by Solano County for providing copies of the recorded map to the City (\$15.00/sheet).
56. Submit street names for proposed public streets and private alleys to **Public Works Department** for review and approval by Fire, Police, and Public Works departments and U.S. Postal Service.
57. Concurrent to submittal of Final Map, submit CC&R for standard lots that have access through proposed public streets and for paseo homes that have access through alleys for review.
58. Vertical and horizontal alignment of all streets shall conform to City of Vallejo standards.
59. Prior to building permit issuance, the developer shall file with the City of Vallejo, pursuant to the State of California Vehicle Code, a petition requesting that the provisions of said Vehicle Code be applied to private Alleys in the subdivision and post and maintain signs at the entrances to the subdivision giving notice of enforcement of said Vehicle Code. This activity shall be coordinated with the Fire Department.

60. Prior to occupancy of first lot within the proposed Alleys install Vehicle Code sign at the entrance of each Alley per City standard and install "No Parking" sign along the Alleys.
61. Prior to building permit issuance for the lots associated with the Alleys, establish a Homeowners Association for operation and maintenance of improvements, irrigation system, drainage ditches, fences, masonry walls, private Alleys, signage and hardware, private street light system, private open spaces and other private facilities subject to the approval of the Planning Division, Public Works Director, and the City Attorney. The Covenants, Conditions and Restrictions of all deeds issued for lots have access to the Alleys shall contain provisions requiring participation in the said Homeowners Association. The Homeowners Association must accept the above private elements of subdivision improvements prior to acceptance.
62. If the subject property is within an assessment district, the subdivider shall provide apportionment of the assessment resulting from the subdivision prior to approval of the final or parcel map.
63. During construction, it shall be the responsibility of the contractor to provide for safe traffic control in and around the site. This may include but not be limited to signs, flashing lights, barricades and flag persons.
64. If any lot is to be re-graded after mass grading is completed, a new grading permit is required. Review of the proposed grading by a soils engineer and field testing of the grading shall be required.
65. Prior to occupancy of any unit, underground overhead utility wires along Rollingwood Drive and Benicia Road fronting the property shall be completed.
66. Once subdivision improvements are accepted by the City as public rights-of-way, they are not to be used for staging building construction activities, including but not limited to, storage of construction material and equipment. The street and sidewalks must be kept free of construction debris, mud, and other obstacles and must remain open to traffic at all times.
67. Prior to acceptance of the project, the landscape architect for the project must perform a complete and thorough field review of the landscape irrigation and planting within the project and provide the City in writing a certificate that all landscaping, planting, and irrigation within the project is in full compliance with the City ordinances and guidelines and approved landscape, planting and irrigation plans.
 - a. Alley's parcels "X", "J", "H" & "G" shows 22 feet width, demonstrate how garbage bins can be accommodated and collected.
 - b. The submittal does not show location of proposed mail boxes. Mailbox locations, if clustered, shall not impact flow of traffic.
 - c. Within paseo parcels "EE", "FF" & "GG" proposed pedestrian accesses crossing the alleys. Demonstrate that this situation be safe for pedestrians.

THE FOLLOWING STANDARD COMMENTS/CONDITIONS APPLICABLE TO THE SUBJECT ARE SO MARKED:

Note: The following are City of Vallejo Code & Standard requirements, deviation from these requirements requires City Council approval. All Code and Standard exceptions shall be identified on the Tentative Map.

68. Submit a final map prepared by a qualified registered civil engineer or Land Surveyor for review and approval. Submit preliminary title report and all pertinent documents for map review. (VMC 15.12. 030).
69. Prior to occupancy provide six inches of topsoil on the lot. (VMC Section 12.40.070-G).
70. Install standard driveway approach per City standard. (COV, Regulations & Standard Specifications,1992).
71. Maintain five feet set back from the property line to the beginning of driveway approach flare fronting the property.
72. (COV, Regulations & Standard Specifications,1992).
73. Provide 3 feet flat area behind the sidewalk along proposed public streets. (COV, Regulations & Standard Specifications,1992).
74. Signing and striping shall be per City standard. (VMC, Section 16.62.140)

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED:

75. **HOW PROJECT CONDITIONS SATISFIED.** Prior to building permit issuance, submit a numbered list to the **Planning Division** stating how each condition of project approval contained in this report will be satisfied. The list should be submitted to the project planner who will coordinate development of the project.
76. **PUBLIC IMPROVEMENT STANDARDS.** All public improvements shall be designed to City of Vallejo standards and to accepted engineering design standards. The **City Engineer** has all such standards on file and the Engineer's decision shall be final regarding the specific standards that shall apply. (COV, Regulations & Standard Specifications,1992).
77. **IMPROVEMENT PLANS.** Prior to building permit submittals, submit three sets of plans to the **Department of Public Works** for plan check review and approval. (Improvement or civil plans are to be prepared by a licensed civil engineer.) Plans are to include, but may not be limited to, grading and erosion control plans, improvement plans, joint trench utility, street light plans, and landscaping, irrigation and fencing plans and all supporting documentation, calculations and pertinent reports. (COV, Regulations & Standard Specifications,1992 Section 1.1.7-A).
78. **GRADING** Prior to issuance of grading permit, submit a soils report for review. An independent soils and geological review of the project may be required. The City shall select the soils engineer with the cost of the study to be borne by the developer/project sponsor. Site grading shall comply with City Municipal Code. (VMC, Chapter 12.40).
79. **LINE OF SIGHT CRITERION.** In design of grading and landscaping, line of sight distance shall be provided based on Caltrans standards. Installation of fencing, signage, above ground utility boxes, etc. shall not block the line of sight of traffic and must be set back as necessary. (VMC, Section 10.14).
80. **ON-SITE SOILS ENGINEER.** During grading operations, the project geologist or soils engineer and necessary soils testing equipment must be present on site. In the absence of the soils engineer or his representative on site, the **Department of Public Works** shall shut down the grading operation. (VMC, Section 12.40.080).

81. **DUST AND EROSION CONTROL.** All dust and erosion control shall be in conformance with City standards and ordinances. (VMC, Sections 12.40.050 & 12.40.070).
82. **COMPACTION TESTS.** Prior to building permit issuance or acceptance of grading, compaction test results and certification letter from the project soils engineer and civil engineer confirming that the grading is in conformance with the approved plans must be submitted to the **Department of Public Works** for review and approval. Test values must meet minimum relative compaction recommended by the soils engineer (usually at least 90 percent). (VMC, Section 12.40.070-R).
83. **DRIVEWAY STANDARDS.** Entrances to any private project must be standard driveway approaches unless deviation is permitted by the **City Engineer**. (VMC, Section 12.04.100).
84. **STREET EXCAVATION PERMIT.** Obtain a street excavation permit from the **Department of Public Works** prior to performing any work within City streets or rights-of-way, or prior to any cutting and restoration work in existing public streets for utility trenches. All work shall conform to City standards. (VMC, Section 10.08).
85. **ENCROACHMENT PERMIT.** Prior to building permit issuance, obtain an encroachment permit from the **Department of Public Works** for all work proposed within the public right-of-way. (VMC, Section 10.16).
86. **TRAFFIC CONTROL PLAN.** Prior to start of construction, submit a traffic control plan to the **Department of Public Works** for review and approval. (Caltrans Traffic Manual).
87. **COORDINATION OF CONSTRUCTION INSPECTION.** Construction inspection shall be coordinated with the **Department of Public Works** and no construction shall deviate from the approved plans. (COV, Regulation & Standard Specification Sections 1.1.4 & 1.1.5).
88. **PLAN CHANGES.** The project design engineer shall be responsible for the project plans. If plan deviations are necessary, the project engineer must first prepare a revised plan or details of the proposed change for review by the **Department of Public Works** and, when applicable, by **Vallejo Sanitation and Flood Control District**. Changes shall be made in the field only after approval by the City. At the completion of the project, the design engineer must prepare and sign the "as built" plans. (COV, Regulation & Standard Specification Section 1.1.9).
89. **BONDS AND FEES.** Prior to approval of construction plans, provide bonds and pay applicable fees. Bonding shall be provided to the City in the form of a "Performance Surety" and a separate "Labor and Materials Surety" in amounts stipulated by City ordinance. (VMC, Section 15.12.090, Resolution Nos. 84-554 N. C. and 02-55 N. C.)
90. **INSTALL IMPROVEMENTS.** Prior to occupancy/final building inspection, install the improvements required by the **Department of Public Works** including but not limited to streets and utilities. (VMC, Section 12.04.060).
91. **STREET TREES.** Prior to release for occupancy, plant required street trees in accordance with City Municipal Code. The list of approved trees is available in the office of the Public Works Director. The minimum standard shall be at least one tree for each 50 feet of street frontage or fraction thereof, including secondary or side streets. Street tree(s) shall be inspected by Public Works Landscape Inspector prior to release for occupancy. (VMC, Section 15.06.190 and Regulations and Standard Specifications Section 3.3.48).
92. **JOINT TRENCH.** The developer shall provide joint trench plans for the underground electrical, gas, telephone, cable television and communications conduits and cables

including the size, location and details of all trenches, location of all building utility service stubs and meters and placement or arrangements of junction structures as a part of the Improvement Plans submitted for the project. The composite drawings and/or utility improvement plans shall be signed by a licensed civil engineer. (VMC, Sections 15.06.160&170).

93. **SIGNAL INTERCONNECT CABLES.** There are fiber optic and /or copper signal inter connect cables located at the edge of the roadway or under the sidewalk. The plans should address either the relocation of these cables or a note should be made of the cable location. A warning should be included on the plans stating that if the cable damaged, the entire length of the cable between the two nearest hubs will be replaced by the contractor unless otherwise authorized by the City Engineer.
94. Corner of Benicia Road and Rollingwood Drive shall be improved for right turn and may require additional right of way dedication to the City. Traffic study shall address this concern.
95. No designated parking area allowed fronting the property along Rollingwood Drive.
96. Necessary turn lane shall be provided for cemetery access on Benicia Road and Rollingwood Drive.
97. Provide turnaround at the end of proposed alleys (minimum hammer head).
98. Designate required public utility easement area between buildings and back of curb for utility facilities in alley homes area.
99. Surface drainage of this project shall not adversely affect the neighboring properties.

WATER DIVISION**THE FOLLOWING STANDARD COMMENTS/CONDITIONS APPLICABLE TO THE SUBJECT ARE SO MARKED:**

100. **WATER SYSTEM PLANS.** All water system improvements shall be consistent with the Vallejo Water System Master Plan, 1985, prepared by Kennedy/Jenks Engineers as updated by Brown & Caldwell, 1996. Prior to Improvement Plan approval and building permit issuance, water system improvement plans shall be submitted to the **Water Division** for review and approval, and shall contain at least:
- a. Location and size of fire sprinkler service connection(s).
 - b. Location and size of domestic service connection(s).
 - c. Location and size of irrigation service connection(s).
 - d. Location of fire hydrants.
 - e. Location of structures with respect to existing public water system improvements, such as mains, meters, etc.
 - f. Location and size of any new water mains.
 - g. Location and size of backflow prevention devices (required on water service connections to irrigation systems, certain commercial water users, and to commercial fire sprinkler systems, per City Ordinance 922 N.C. (2d).
101. **FIRE FLOW REQUIREMENTS.** Fire flow requirements of the Fire department shall be complied with. Fire flow at no less than 25 psig residual pressure shall be available within 1,000 feet of any structure. One half of the fire flow shall be available within 300 feet of any structure.
- a. For single family residential units, the fire flow is 1,500 gpm.
 - b. For other developments, see the Vallejo Water System Master Plan, 1985, prepared by Kennedy Jenks and its latest update by Brown and Caldwell dated April 1996.
102. **HYDRAULIC CALCULATIONS.** Prior to Improvement Plan approval and building permit issuance, hydraulic calculations shall be submitted to the **Water Superintendent** demonstrating that the fire flow requirements are complied with.
103. **FIRE PROTECTION SYSTEMS.** Fire hydrant placement and fire sprinkler system installation, if any, shall meet the requirements of the Fire Department. For combined water and fire services, the requirements of both the Fire Department and the Vallejo Water System Master Plan, with latest revisions, shall be satisfied.
104. **WATER EASEMENTS.** Easements shall be granted for all water system improvements installed outside the public right-of-way in the City's Standard Form for Grant of Water Line Easement with the following widths:
- a. 15 ft. wide (minimum) for water mains.
 - b. 10 ft. wide (minimum) for fire hydrants, water meters, backflow preventers, double detector check valves, etc.
 - c. Other facilities will be reviewed by the Water Division.
105. **WATER SERVICE BONDS AND FEES.** Water service shall be provided by the City of Vallejo following completion of the required water system improvements and payment of applicable fees. Performance and payment bonds shall be provided to the City of Vallejo prior to construction of water system improvements. Fees include those fees specified in the Vallejo Municipal Code including connection and elevated storage fees, etc., and fees for tapping, tie-ins, inspections, disinfection, construction water, and other services

provided by the City with respect to the water system improvements. The Water Division may be contacted for a description of applicable fees.

106. **WATER SYSTEM INSTALLATION.** Prior to occupancy or final building inspection, install water system improvements as required. Backflow device/s where required shall be installed in areas hidden from public view and/or shall be mitigated by landscaping.

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED ABOVE:

107. There is insufficient water pumping capacity, insufficient pipe supply capacity, and insufficient maximum day water storage for this development in the 400 water pressure zone that will serve this development.
108. A comprehension water study (master plan) must be done for the 400 water zone and the water zones fed from the 400 water zone (Kathy Ellen 337, Devlin 262, Burnham 329, Capital 280, Bay Village 292, Maritime Academy 267, and Crystal 400 zones) to determine existing capacities and future demands and required improvements to water mains, pump stations, water reservoirs, etc. The Developer shall prepare this comprehension water study, including a unit cost benefit calculation, consistent with the existing Water Master Plan and submit it to the Water Division for review and approval. The study shall be prepared by a California Registered Civil Engineer with experienced in such field.
109. The developer shall pay to the city its prorated share of the costs of these improvements including but not limited to, 400 zone master plan preparation costs, right of way costs, planning and engineering costs, construction costs, etc before the development improvements plans can be approved.
110. The Developer's cost for compliance for Conditions W11, W12 and W13 shall be in addition to the costs required to be paid to the Water Division as mentioned in Condition W9 with the one exception that no elevated Storage Fee shall be charged.
111. The developer can, in lieu of complying with conditions W11, W12, W13, and W14, pay to the Water Division a sum to be determined by agreement to be made with the Water Division for the city to provide water storage and zone pumping capacity for this project. In order to expedite development, similar projects have agreed to an amount between \$6,500 and \$10,000 (in 11/2005 dollars) per unit to forgo the conditions W11-W14. This amount is for the 400 water zone supplement capacity fee charged for new projects in the 400 water zone to allow the city to construct the required storage, pumping, and pipeline modifications necessary. This money shall be paid to the Water Division before the improvements can be approved by the city. At the time that the amount is paid the amount shall be adjusted for inflation from the date of the agreement to the date of payment. This negotiated amount is in lieu of the elevated storage fee only. All other water fees are due and applicable as per condition W9.
112. The master meters and back flow devices for the Paseo Homes shall be placed in landscaped areas at the back of public sidewalks in a PUE.

BUILDING DIVISION

113. In January 2007 the Building Division will be using the 2007 California Building Code, which requires that anything closer than five feet from property line be a fire wall. This may require that some of the windows on the sides in the paseo homes be fire rated.

VALLEJO SANITATION AND FLOOD CONTROL DISTRICT**GENERAL COMMENTS:**

114. After plans are approved submit a Connection Permit Application (SSI) Form for connection fee calculation (\$20 submittal fee). Non-residential developments shall also submit a Pretreatment Questionnaire for review by VSFCO Pollution Control Department. (See note below)

THE FOLLOWING STANDARD COMMENTS/CONDITIONS APPLICABLE TO THE SUBJECT ARE SO MARKED:

115. If any of the VSFCO comments are in conflict with comments from other reviewing agencies, please request clarification.
116. The existing storm drain systems, downstream of Dryden Drive and downstream of Benicia Road, are undersized for the existing runoff and the intended runoff from the project. The secondary use of the park for storm water detention is intended to mitigate the increased runoff from the project.
117. **STORM WATER QUALITY AND PRE-TREATMENT.** Pretreatment of storm water runoff is required. Refer below to standard conditions of approval. If the first flush of storm water is filtered over the landscaped area of the park, this should satisfy the requirement for pre-treatment.
118. **GRADING.** The storm overflow pattern will need to be shown on the grading plans. Determine the 100-year storm tributary area. This may differ from the 15-year tributary area. Ensure that there are no buildings within the limits of the 100-year storm overflow pattern. Where practicable, the overland release shall be into the Park Parcel A so that the overland release does not end up going down to the bottom of Court I.
118. Grading and improvement plans shall include storm water pollution prevention plans for use during site development and building construction to mitigate impacts of this development. Refer below to standard conditions of approval.
119. **SEWER.** SS Laterals: On the improvement plans, the proposed locations of SS laterals will need to be coordinated with the location of proposed street trees. Sanitary Sewer laterals are to split the distance between street trees.
120. **MAINTENANCE ACCESS TO FACILITIES.** Refer to comment below related to standard access for maintenance vehicles to VSFCO facilities. It appears that standard maintenance access to SD/SS structures may be an issue at the following locations:
- a. 2 SDMH west of Parcel D. It appears that the SD can be realigned so that the VSFCO structures will be accessible for maintenance from Benicia Road.
 - b. SD in Parcel C and D.

PROJECT SPECIFIC CONDITIONS AND OTHER COMMENTS/CONDITIONS NOT COVERED BY THE STANDARD CONDITIONS LISTED:

121. Applicant shall pay all fees (plan review fees, connection fees, etc.) required by VSFCDD for the subject project.
122. Improvement plans shall comply with the VSFCDD Engineering Design Standards and Policies, within the Master Bid Document dated March 2007 or later, regarding design and construction of storm drains (SD) and sanitary sewer (SS) facilities. Improvement plans shall indicate that proposed improvements are to be constructed in accordance with Standard Plans And Specifications included in the VSFCDD Master Bid Document dated March 2007, or later edition.
123. Grading and improvement plans shall include storm water pollution prevention plans for use during site development and building construction to mitigate impacts of this development. This plan shall include calculations, measures related to debris, refueling areas, disposal of excess materials, site cleanup, hazardous substance containment, street cleaning, catch basin cleaning, and other similar measures (see Section 10 Storm Water Runoff of the VSFCDD Engineering Design Standards and Policies).
124. Gravity sewer mains with lateral connections shall be no more than 10-feet deep.
125. Pretreatment of storm drainage water runoff is required, storm drainage runoff shall be conveyed over landscaped areas, or otherwise treated, using structures before discharging into the public system. This is to improve the stormwater quality leaving the site. As much as practicable, developer shall incorporate measures described in "Start at the Source" a residential site planning and design guidance manual for storm water quality protection (written by BASMA) as a means of mitigating project impacts, and reduce impacts of increases in impervious surfaces.
126. The development shall be planned so that all VSFCDD SD and SS facilities shall be accessible by standard access. Standard access means that each structure (MH or CB) shall be accessible by an AC paved path, 15-feet wide from the public street to each maintenance structure (MH, CB, etc.), such that VSFCDD maintenance vehicles can drive to and park over the facilities. Unless otherwise allowed by VSFCDD, District vehicles must be able to drive forward in and forward out. The designer shall refer to VSFCDD standard detail No 25 for access criteria.
127. Existing storm drain and/or sanitary sewer facilities that are to be abandoned in place shall be filled with grout, otherwise they shall be removed.
128. The grading and landscaping (planting and irrigation) in Parcels B, C and LL will be limited by the requirement for the standard maintenance vehicle access to the storm drain structures (SDDI and SDMH in Parcels B and C) and account for any proposed public SD that is below ground in Parcel B and LL.

Sheet 3 and 4 - Grading and Utility Plan.

129. **Western Slope of Park Parcel A:** Some treatment will be required at the toe of this slope because of the amount of water that will be draining along the toe. Treatment may include bio-swale or turf reinforcement, or other. This toe of slope may become saturated and will be the last place in the park to dry out after a storm, especially in the area of the outlet works. The solution will also need to be coordinated with, and approved by Greater Vallejo Recreation District (GVRD). Assuming that the access road will be for pedestrian use also, and depending on the treatment for the toe of slope, the proposed location for the access road will need to be evaluated as part of the review of the grading and improvement plans.
130. VSFCDD has no objection to the concept of using the park for pre-treatment of the first flush of storm water runoff. We recommend that the heavier flows do not run on the surface of

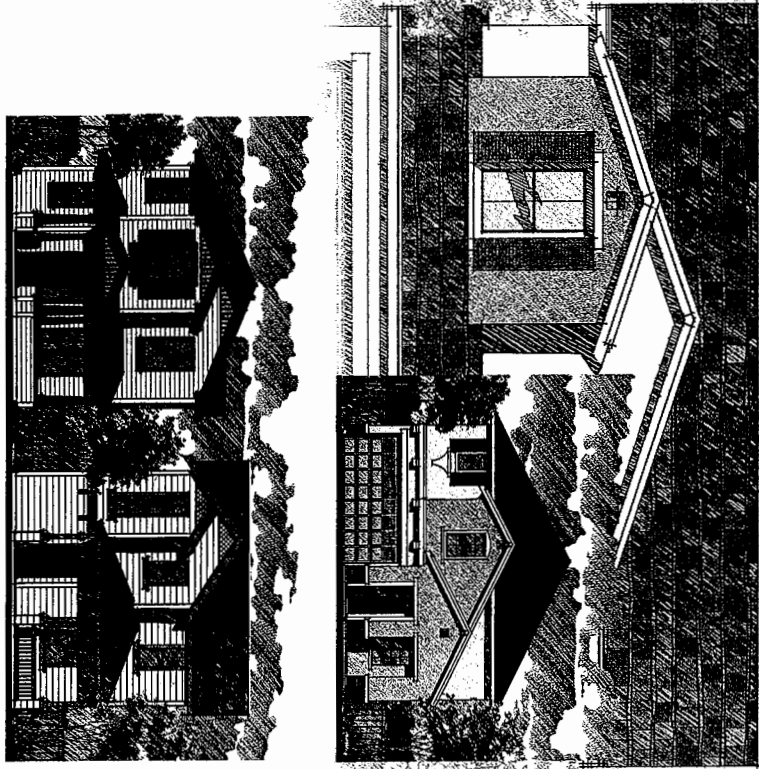
the park, but bypass the park into the storm drain outlet and/or detention system. This will need to be coordinated with GVRD. VSFCDC has no objection to the concept of using the storage capacity of the park to detain peak flows in heavy or moderate storm events. However, VSFCDC does have some comments on the particulars of the current design of the system as proposed on the VTM. The system will require some slide gates and weirs in drainage structures in order to operate as intended by VSFCDC. After approval of the tentative map, VSFCDC Engineering and Field Operations (FOPS) staff will meet with the developer to discuss these particulars.

131. **Parcel B SD Alignment:** After approval of the tentative map, VSFCDC Engineering and Field Operations (FOPS) staff will meet with the developer's engineer to discuss the SD alignment and the ownership and maintenance responsibility of the SD in Parcel B and the off-site SD to the north of Parcel B.
132. Parcel B wall adjacent to vehicle turnaround: Note the clear area required adjacent to the pavement as described in note 4 of VSFCDC standard detail No. 25.
133. **Grading in Parcel LL:** VSFCDC will waive the requirement for an all weather (paved) surface over this public SD. However, the ground surface over the SD needs to be graded in accordance with VSFCDC requirements for standard vehicle maintenance access.
134. **Hydrology and Hydraulic Report:** Will be required for the storm drain system. The report will need to compare the pre-development and post-development storm water discharge for flows leaving the site to the north through Parcel B and to the south into Benicia Road.
135. Where landscaping is allowed over SD or SS public utilities owned and maintained by VSFCDC, landscaping will be limited to mulch, turf or low ground cover. Trees and bushes are not allowed. This may apply in Parcels LL and Parcel B.
136. Drainage along the west boundary of Parcels B, C, LL, Lots 83-104 and north boundary of Lots 58-69: It appears that this drainage is intended to be in a concrete J-ditch. This J-ditch will not be maintained by VSFCDC. The map must address the responsibility for maintenance of this J-ditch. Consult with City of Vallejo (COV) on the proper method (reciprocal easement or agreement) to address maintenance of this drainage ditch.
137. **SD between Lots 91 and 92:** In order for this to be a VSFCDC owned and maintained SD it will need to be constructed to VSFCDC standards for vehicle maintenance access, easement width, construction materials, pipe size, and inlet size.
138. **Drainage along north boundary of Lots 58-69:** VSFCDC will not maintain the slope or above ground drainage on these private lots. Refer also to section C on sheet 2. If the developer hopes to collect and convey public drainage to be maintained by VSFCDC then the drainage collection will have to be by VSFCDC standard inlet facilities and the drainage conveyance will have to be by VSFCDC standard drainage pipe. Vehicle maintenance access, that meets the standards of VSFCDC, will be required to all VSFCDC facilities. The drainage J-ditch appears to end at the west boundary of Lot 64. If the flow is intended to discharge to the ground and flow overland, then there should be engineered means of erosion control/slope protection.
139. **Off-site Sanitary Sewer:** The limits of improvement of the offsite sanitary sewer are pending the final analysis of the impacts of the development on the VSFCDC system. Any new offsite sewer that is not below a paved surface will need to be constructed of butt-fused high density polyethylene pipe (HDPE).

140. **Off-site Storm Drain north of Parcel B:** The developer will be required to negotiate any easements required for new storm drains including the new SD shown north of Parcel B on the Land of Humphreys.
141. SS and SD utilities in the public right-of-way (R/W) will need to be publicly owned and maintained by VSFCO. Once a given facility is public, the downstream extension of that facility must also be public. The downstream extensions of the SD or SS facilities cannot revert back to private ownership and maintenance. It appears that this may be an issue where the private SD and SS exit Parcels X, Y, and Z and enter the public R/W of Street D. The downstream extensions of these utilities continue into the private streets of Parcels H and J. There are a couple of options for addressing this situation. The SD and SS can be realigned so that the downstream extensions of the utilities, once public, remain in the public R/W. It appears that this can conveniently be done for the SD from Parcel Y and Z. Alternately the SS and SD in Parcels J, I, and H can be publicly owned and maintained by VSFCO provided that VSFCO rights are addressed (by easement or other) on the final map. Note that the sewers in Parcel K and G cannot be publicly owned and maintained as shown on the VTM because they do not satisfy the VSFCO requirement for access to the manholes at the upper end of the sewer mains.

Sheet 5 Ownership and Maintenance

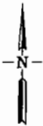
142. VSFCO rights to maintain the SD in Parcels B, C and LL will be by easement or agreement (on COV owned property). VSFCO will not own the parcels as shown on sheet 5 of the VTM.
143. **Private Streets maintained by Homeowners Association (HOA):** We understand that the intent is that the SD and SS utilities below these private streets will also be maintained by HOA.
144. SS constructed in the private streets will be installed to VSFCO standards for design, materials and construction except that MH covers shall not be marked "VSFCO."



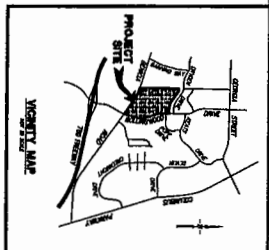
ROLLINGWOOD

9 NOVEMBER 07

PLANNED UNIT DEVELOPMENT ROLLINGWOOD CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA



LOCATION MAP



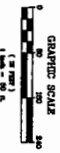
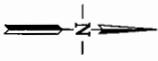
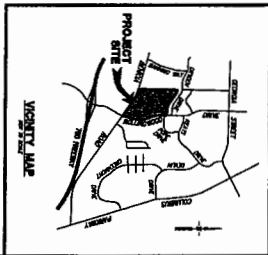
- SHEET INDEX**
1. TITLE SHEET
 2. PRELIMINARY PLAN
 3. SITE PLAN
 4. SITE PLAN AND MAINTENANCE RESPONSIBILITIES SHEET
- TENTATIVE MAP**
5. COVER SHEET
 6. TYPICAL SECTIONS AND DETAILS
 7. GRADING AND UTILITY PLAN
 8. GRADING AND UTILITY PLAN
 9. EROSION CONTROL PLAN
 10. EROSION CONTROL PLAN
 11. EROSION CONTROL PLAN

Ruggel -
Azer & Associates
PLANNERS & ARCHITECTS, C. 1988
1000 PINE STREET, SUITE 700, VALLEJO, CA 94590
DATE: OCTOBER 28, 2007

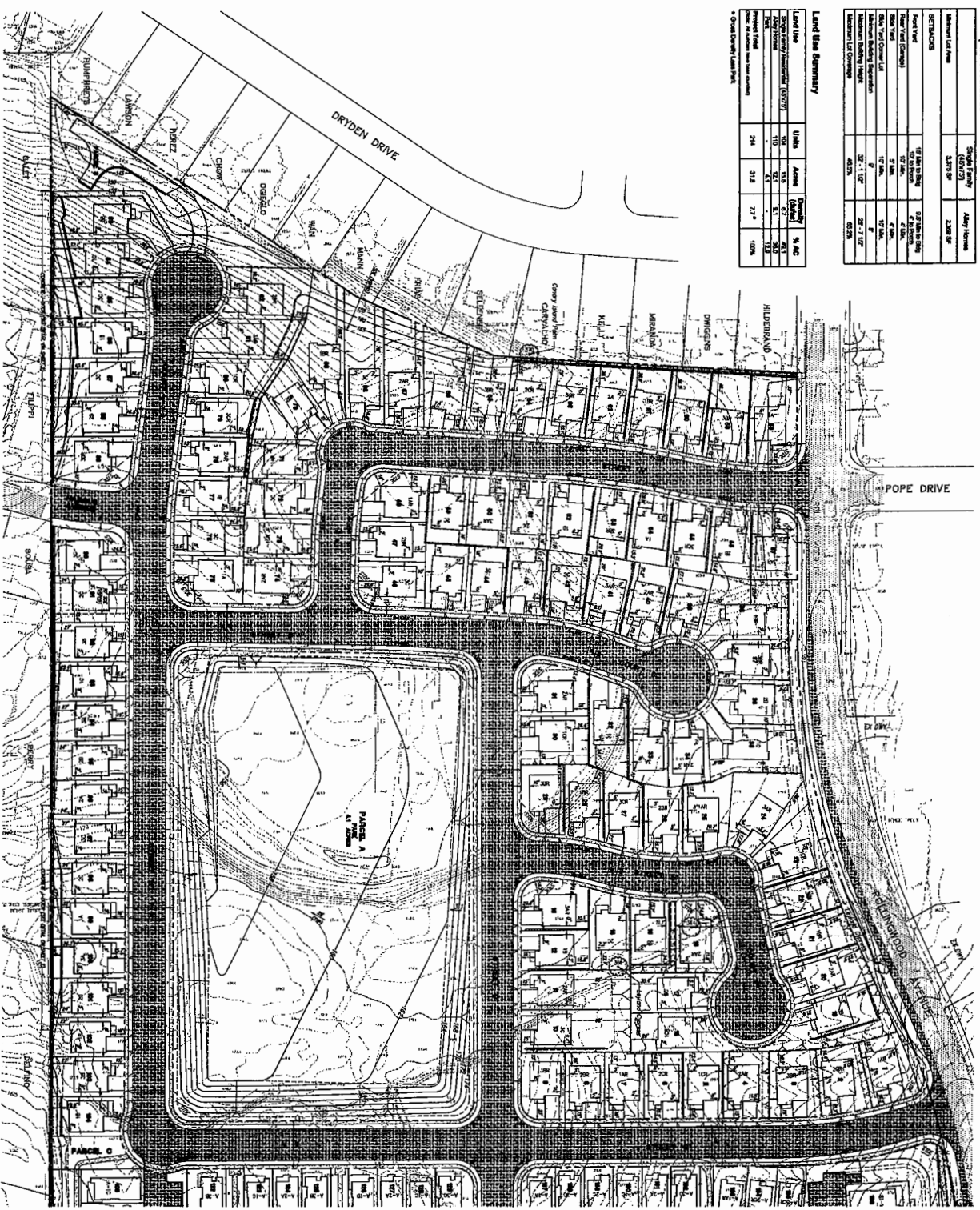
4



AERIAL PHOTO - ROLLINGWOOD



Jensen & Associates
 1000 N. 1000th Street, Suite 200, Aurora, CO 80015
 303.733.1111
 www.jensenandassociates.com



Development Standards

Development Standard	Single Family (R27.07)	Accessory Dwelling
Minimum Lot Area	3,217 sq'	2,281 sq'
Setbacks	15' Side to Street, 10' Front, 10' Rear	5.2' Side to Street, 5' Front, 5' Rear
Height	35' Max.	35' Max.
Maximum Building Footprint	27.1' x 107'	27.1' x 107'
Maximum Lot Coverage	40.5%	40.5%

Land Use Summary

Land Use	Units	Area (Total)	Percentage	% AC
Single Family (R27.07)	110	113	47.1	48.1
Accessory Dwelling	10	10	4.3	1.7
Open Space	1	1	0.4	0.2
Public Use	1	1	0.4	0.2
Other	244	244	100.0	100.0

* Open Space (Park)

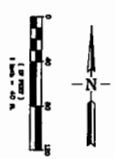
SEE PUD SHEET 4 FOR CONTINUATION

Unit Summary

Unit Type	Number of Units	Approx. Area (sq ft)	Approx. Area (Acres)
Single Family	110	1,130,000	25.8
Accessory Dwelling	10	100,000	2.3
Open Space	1	100,000	2.3
Public Use	1	100,000	2.3
Total	122	1,340,000	30.7

Site Plan Summary

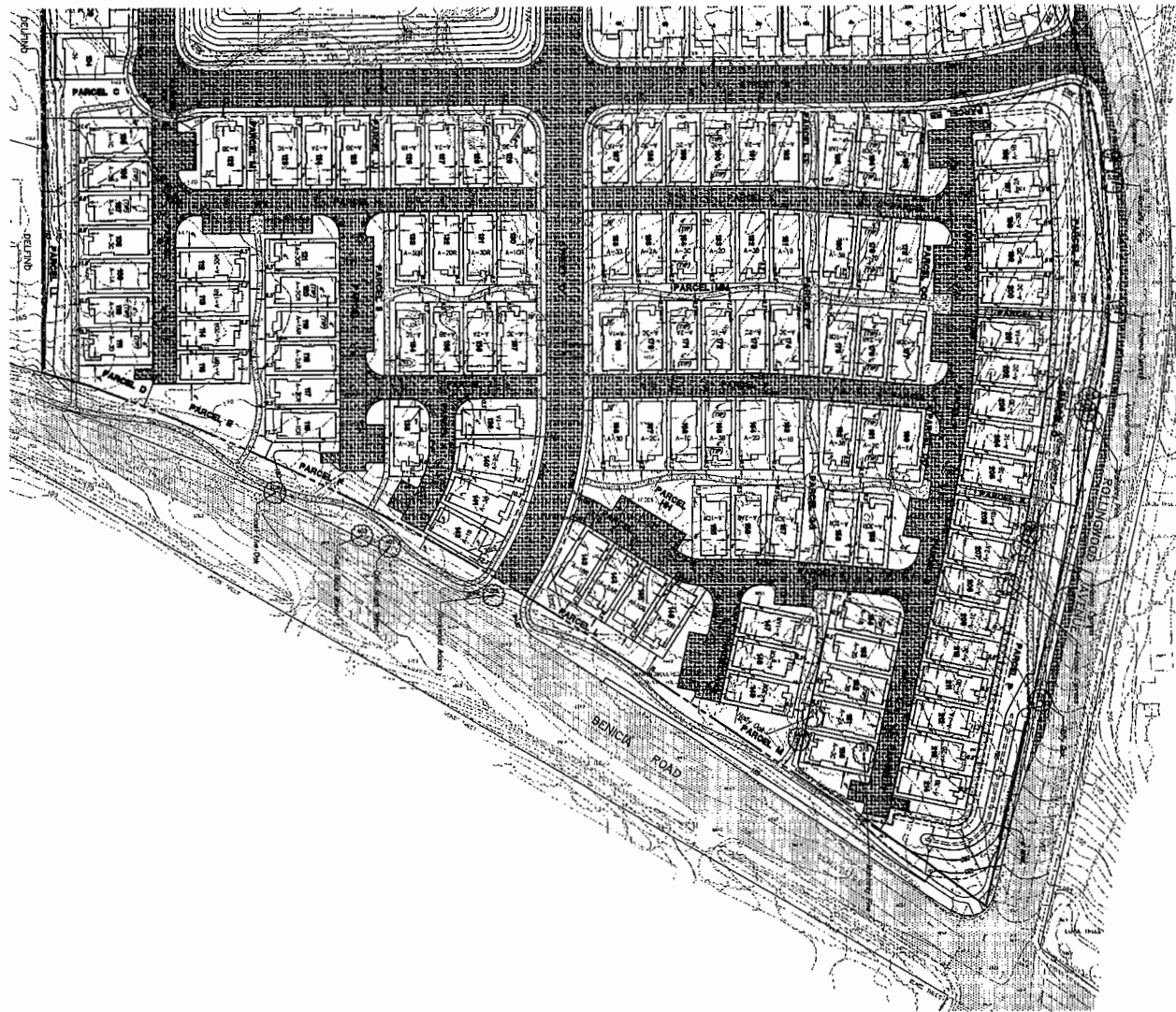
Item	Area (Acres)	% of Total Area
Single Family	25.8	84.1
Accessory Dwelling	2.3	7.5
Open Space	2.3	7.5
Public Use	2.3	7.5
Total	30.7	100.0



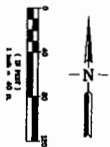
SITE PLAN
ROLLINGWOOD

Russell - Jensen Associates
 ARCHITECTS
 1000 15th Street, Suite 100
 Denver, CO 80202
 DATE: OCTOBER 26, 2007
 DRAWING NO.: RJA-2007-001
 SHEET NO.: 3

SEE PUD SHEET 3 FOR CONTINUATION

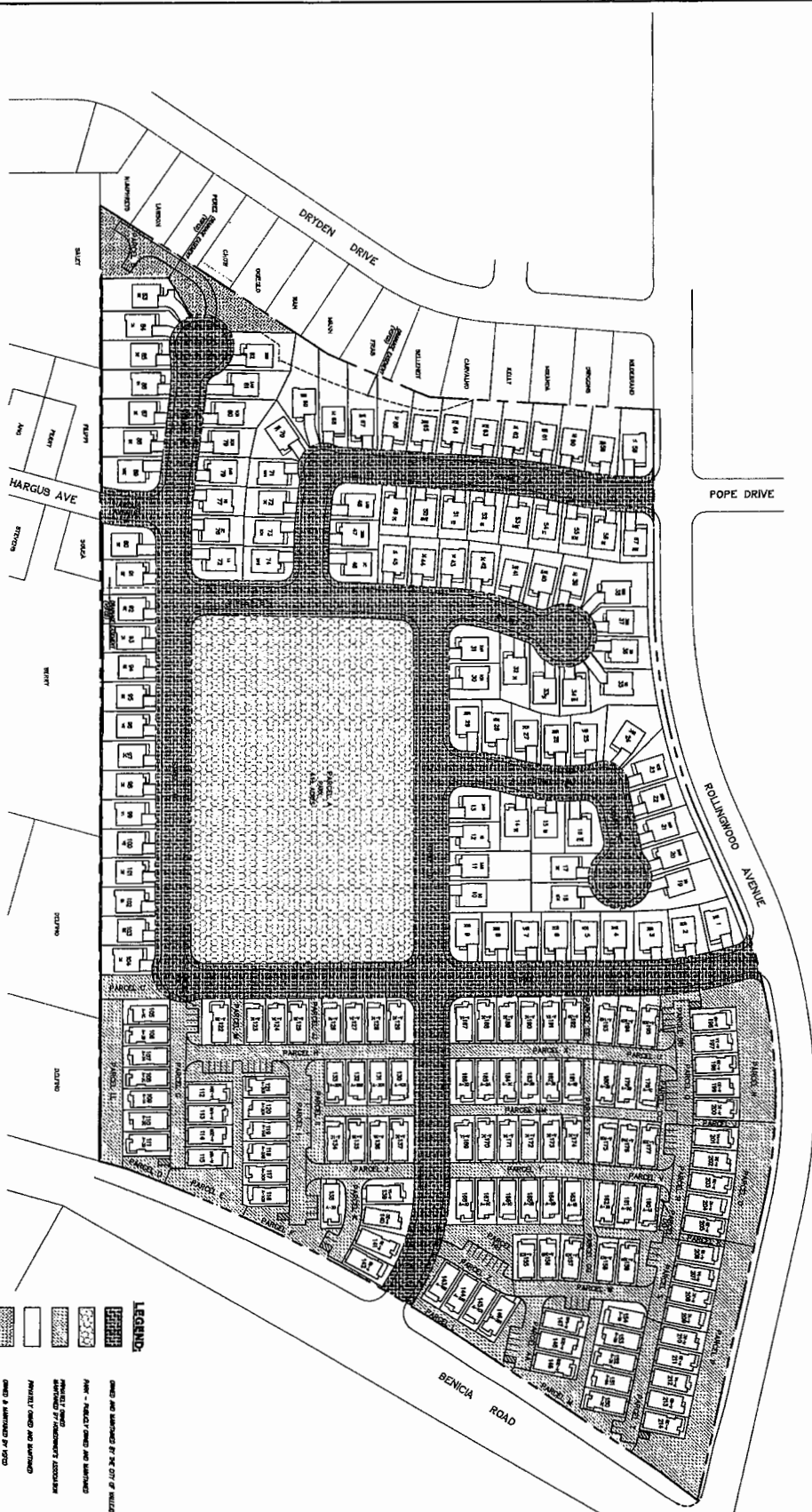


SITE PLAN
ROLLINGWOOD



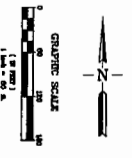
Ruggert
Planners & Associates
10000 15th Avenue, Suite 100
Denver, CO 80202
Phone: (303) 555-1557 Fax: (303) 555-1558

DATE: OCTOBER 24, 2007
DRAWN BY: [Name]
CHECKED BY: [Name]
SCALE: AS SHOWN
SHEET NO. 7

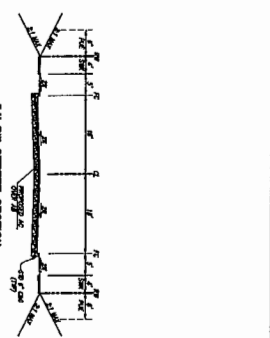


OWNERSHIP AND MAINTENANCE RESPONSIBILITIES EXHIBIT - ROLLINGWOOD

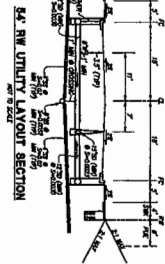
- LEGEND:**
- OWNED AND MAINTAINED BY THE CITY OF ROLLINGWOOD
 - HOME - PROJECT OWNED AND MAINTAINED BY HOMEOWNERS ASSOCIATION
 - PRIVATELY OWNED AND MAINTAINED BY HOMEOWNERS ASSOCIATION
 - PRIVATELY OWNED AND MAINTAINED BY HOMEOWNERS ASSOCIATION
 - HOME & MAINTAINED BY HOMEOWNERS ASSOCIATION



Russell -
Rollingwood Association
 1000 ROLLINGWOOD AVENUE
 ROLLINGWOOD, CA 94575
 DATE: OCTOBER 28, 2007
 DRAWN BY: [Name]
 CHECKED BY: [Name]



E4. RW STREET SECTION
12' RW
6' SIDEWALK
6' STREET
6' GUTTER

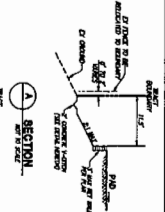


E4. RW UTILITY LAYOUT SECTION

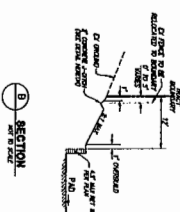
E2. RW UTILITY LAYOUT SECTION



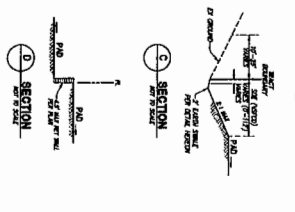
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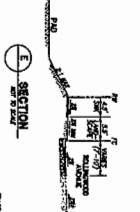
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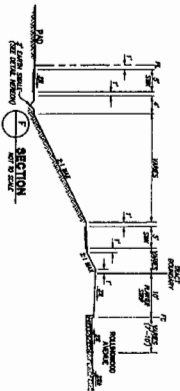
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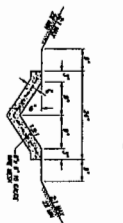
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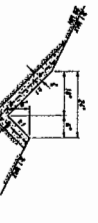
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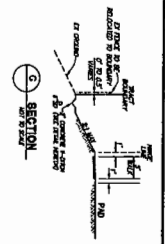
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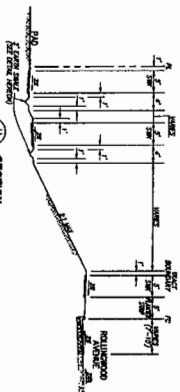
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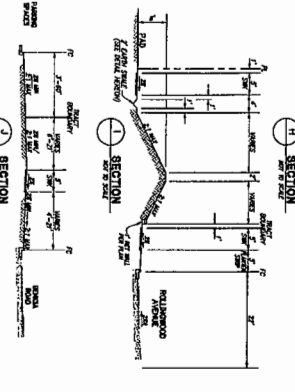
E3. RW CONCRETE SITER



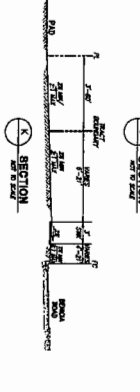
G SECTION



H SECTION



I SECTION



J SECTION



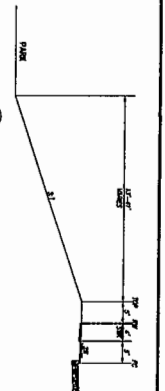
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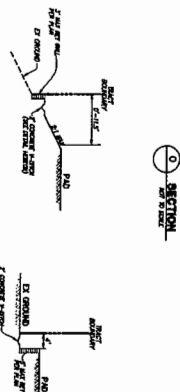
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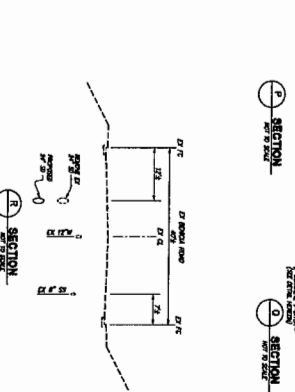
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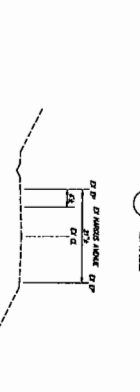
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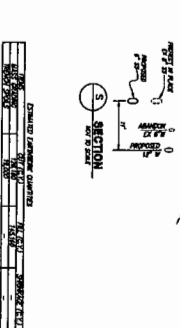
O SECTION



P SECTION



Q SECTION



R SECTION

ESTIMATED FINISHING QUANTITIES

ITEM	UNIT	QUANTITY
1. ASPHALT	CY	100.00
2. CONCRETE	CY	50.00
3. GUTTER	LF	100.00
4. CURB	LF	100.00
5. SIDEWALK	LF	100.00
6. STREET	LF	100.00
7. GUTTER	LF	100.00
8. CURB	LF	100.00
9. SIDEWALK	LF	100.00
10. STREET	LF	100.00

PRELIMINARY PAVEMENT DESIGN CHART

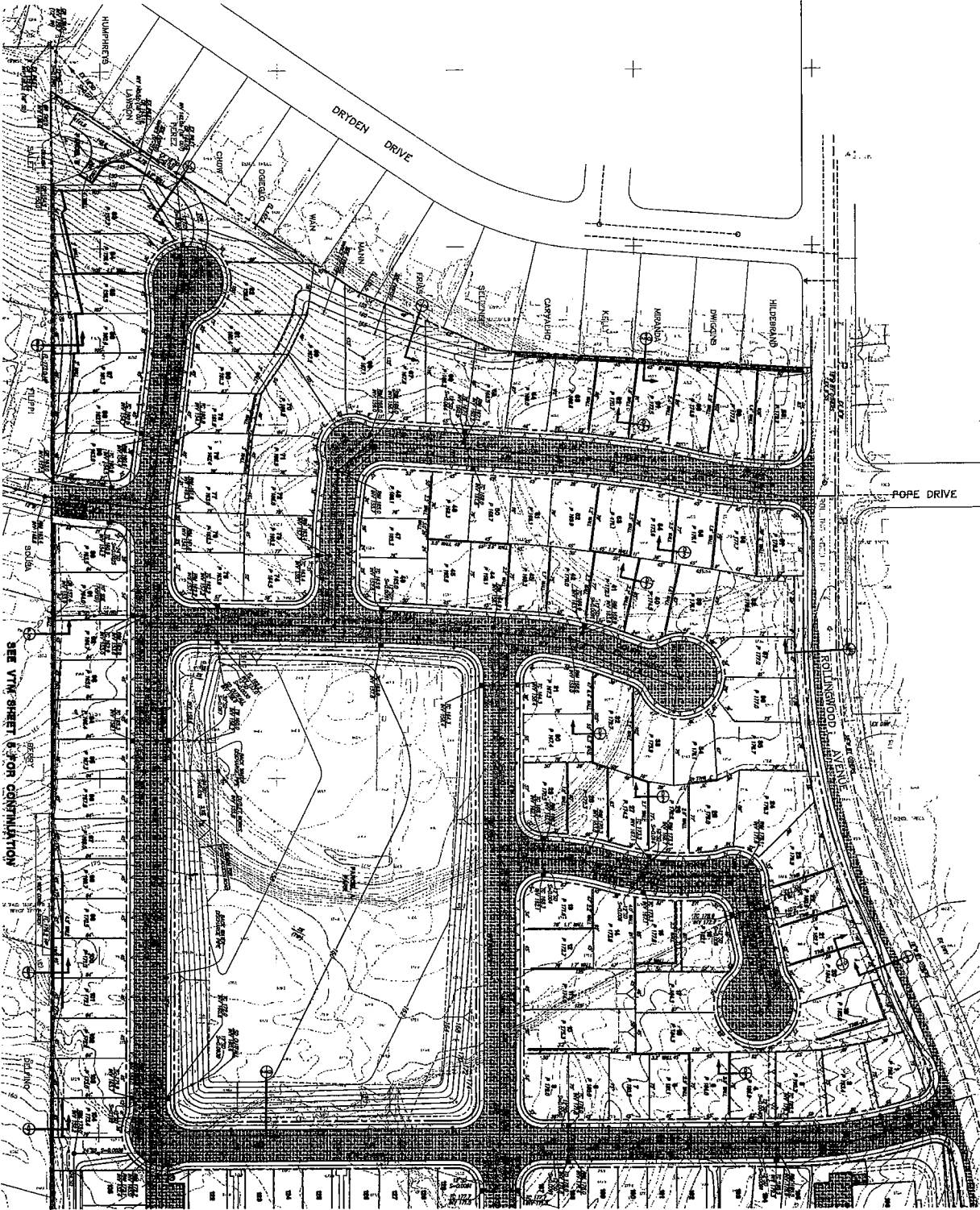
DESIGN YEAR	DESIGN SPEED	DESIGN TRAFFIC	DESIGN PAVEMENT
2010	35	1000	ASPHALT
2015	35	1000	ASPHALT
2020	35	1000	ASPHALT
2025	35	1000	ASPHALT
2030	35	1000	ASPHALT
2035	35	1000	ASPHALT
2040	35	1000	ASPHALT
2045	35	1000	ASPHALT
2050	35	1000	ASPHALT

VESTING TENTATIVE MAP
TYPICAL SECTIONS ADDETAILS
ROLLINGWOOD
CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

Ruggeri - Jensen & Associates
1000 RUGGERI DRIVE, SUITE 100, VALLEJO, CA 94590
PHONE (925) 274-1000 FAX (925) 274-1001

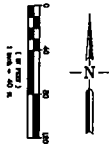
FOR: KB HOME

NO.	DATE	DESCRIPTION
1	10/10/00	PRELIMINARY DESIGN
2	11/10/00	FINAL DESIGN
3	12/10/00	CONSTRUCTION
4	01/10/01	AS-BUILT
5	02/10/01	FINAL
6	03/10/01	REVISION
7	04/10/01	REVISION



SEE VTM SHEET 4 FOR CONTINUATION

SEE VTM SHEET 4 FOR CONTINUATION



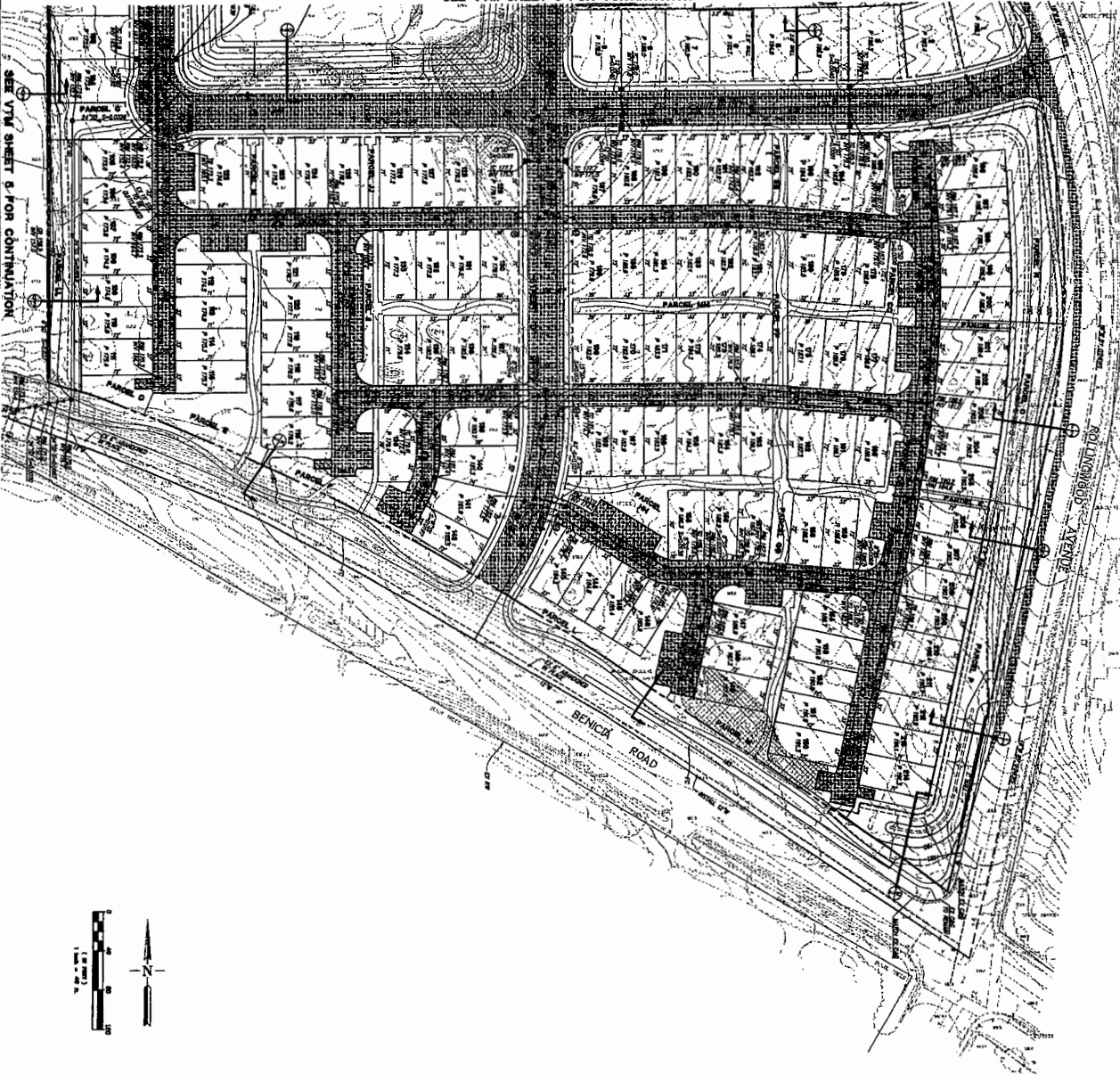
SHEET NO.	8
	8
DATE	6/1/88
SCALE	AS SHOWN
DESIGNED BY	ZAC
CHECKED BY	ZAC
APPROVED BY	ZAC
DATE	6/1/88

FOR: KB HOME

VESTING TENTATIVE MAP
 GRADING & UTILITY PLAN
ROLLINGWOOD
 CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

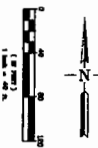
Ruggert-Jensen & Associates
 ENGINEERS
 4000 CHARD DRIVE, SUITE 200 • PLEASANTON, CA 94588
 PHONE: (925) 574-2020 • FAX: (925) 574-2030

SEE VTM SHEET 3 FOR CONTINUATION



SEE VTM SHEET 5 FOR CONTINUATION

SEE VTM SHEET 10 FOR CONTINUATION



DATE	BY	CHK	APP	REVISION

VESTING TENTATIVE MAP
 GRADING & UTILITY PLAN
ROLLINGWOOD
 CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

FOR: KB HOME

Russell -
Crutten
AZAR & Associates
 4880 CHAMBERLAIN DRIVE, SUITE 200 • REXSBORO, CA 94981
 PHONE: (925) 327-9100 • FAX: (925) 327-2362



SEE VTM SHEET 3 & 4 FOR CONTINUATION

31

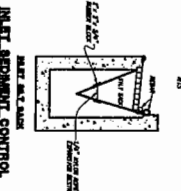
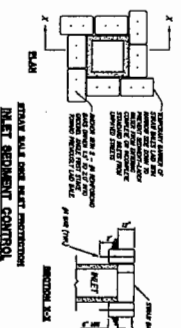
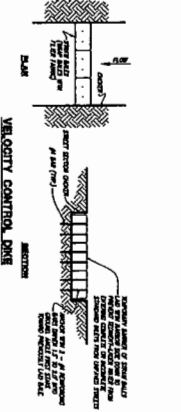
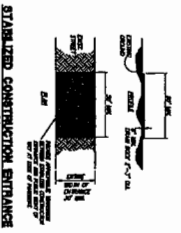
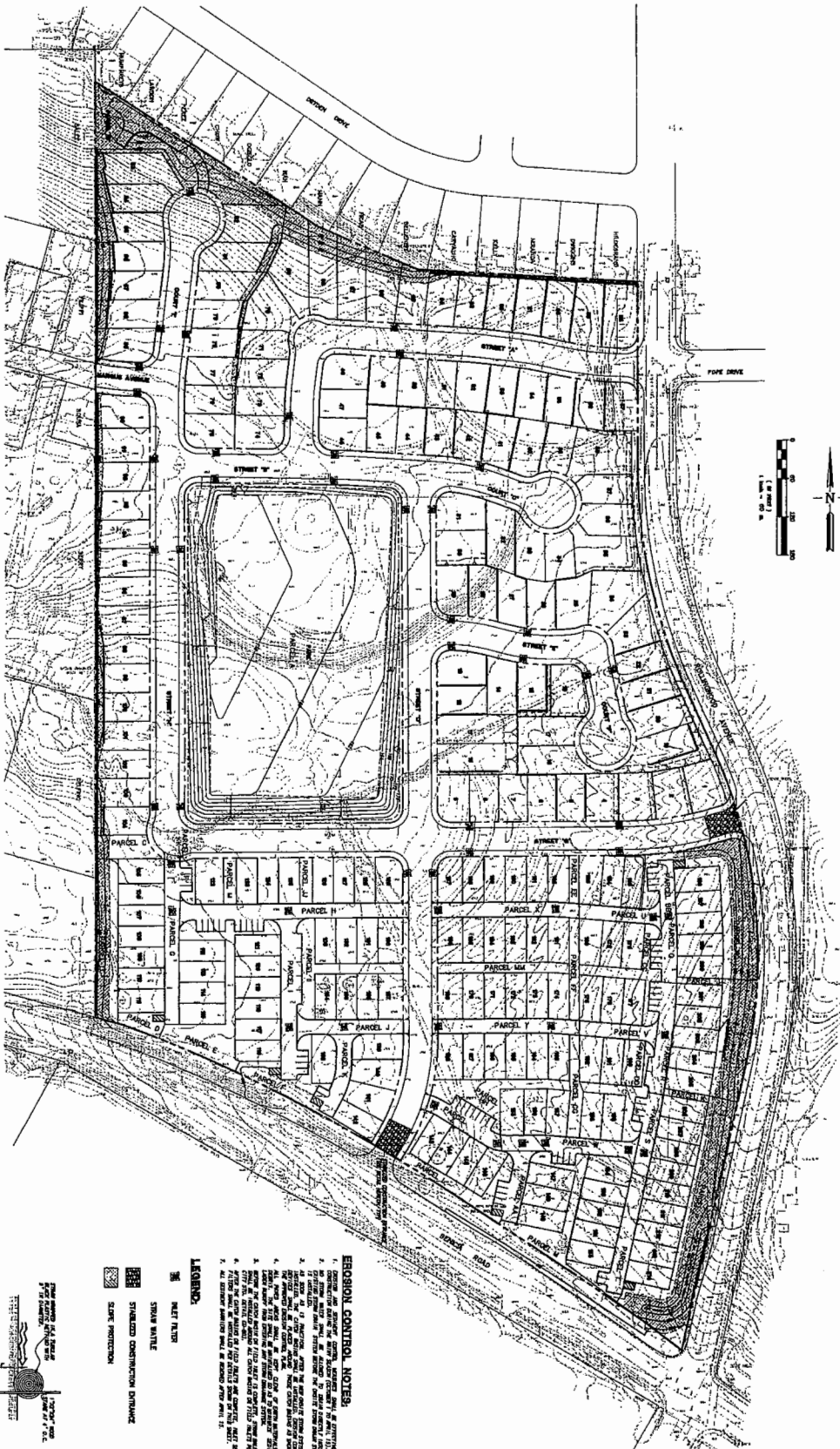
DATE	BY	CHKD BY	APP'D BY

DATE	BY	CHKD BY	APP'D BY

VESTING TENTATIVE MAP
 OFF-SITE UTILITY PLAN
ROLLINGWOOD
 CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

FOR: KB HOME

Ruggeri - Jensen - Jazai & Associates
 4500 CHERRY DRIVE, SUITE 200 - PLEASANTON, CA 94588
 PHONE: (925) 937-4355 FAX: (925) 937-4356



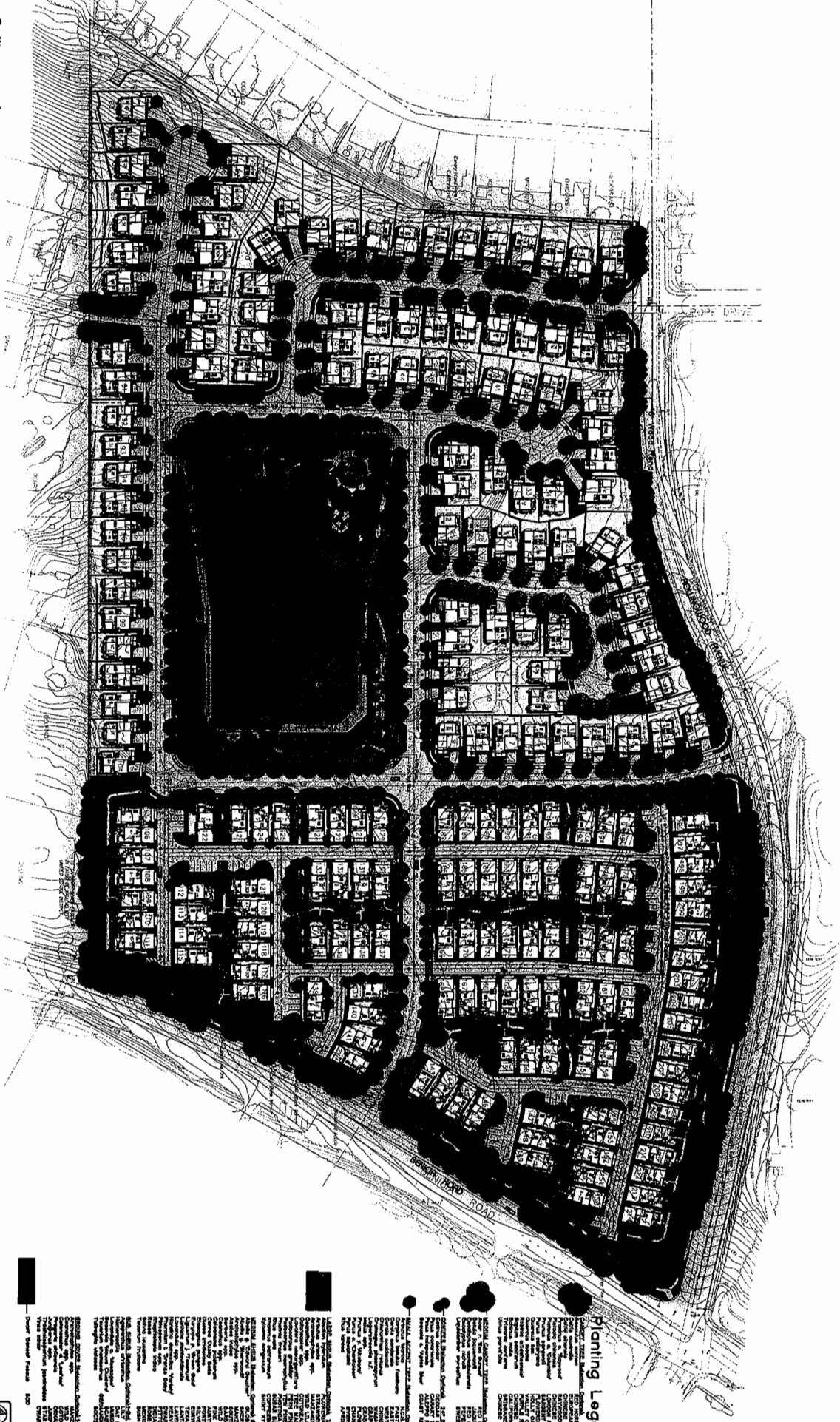
- LEGEND**
- NET FABRIC
 - STRAW WATTLE
 - STANDARD CONSTRUCTION ENTRANCE
 - VELOCITY CONTROL BARRIERS
 - NET SEDIMENT CONTROL
 - STRAW WATTLE DETAIL

- EROSION CONTROL NOTES:**
1. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 3. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 4. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 5. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
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 8. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 9. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 10. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.
 11. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VALLEJO EROSION CONTROL PLAN.

NO.	DATE	BY	REVISION
1	08/11/11	JAZZ	ISSUED FOR PERMITTING
2	08/11/11	JAZZ	ISSUED FOR PERMITTING
3	08/11/11	JAZZ	ISSUED FOR PERMITTING
4	08/11/11	JAZZ	ISSUED FOR PERMITTING
5	08/11/11	JAZZ	ISSUED FOR PERMITTING
6	08/11/11	JAZZ	ISSUED FOR PERMITTING
7	08/11/11	JAZZ	ISSUED FOR PERMITTING
8	08/11/11	JAZZ	ISSUED FOR PERMITTING
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10	08/11/11	JAZZ	ISSUED FOR PERMITTING

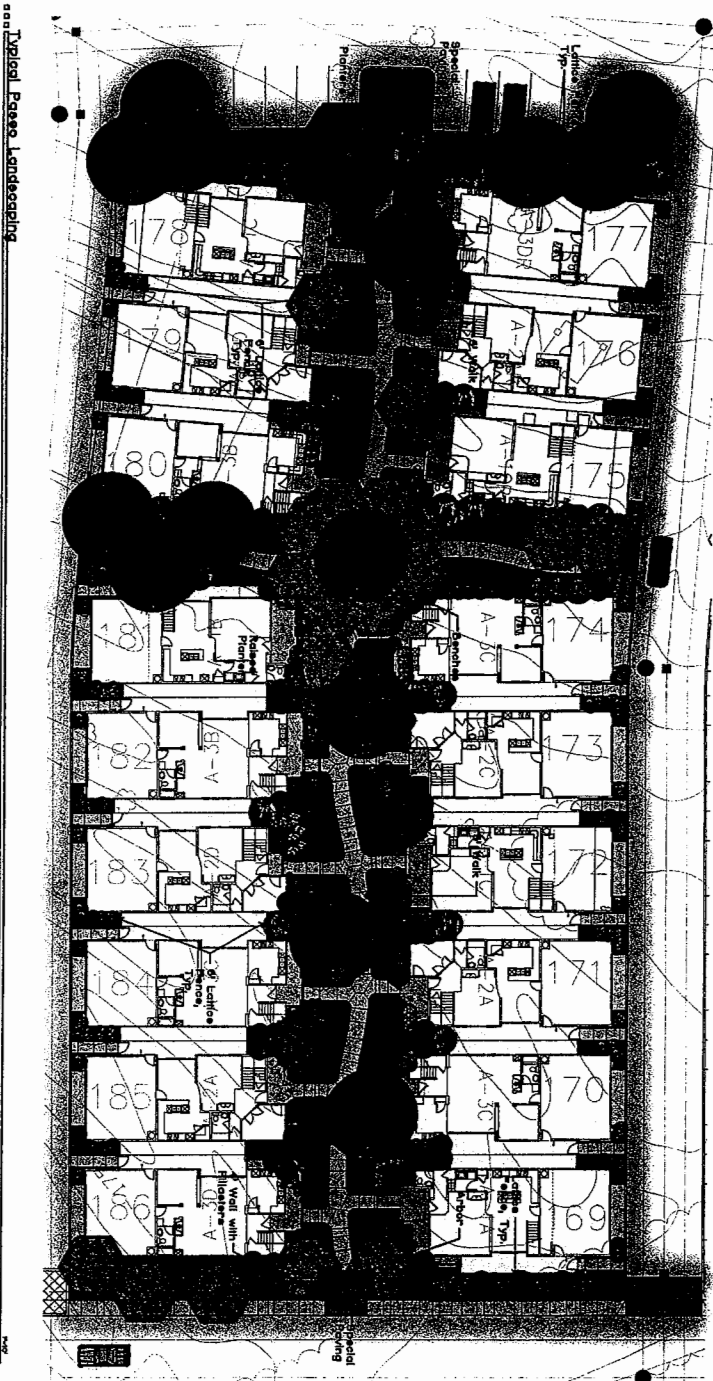
VESTING TENTATIVE MAP
EROSION CONTROL PLAN
ROLLINGWOOD
CITY OF VALLEJO, SOLANO COUNTY, CALIFORNIA

Ruggeri -
Jensen
Jazz & Associates
1880 CHERRY DRIVE, SUITE 200 • PLEASANTON, CA 94566
PHONE: (925) 297-9300 • FAX: (925) 297-8300



Planting Legend

- 1. LARGE TREE: 12" DBH, 30' H, 12" CALIBER
 2. MEDIUM TREE: 8" DBH, 20' H, 8" CALIBER
 3. SMALL TREE: 4" DBH, 10' H, 4" CALIBER
 4. SHRUB: 6' H, 4" CALIBER
 5. BUSH: 4' H, 4" CALIBER
 6. PALM: 10' H, 4" CALIBER
 7. TALL GRASS: 6' H
 8. MEDIUM GRASS: 4' H
 9. SHORT GRASS: 2' H
 10. LAWN
 11. DRIVEWAY
 12. SIDEWALK
 13. PARKING SPACE
 14. DRIVEWAY
 15. SIDEWALK
 16. PARKING SPACE
 17. DRIVEWAY
 18. SIDEWALK
 19. PARKING SPACE
 20. DRIVEWAY
 21. SIDEWALK
 22. PARKING SPACE
 23. DRIVEWAY
 24. SIDEWALK
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 27. SIDEWALK
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 94. PARKING SPACE
 95. DRIVEWAY
 96. SIDEWALK
 97. PARKING SPACE
 98. DRIVEWAY
 99. SIDEWALK
 100. PARKING SPACE



Typical Paved Landscaping



Typical Central Common Area

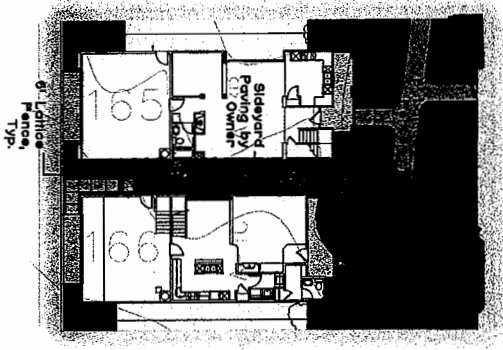


Bench

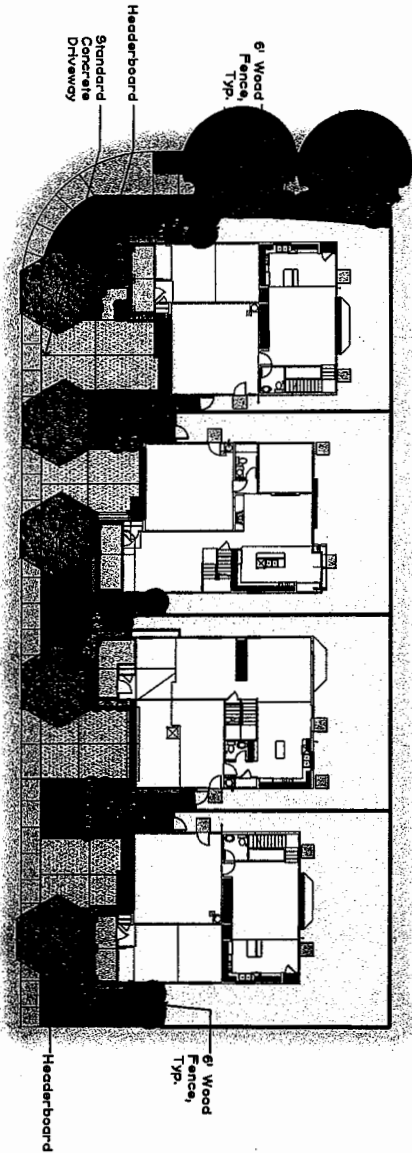


Planter

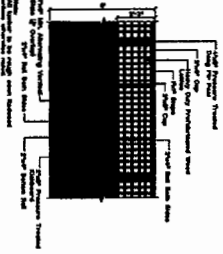
3' 6" Size Unit



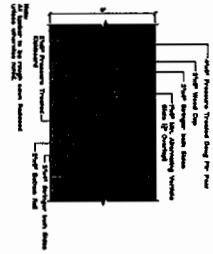
000 Alley-lead Sideyard by Owner 1/2" = 1'-0"



000 Typical Front-lead Streetscape 1/2" = 1'-0"



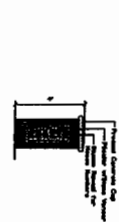
000 Alley Lead Wood Fence 1/4" = 1'-0"



000 Front Lead Wood Fence 1/4" = 1'-0"



000 Cluster Mailbox 1/4" = 1'-0"



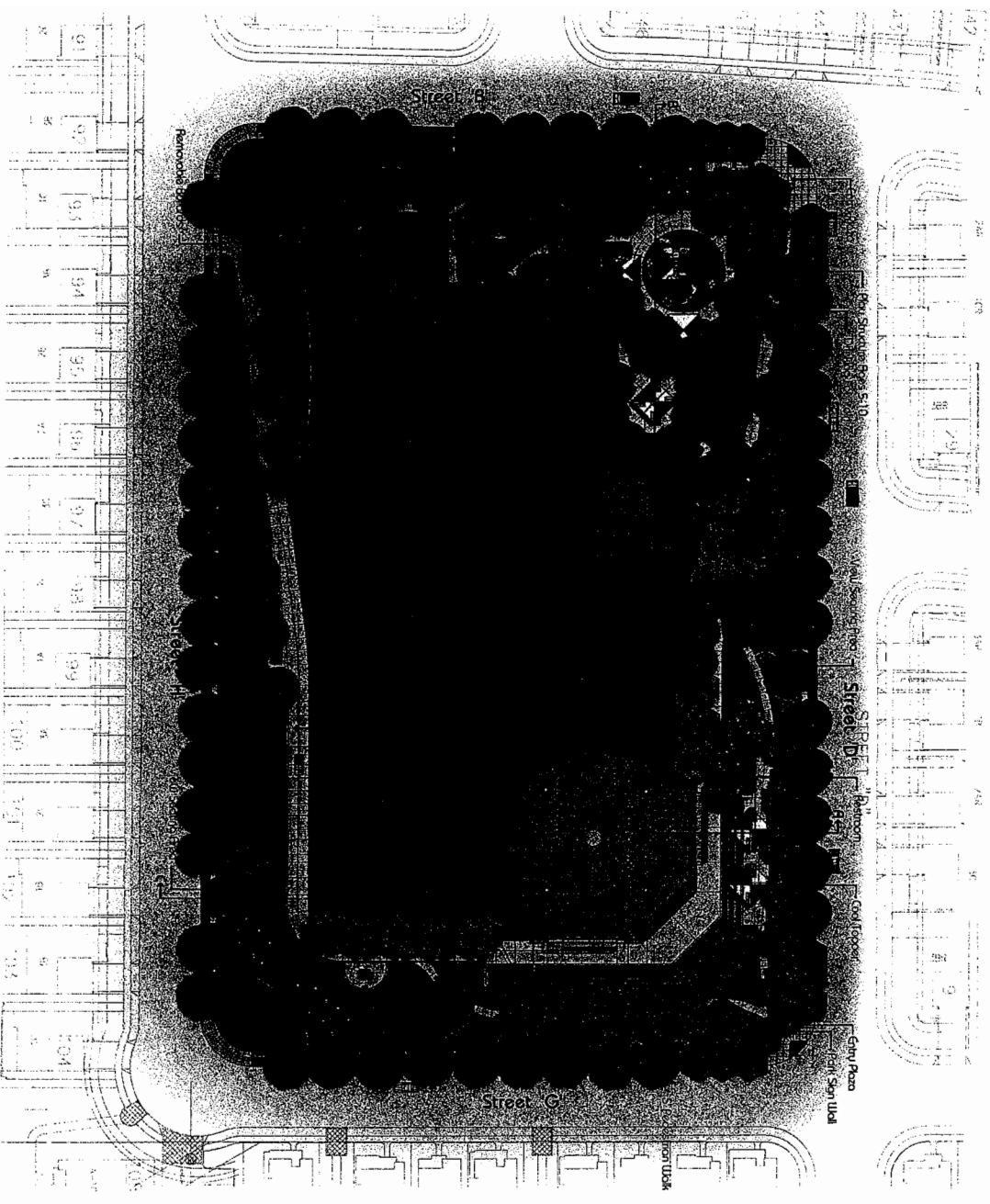
000 Address Piliaster 1/4" = 1'-0"

Planting Legend

- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE
- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
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 NORWAY SPRUCE
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 BIRD PLANTING
 CHERRY LARCH
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- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
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 NORWAY SPRUCE
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 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
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- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
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- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
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- 48' BIRD**
 BIRD PLANTING
 CHERRY LARCH
 CHERRY PINE
 EUROPEAN LARCH
 NORWAY SPRUCE
 NORWAY SPRUCE
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 NORWAY SPRUCE
 NORWAY SPRUCE
 NORWAY SPRUCE

Notes

1. All plantings are to be installed within the first 90 days of the start of construction of the City.
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12. All plantings are to be installed within the first 90 days of the start of construction of the City.
13. All plantings are to be installed within the first 90 days of the start of construction of the City.
14. All plantings are to be installed within the first 90 days of the start of construction of the City.
15. All plantings are to be installed within the first 90 days of the start of construction of the City.



Rollinswood Park
 Preliminary Landscape Plan



000 Section A-A

2'-0"



000 Section B-B

2'-0"



000 Playstructure

N/A



000 Section C-C

2'-0"



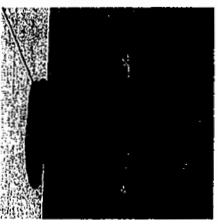
000 Dulmor Recycled Plastic Park Bench

N/A



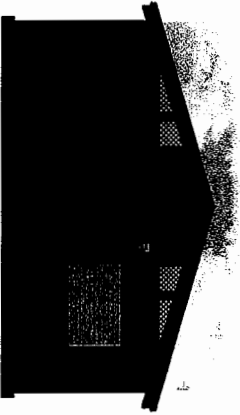
000 Dulmor Recycled Plastic Picnic Table

N/A



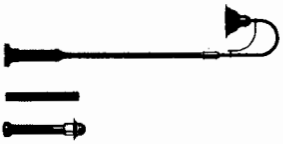
000 Trash Receptacle

N/A



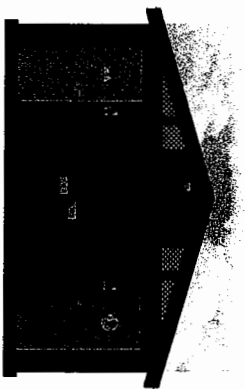
000 Congestion Sign

1/4" = 1'-0"



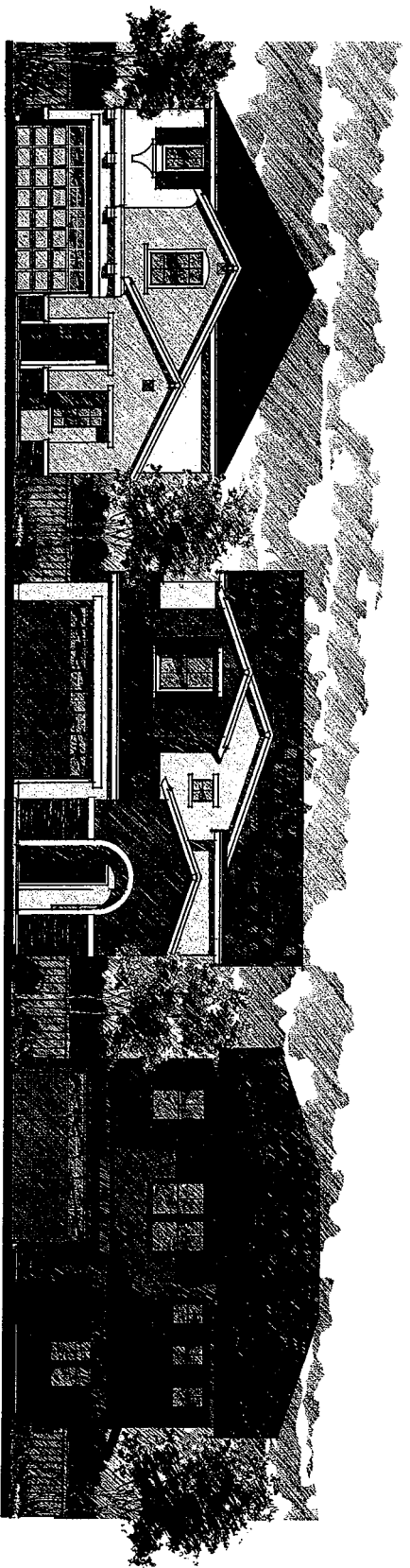
000 Lightpost and Ballerds

N/A



000 Restroom

1/4" = 1'-0"



Plan 1A

Plan 2B

Plan 3C

Standard Product
Rollingwood
Vallejo, California



300 Archittoria, Inc.
925834, 7000
00 August 07

EXTERIOR MATERIALS

A ELEVATIONS

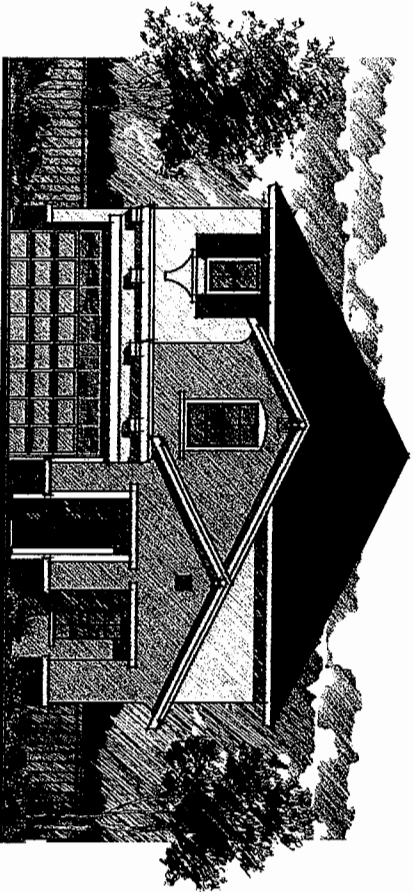
Flat concrete tile roofing
 Gables & hip rock
 Stone veneer
 Decorative stucco over flem cobble
 Rock-up garage doors with optional glass
 Stone veneer

B ELEVATIONS

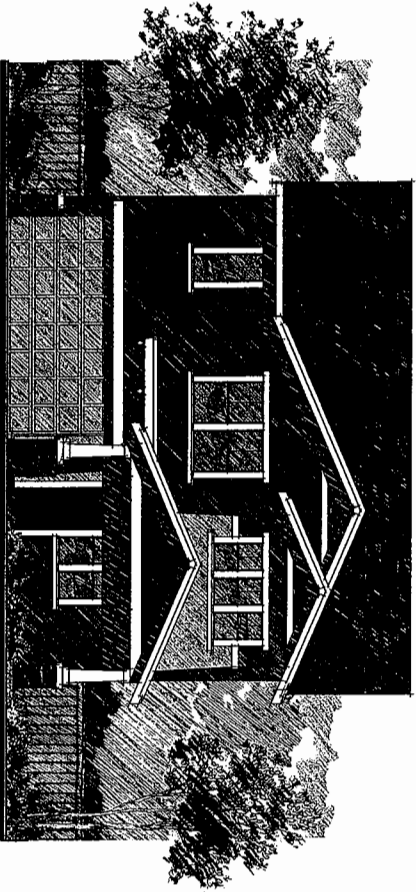
Flat concrete tile roofing
 Gables & shed rock
 Stucco veneer finish
 Decorative stucco over flem cobble
 Rock-up garage doors with optional glass
 Decorative wrought iron accents
 Stone veneer

C ELEVATIONS

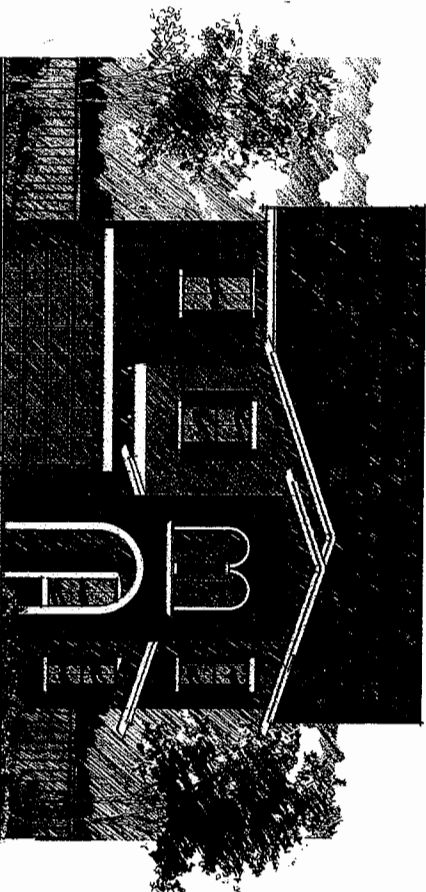
Flat concrete tile roofing
 Gables rock
 Stucco veneer finish w/ wood siding accents
 Decorative stucco over flem sills & trim
 Rock-up garage doors with optional glass
 Wood cobble
 Stone veneer



Elevation A



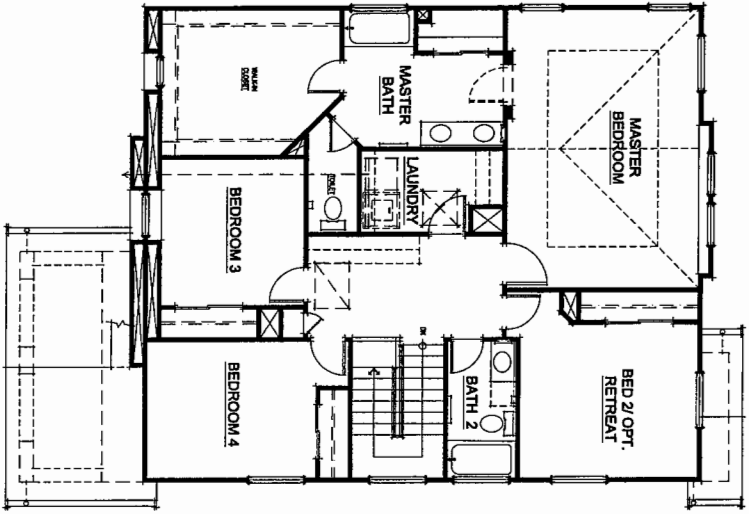
Elevation C



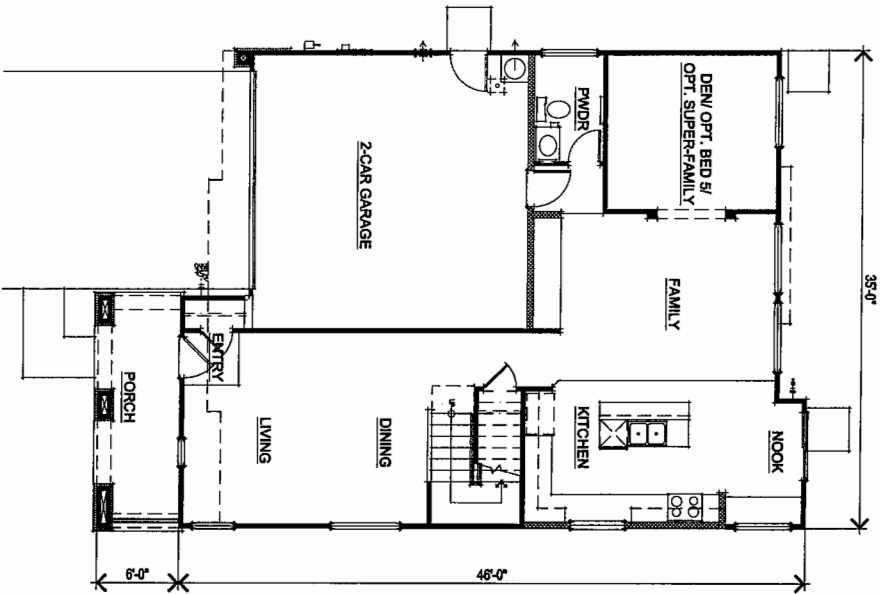
Elevation B



PLAN 1
STANDARD LOT PRODUCT
R O L I N G W O O D
 V A L I E J O . C A L I F O R N I A



SECOND FLOOR PLAN



FIRST FLOOR PLAN

SQUARE FOOTAGE	
FIRST FLOOR:	1085 SQ. FT.
SECOND FLOOR:	1887 SQ. FT.
TOTAL:	2972 SQ. FT.
2-CAR GARAGE:	414 SQ. FT.

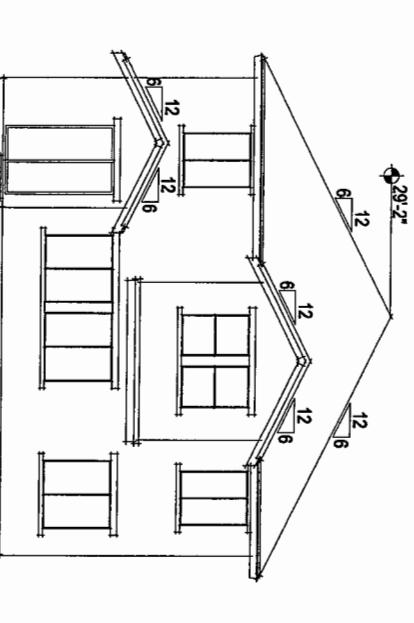
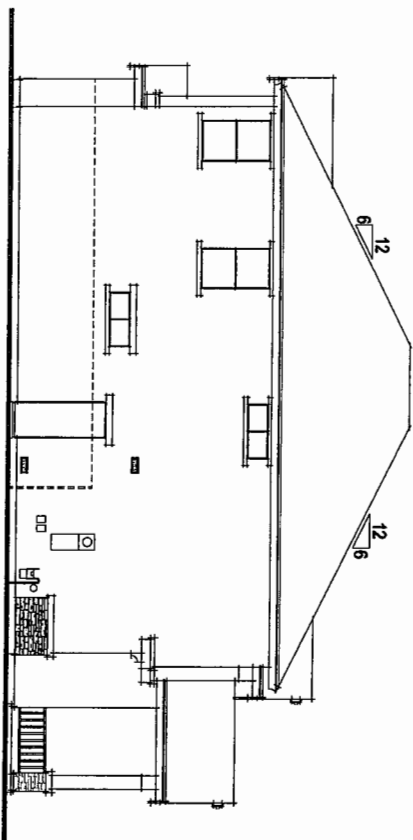
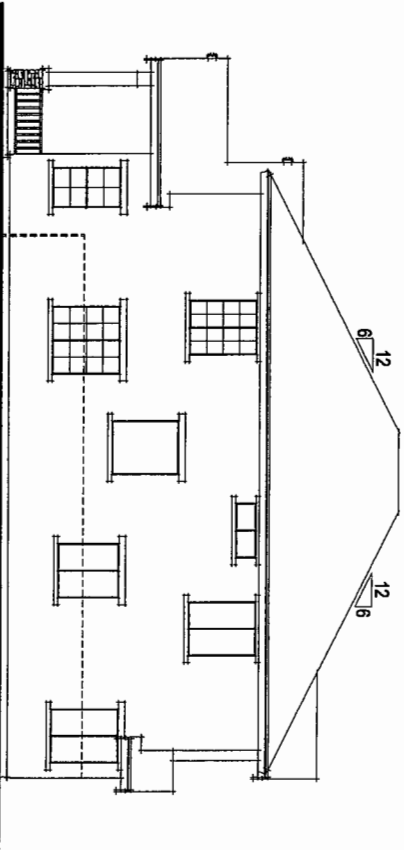
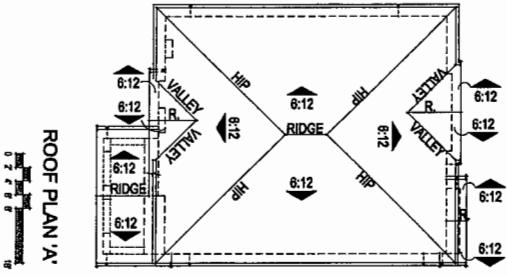
PLAN 1

STANDARD LOT PRODUCT

R O L I N W O O D

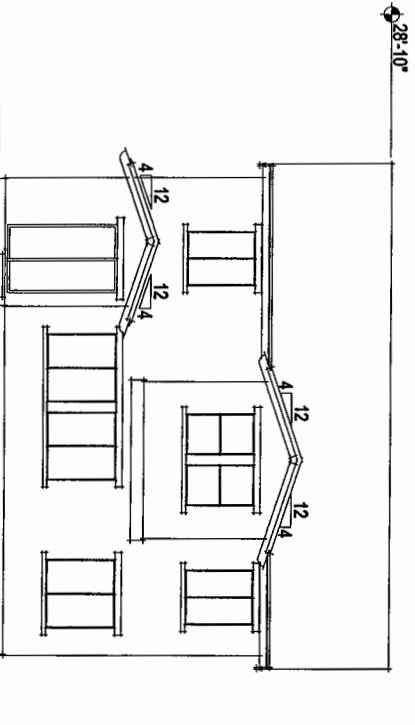
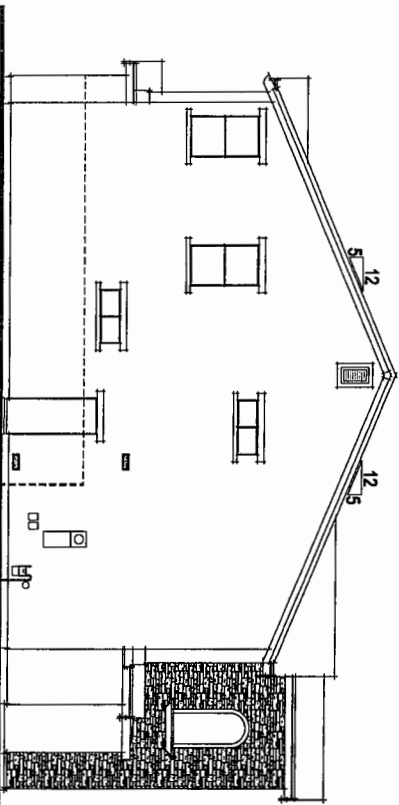
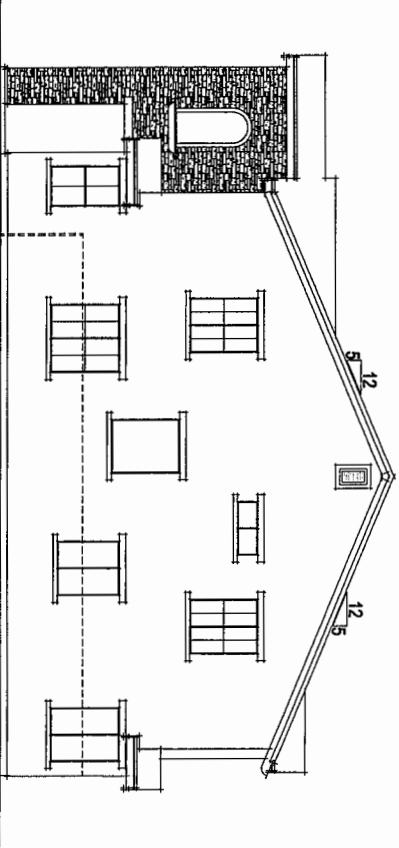
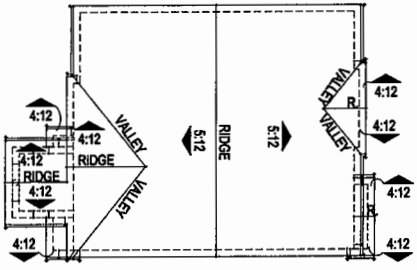
V A I L E J O, C A L I F O R N I A

9 0 6 A R C H I T E C T S, I N C.
 925.834.7009
 08 August 07



PLAN 1
STANDARD LOT PRODUCT
R o l l i n g w o o d
v a l l e y , c a l i f o r n i a

S D G A F S O N L I F E S I E . I N C .
920-834-7800
00 August 07



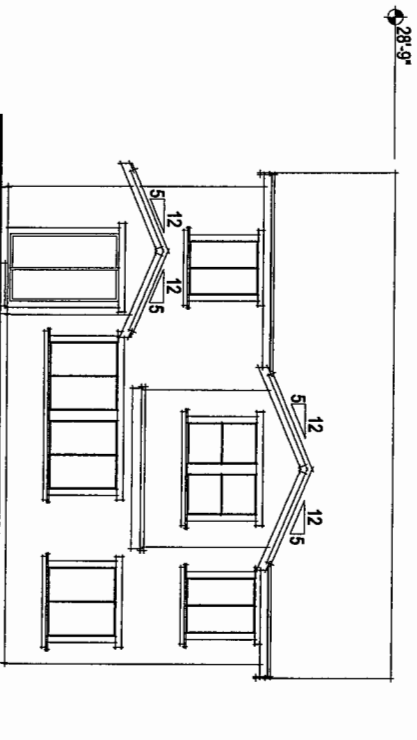
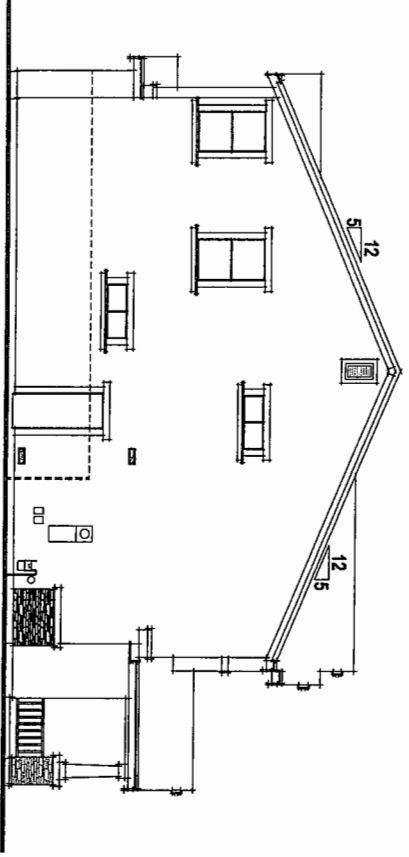
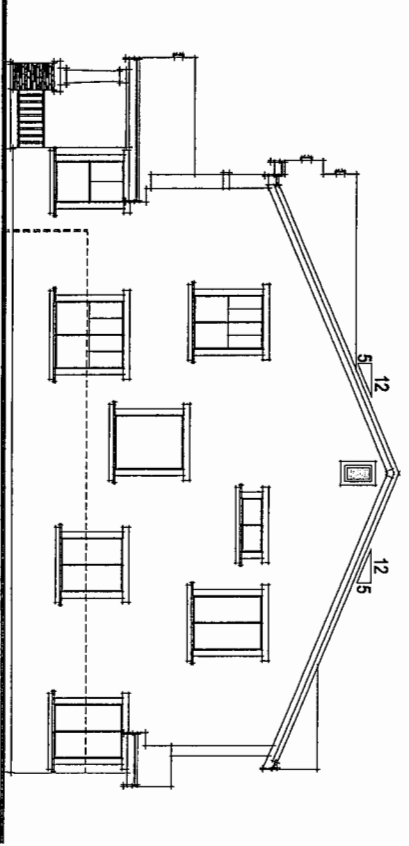
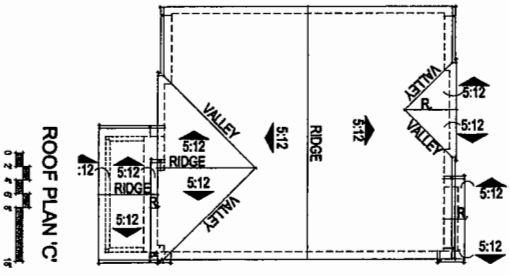
PLAN 1
STANDARD LOT PRODUCT

R O L I I N G W O O D

V A I L I O, C A L I F O R N I A



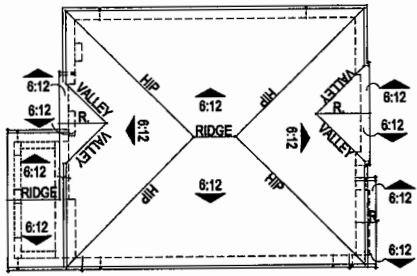
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5 5 A 2 5 2 1 9 7



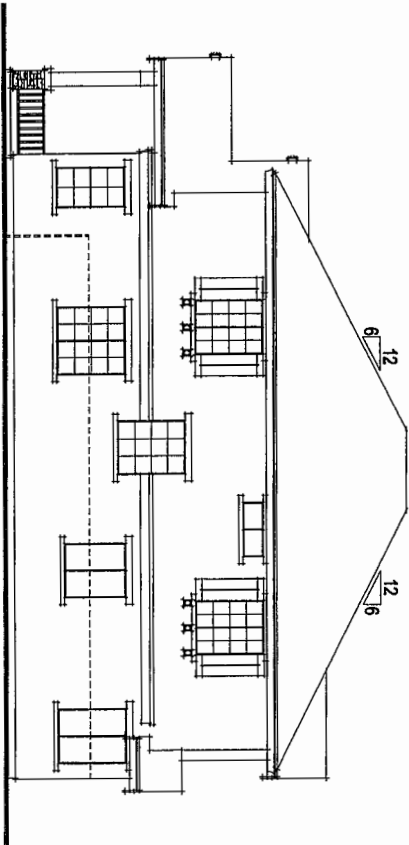
PLAN 1
STANDARD LOT PRODUCT

R O I I I N G W O O D
V A I L E J O , C A L I F O R N I A

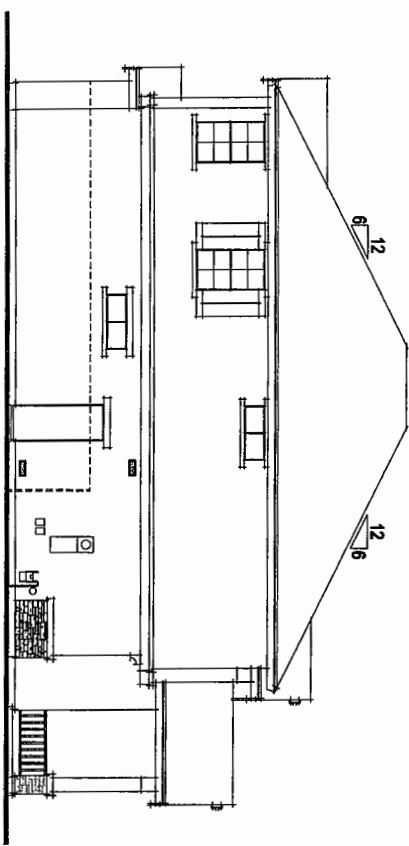
S D G A R C H I T E C T S , I N C.
828.834.7888
08 August 07



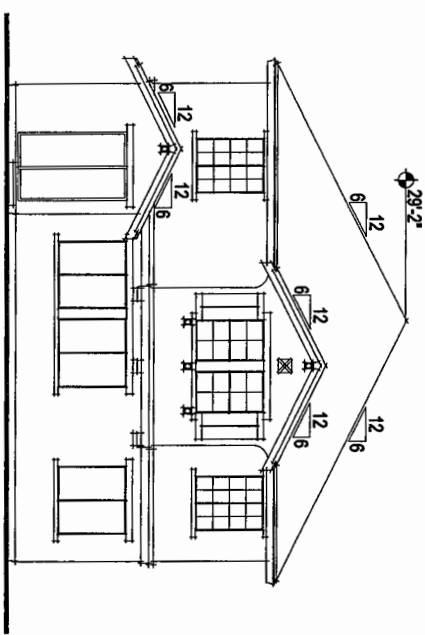
ROOF PLAN 'A'



RIGHT ELEVATION 'A'



LEFT ELEVATION 'A'

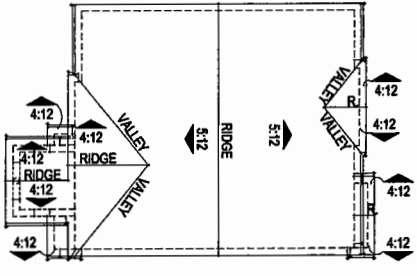


REAR ELEVATION 'A'

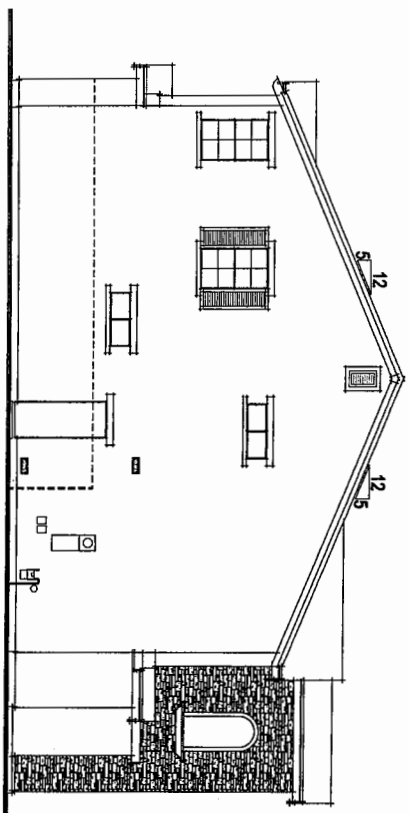
PLAN 1
 STANDARD LOT PRODUCT
 ENHANCED ELEVATIONS WHERE
 LOT IS ADJACENT TO ROADWAY

R O I L I N G W O O D
 V a l l e j o, C a l i f o r n i a

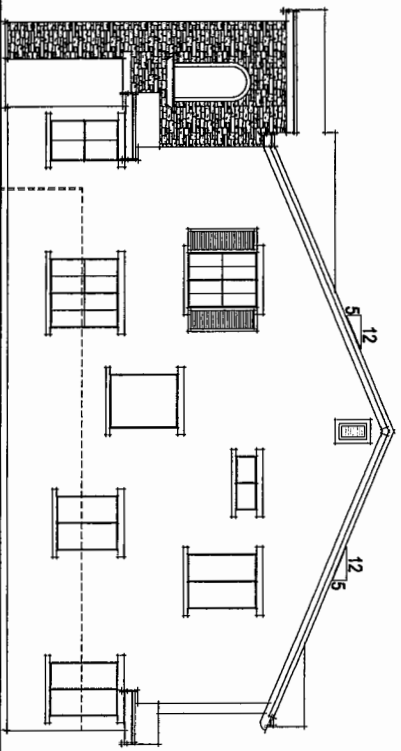
800 ARSHLINS, INC.
 928.834.7000
 08 August 07



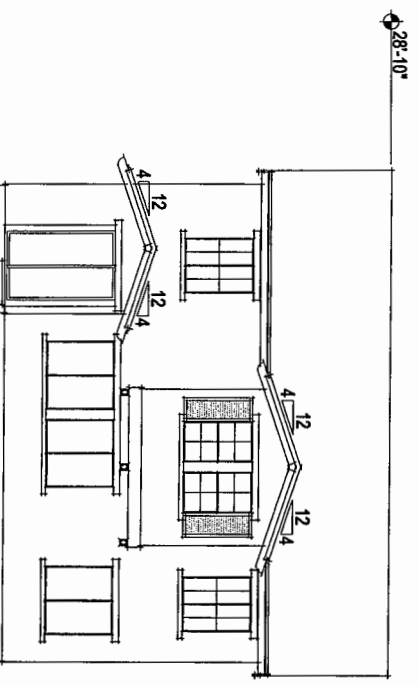
ROOF PLAN 'B'
0 2' 4' 6' 8' 10'



LEFT ELEVATION 'B'



RIGHT ELEVATION 'B'



REAR ELEVATION 'B'

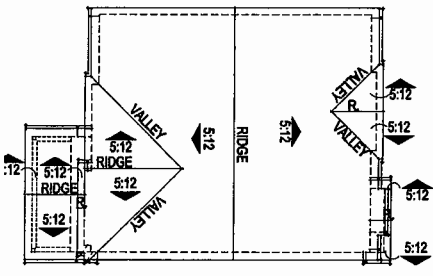
PLAN 1
STANDARD LOT PRODUCT
ENHANCED ELEVATIONS WHERE
LOT IS ADJACENT TO ROADWAY

R O I I I n g W O O D

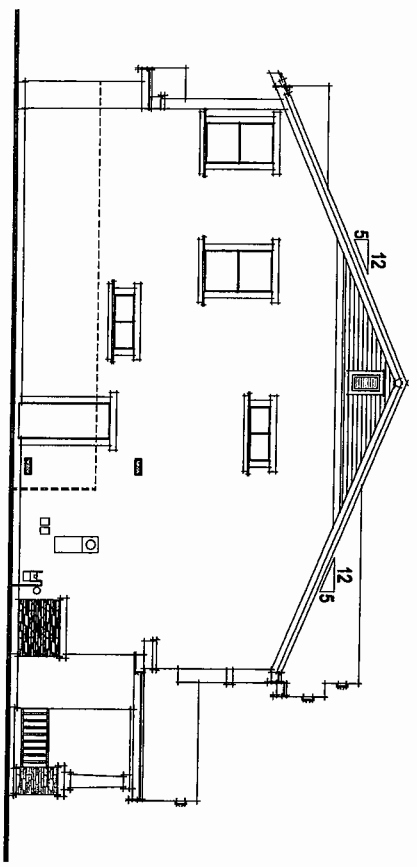
Vallejo, California

800 A rchitectural, Inc.
238-834-7888
08 August 07

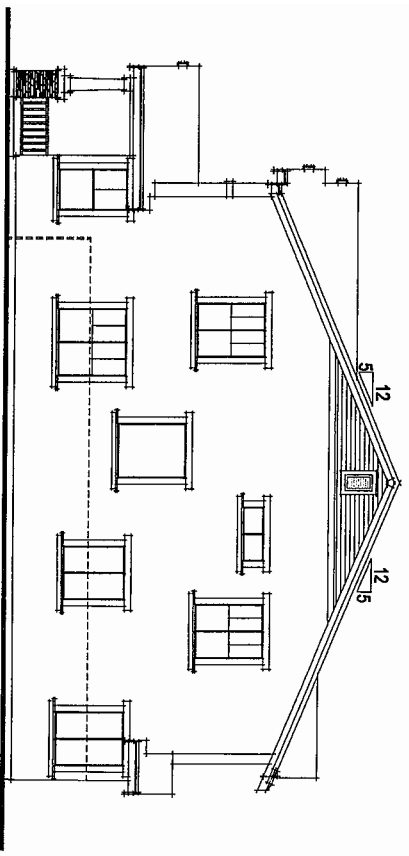




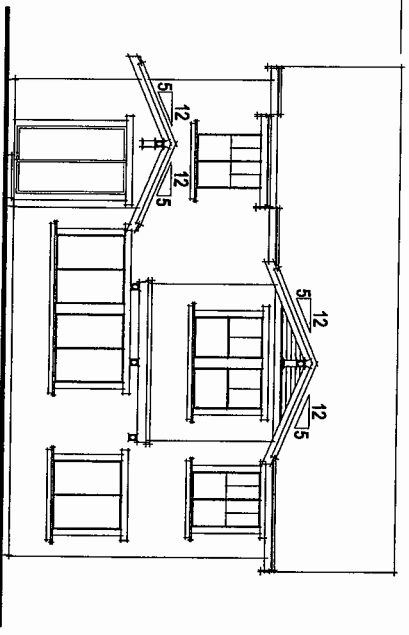
ROOF PLAN 'C'



LEFT ELEVATION 'C'



RIGHT ELEVATION 'C'



REAR ELEVATION 'C'

PLAN 1
 STANDARD LOT PRODUCT
 ENHANCED ELEVATIONS WHERE
 LOTS ADJACENT TO ROADWAY

R o l l i n g w o o d
 V a l l e y , C a l i f o r n i a

S D G A R S H I L L S I N C .
 525.834.7888
 08 August 07



EXTERIOR MATERIALS

A ELEVATIONS

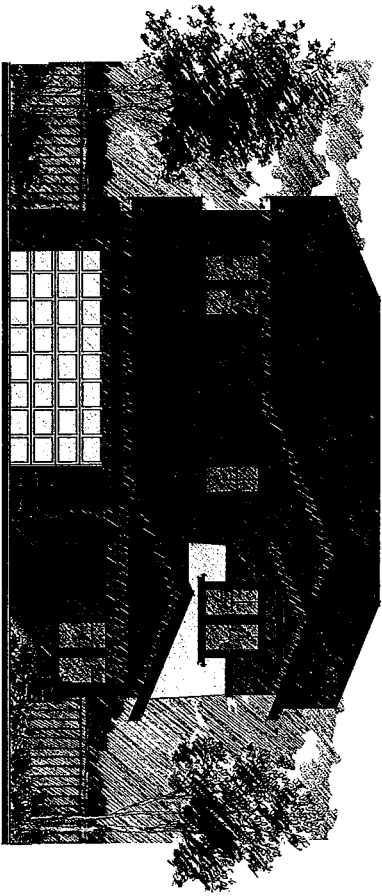
Flat concrete tile roofing
 Gable rock
 Stucco exterior finish
 Decorative stucco over beam sills & trim
 Roll-up garage doors with optional glass
 Stone veneer

B ELEVATIONS

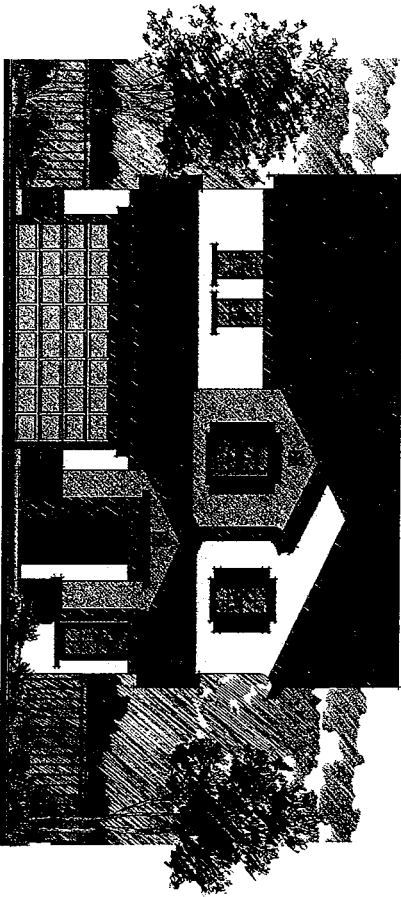
Flat concrete tile roofing
 Gable & shed rock
 Stucco exterior finish
 Decorative stucco over beam sills & trim
 Roll-up garage doors with optional glass
 Stone veneer

C ELEVATIONS

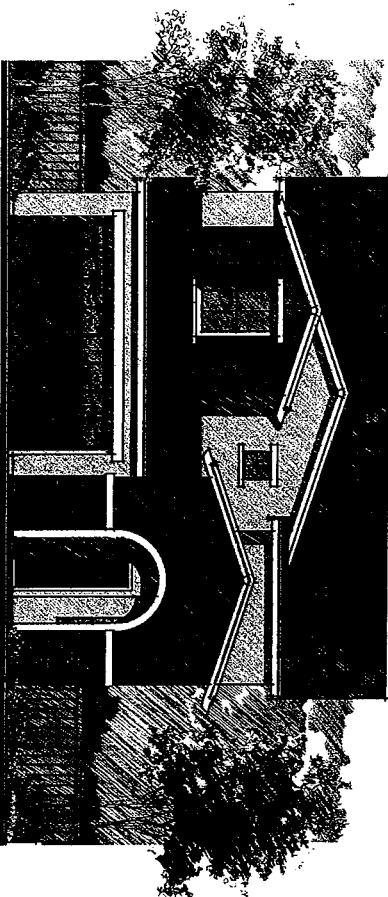
Flat concrete tile roofing
 Gable rock
 Stucco exterior finish w/ wood siding accents
 Decorative stucco over beam sills & trim
 Wood garage doors with optional glass
 Stone veneer



Elevation C



Elevation A



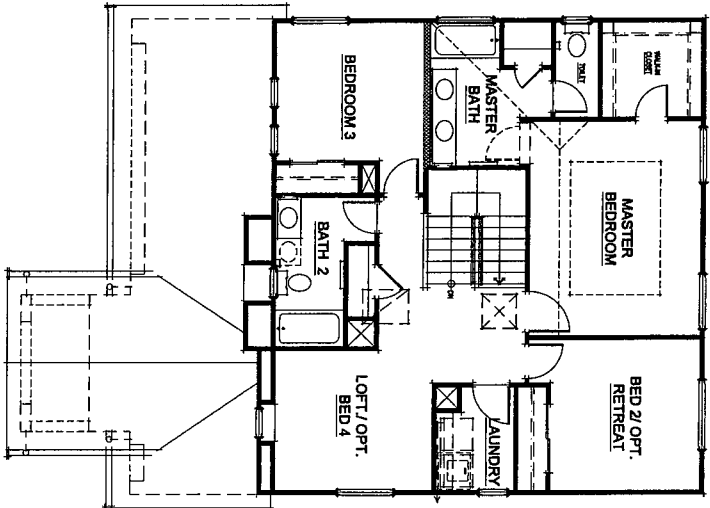
Elevation B

PLAN 2
STANDARD LOT PRODUCT
R O L I N G W O O D

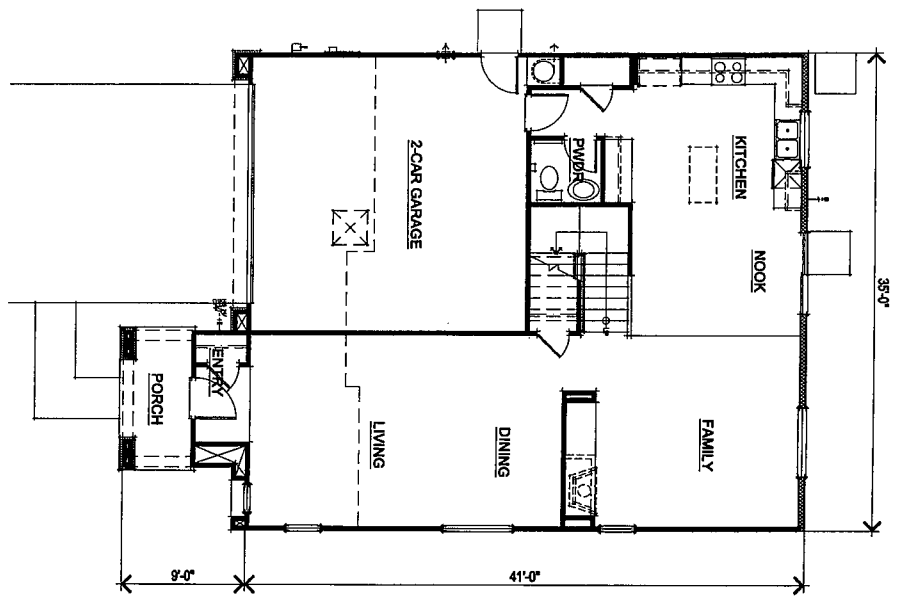
V A L I E J O . C A L I F O R N I A



3 0 0 0 A R I S T O T E L L I P O L I S , I N C.
 9 2 9 . 9 3 4 . 7 9 9 9
 0 6 August 07



SECOND FLOOR PLAN



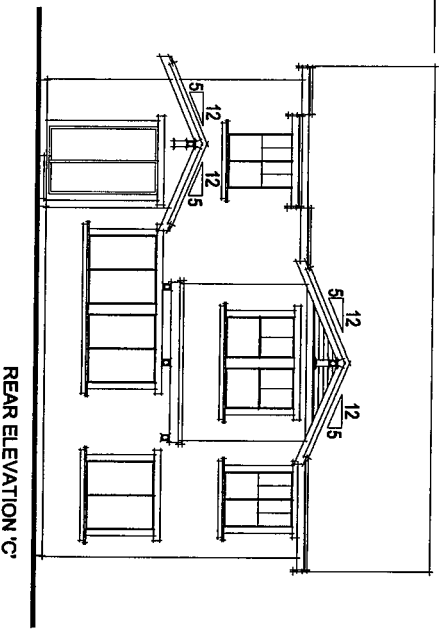
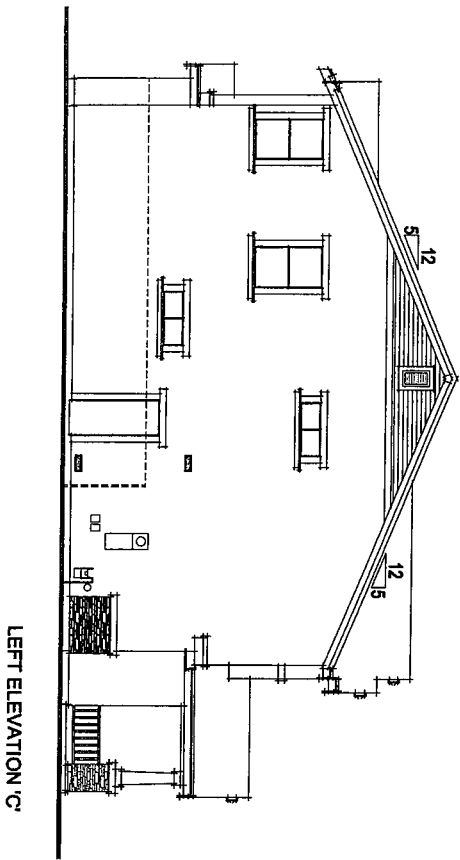
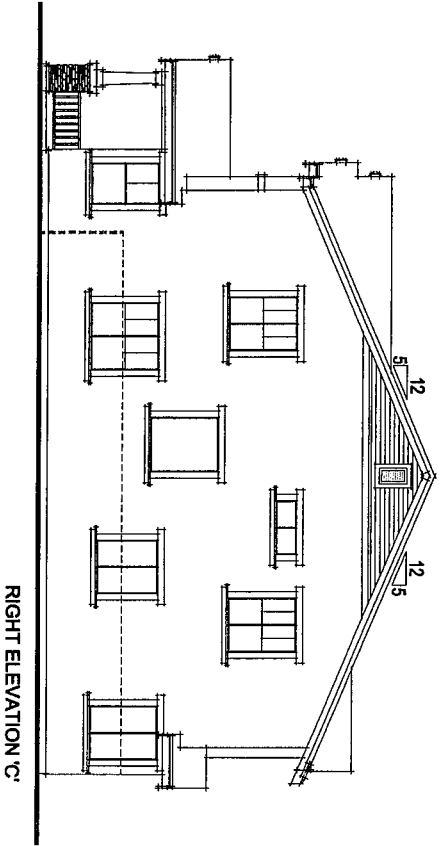
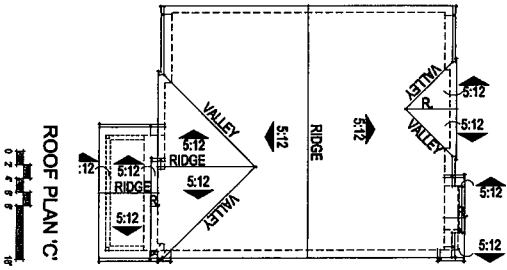
FIRST FLOOR PLAN

SQUARE FOOTAGE	
FIRST FLOOR:	1047 SQ. FT.
SECOND FLOOR:	1044 SQ. FT.
TOTAL:	2091 SQ. FT.
2-CAR GARAGE:	421 SQ. FT.

PLAN 2
STANDARD LOT PRODUCT

R o l l i n g w o o d
V a l i e j o , C a l i f o r n i a

S D G A r c h i t e c t s , I n c .
925.834.7000
08 August 07



PLAN 1
 STANDARD LOT PRODUCT
 ENHANCED ELEVATIONS WHERE
 LOTS ADJACENT TO ROADWAY

R O I I I N G W O O D
 V a l l e j o, C a l i f o r n i a

8 0 0 A R C H I T E C T S I N C.
 9 2 9 . 2 2 4 . 7 8 8 9
 0 8 A u g u s t 0 7

EXTERIOR MATERIALS

A ELEVATIONS

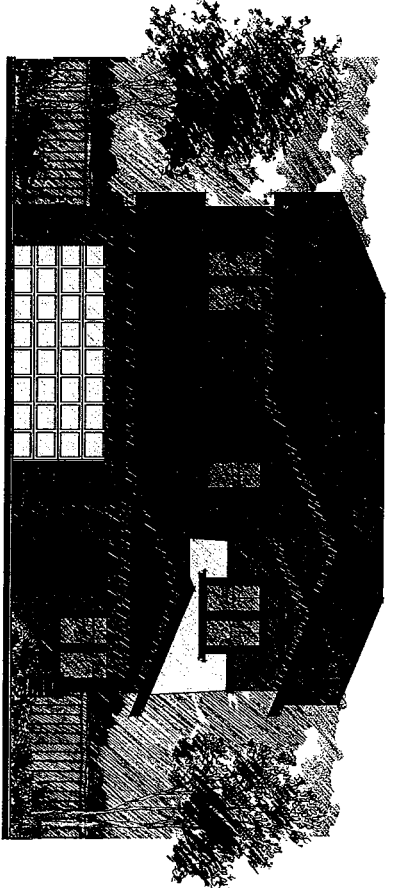
Flat concrete tile roofing
 Calico masonry
 Stucco exterior finish
 Decorative stucco over beam corbels
 Roll-up garage doors with optional glass
 Stone veneer

B ELEVATIONS

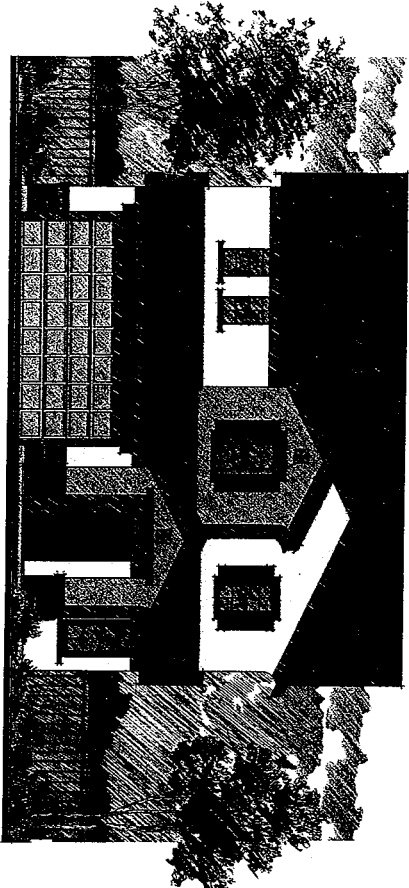
Flat concrete tile roofing
 Slate & steel roof
 Stucco exterior finish
 Decorative wrought iron accents
 Roll-up garage doors with optional glass
 Stone veneer

C ELEVATIONS

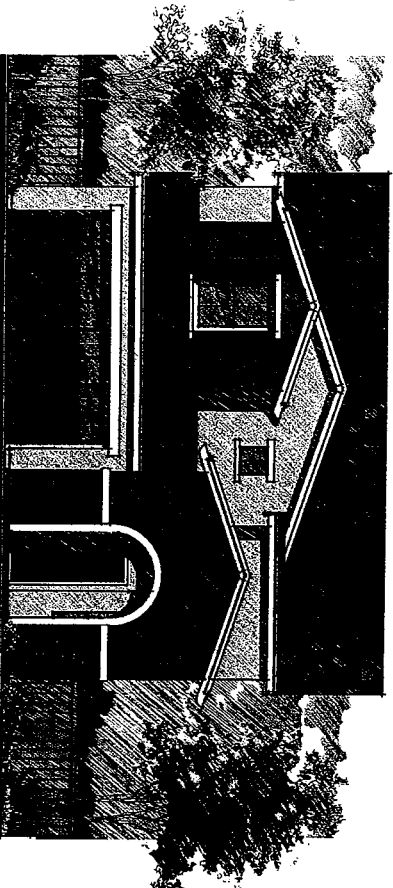
Flat concrete tile roofing
 Slate roof
 Stucco exterior finish w/ wood shingle accents
 Roll-up garage doors with optional glass
 Wood corbels
 Stone veneer



Elevation C



Elevation A



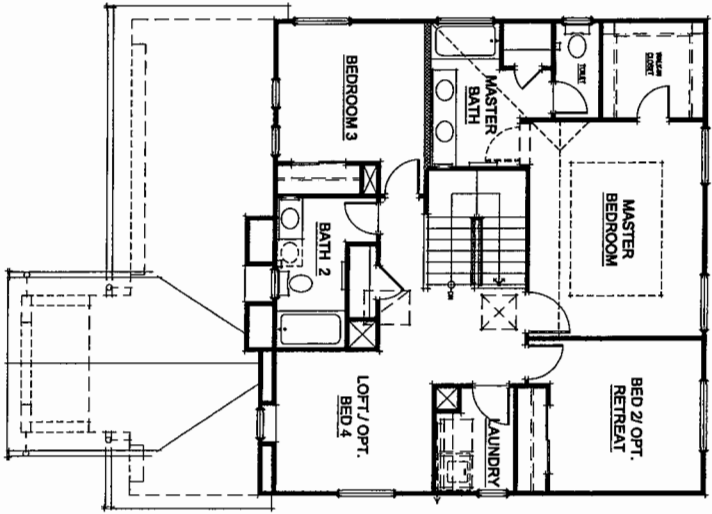
Elevation B

**PLAN 2
 STANDARD LOT PRODUCT**

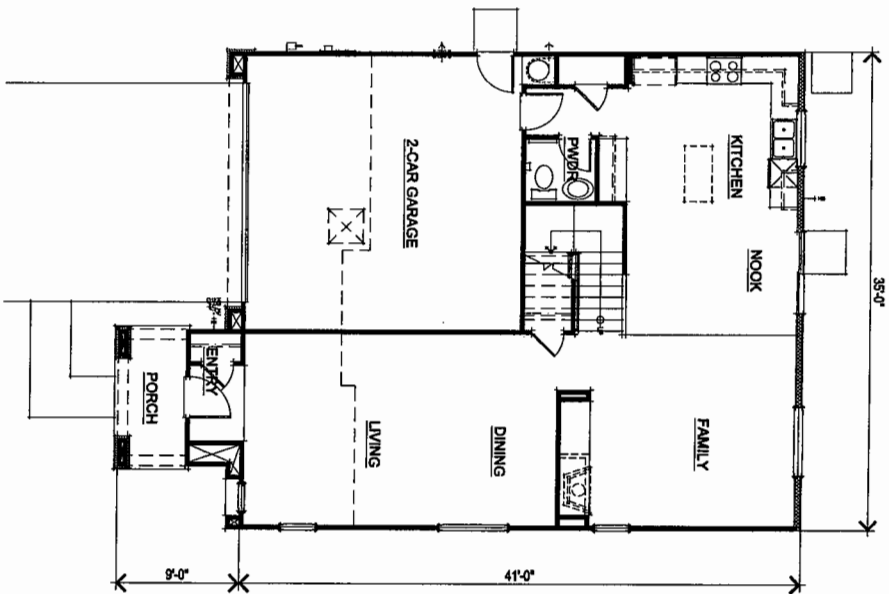
**R O I L I N G W O O D
 V A L L E J O, C A L I F O R N I A**

300 Archillect, Inc.
 328, 334, 336
 06 August 07





SECOND FLOOR PLAN



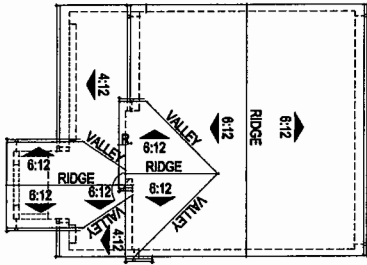
FIRST FLOOR PLAN

SQUARE FOOTAGE	
FIRST FLOOR:	1047 SQ. FT.
SECOND FLOOR:	1084 SQ. FT.
LAUNDRY:	2111 SQ. FT.
2-CAR GARAGE:	441 SQ. FT.

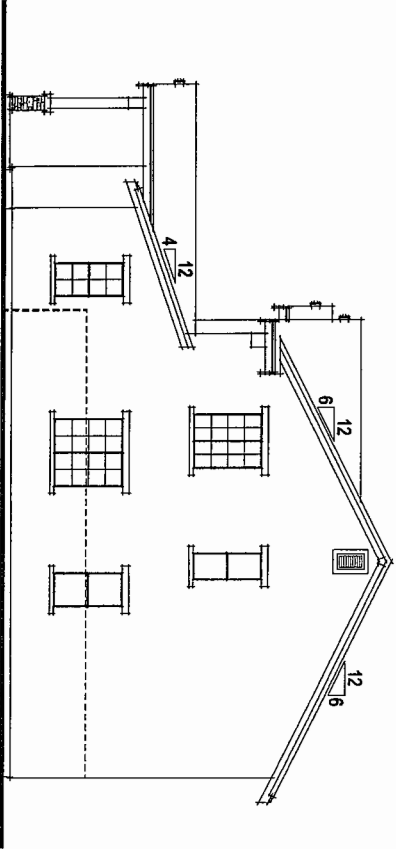
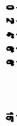
STANDARD LOT PRODUCT
PLAN 2

R O I L I N G W O O D
V A I L I O , C A L I F O R N I A

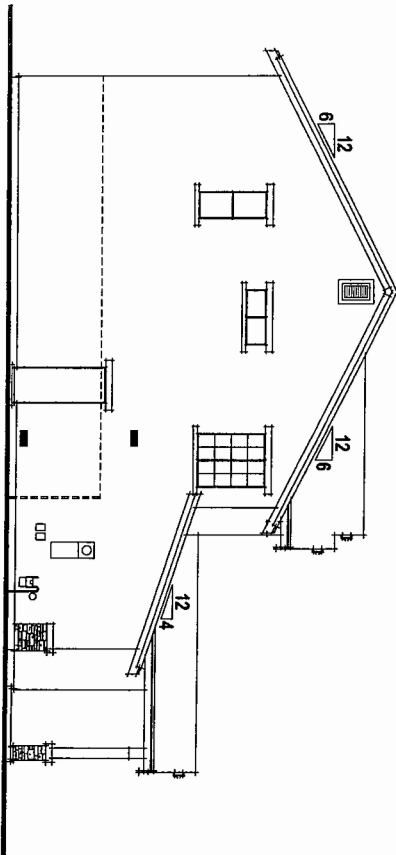
S D G A R C H I T E C T S , I N C.
825.934.7000
08 August 07



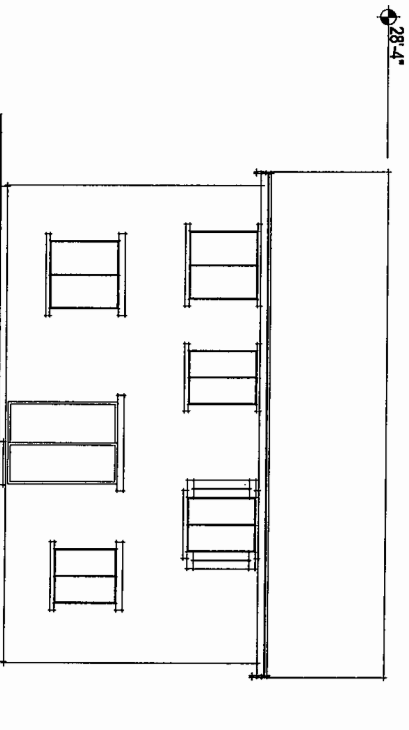
ROOF PLAN 'A'



RIGHT ELEVATION 'A'



LEFT ELEVATION 'A'



REAR ELEVATION 'A'



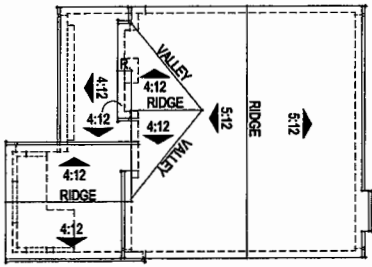
PLAN 2
STANDARD LOT PRODUCT

R O I L I N G W O O D

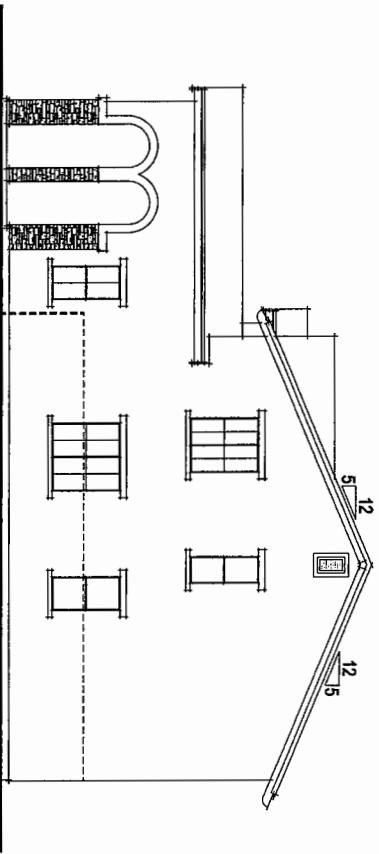
V A I L E J O , C A L I F O R N I A



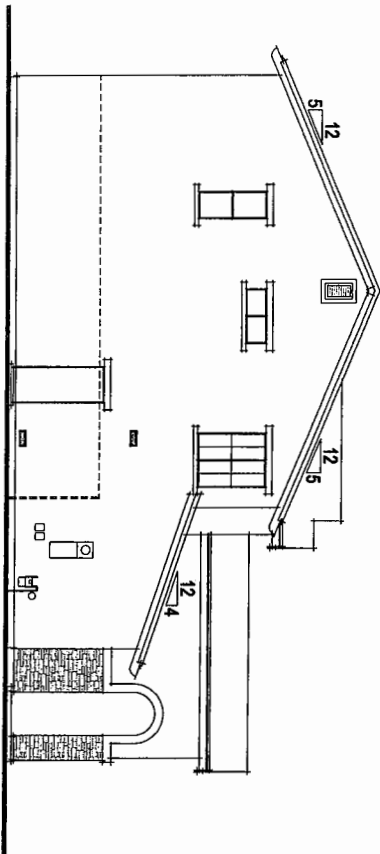
S O G A R C H I T E C T S, I N C.
825.834.7009
08 August 07



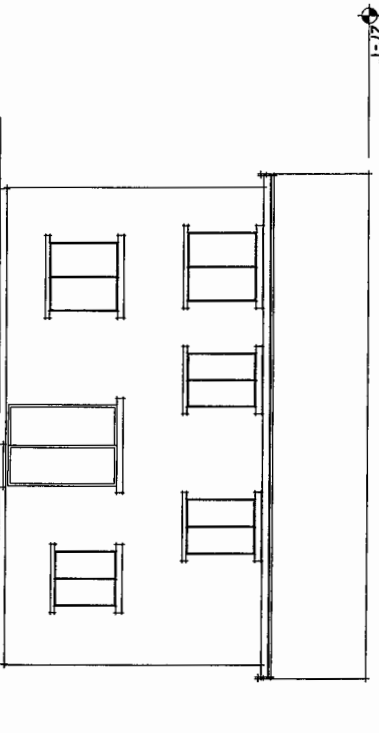
ROOF PLAN 'B'



RIGHT ELEVATION 'B'



LEFT ELEVATION 'B'

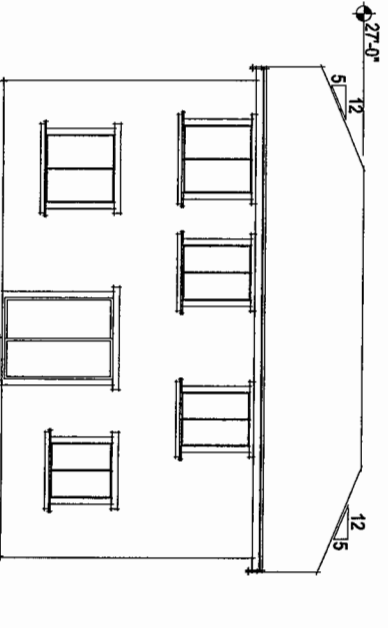
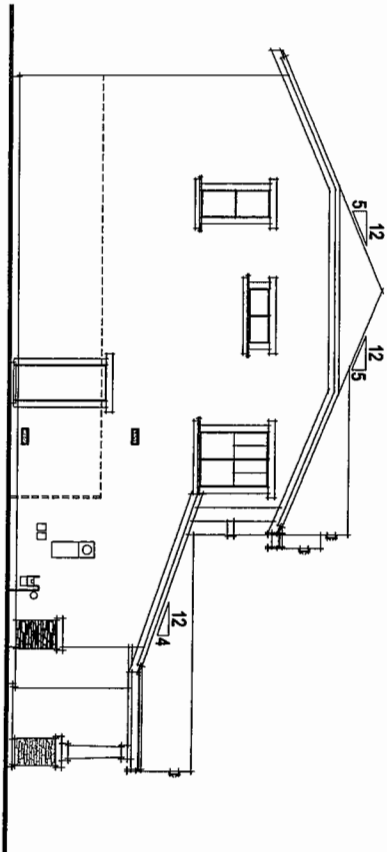
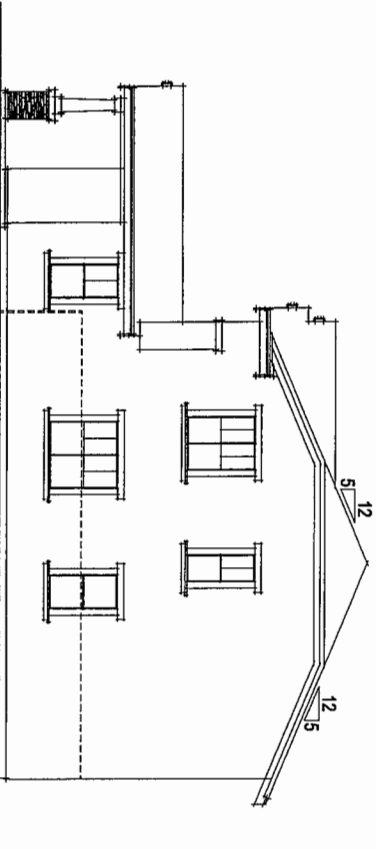
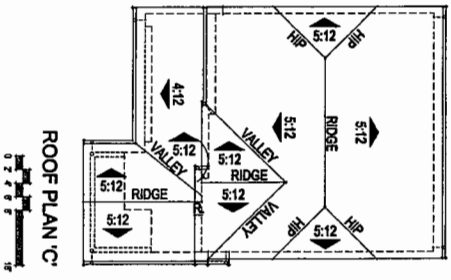


REAR ELEVATION 'B'

PLAN 2
STANDARD LOT PRODUCT

R O I L I N G W O O D
V A I L E J O , C A L I F O R N I A

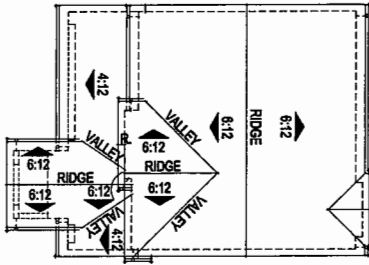
S D G A R C O H I L E S I S . I N C.
925.834.7000
08 August 07



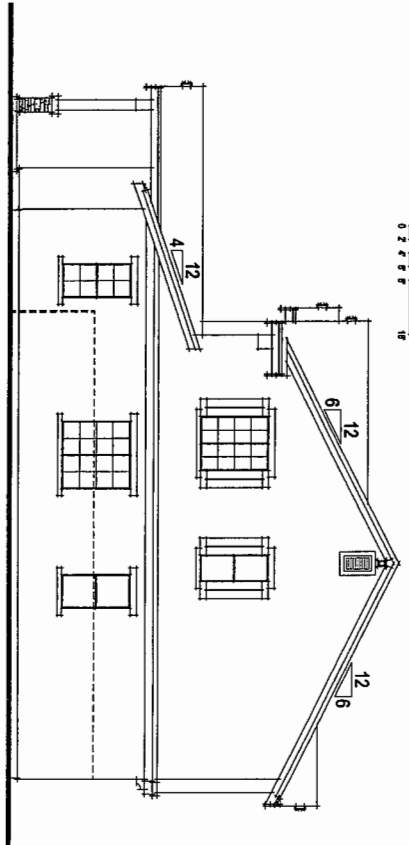
PLAN 2
STANDARD LOT PRODUCT

ROSLINDA WOOD
VALLEJO, CALIFORNIA

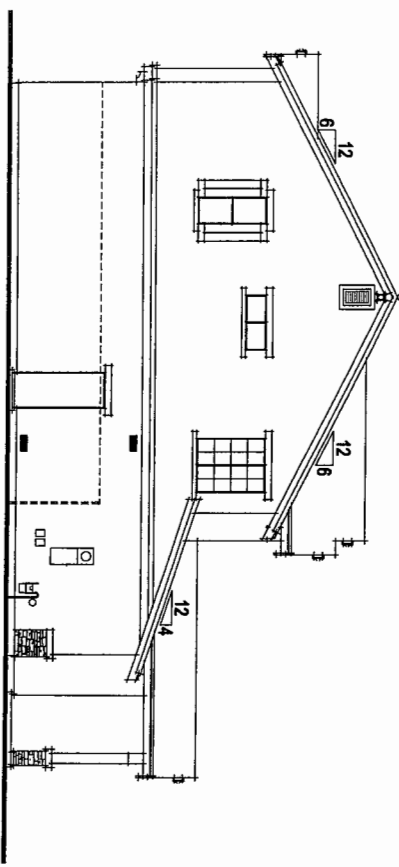
S D G ARCHITECTS, INC.
929.834.7688
08 August 07



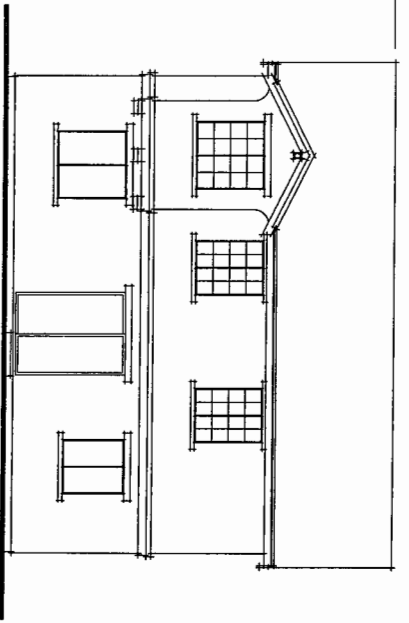
ROOF PLAN 'A'



RIGHT ELEVATION 'A'



LEFT ELEVATION 'A'



REAR ELEVATION 'A'

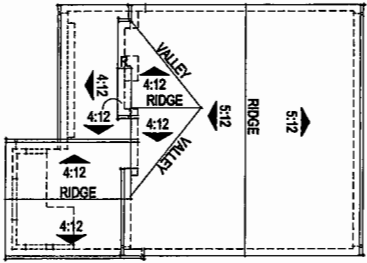


PLAN 2
STANDARD LOT PRODUCT
ENHANCED ELEVATIONS WHERE
LOT IS ADJACENT TO ROADWAY

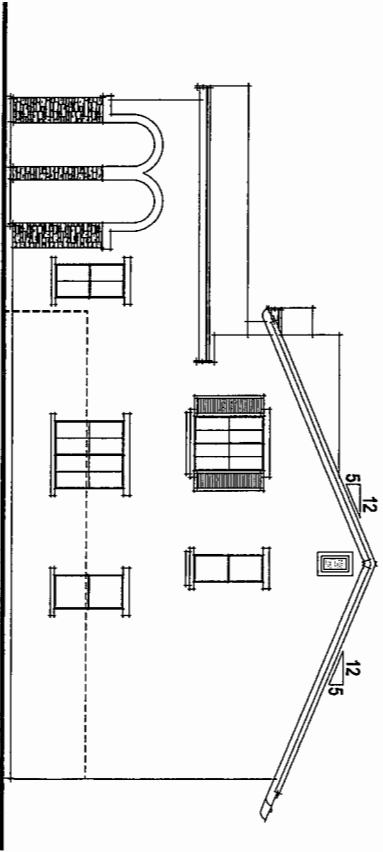
R O I L I N G W O O D

VALLEJO, CALIFORNIA

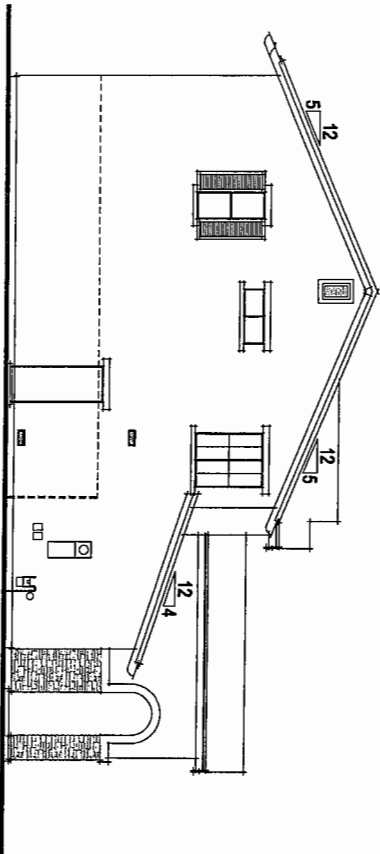
800 ARCHITECTS, INC.
535.934.7000
08 August 07



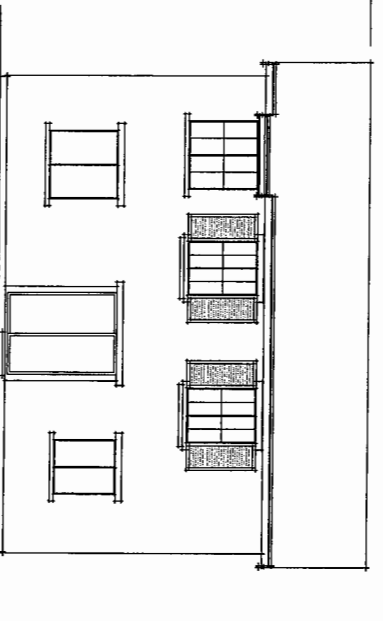
ROOF PLAN 'B'
 0'-0" 10'-0" 20'-0" 27'-1"



RIGHT ELEVATION 'B'



LEFT ELEVATION 'B'



REAR ELEVATION 'B'

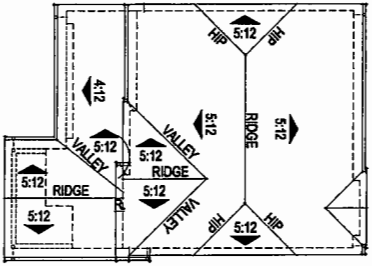
PLAN 2
 STANDARD LOT PRODUCT
 ENHANCED ELEVATIONS WHERE
 LOTS ADJACENT TO ROADWAY

R o l l i n g w o o d

V a l i e j o, C a l i f o r n i a

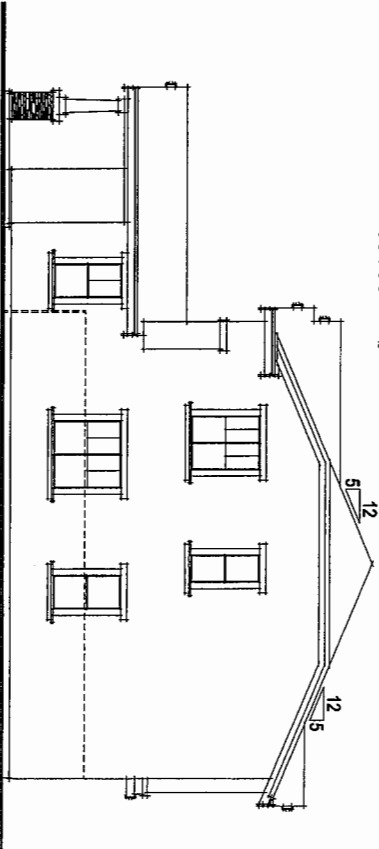


3 0 0 A r c h i t e c t s, I n c.
 6 3 0 . 3 3 4 . 7 0 8 0
 0 7 A u g u s t 0 7

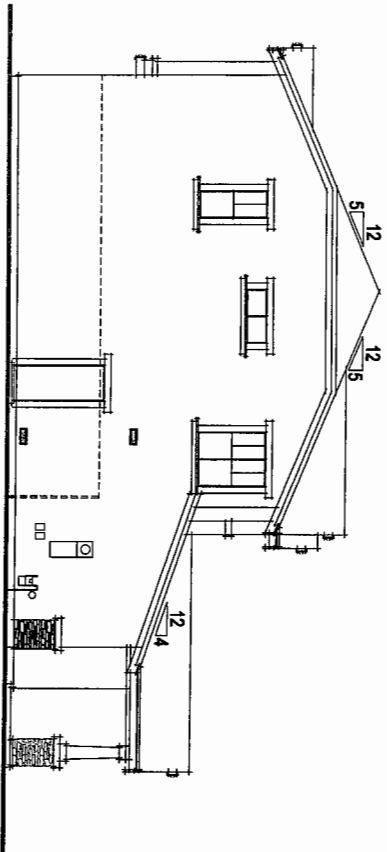


ROOF PLAN 'C'

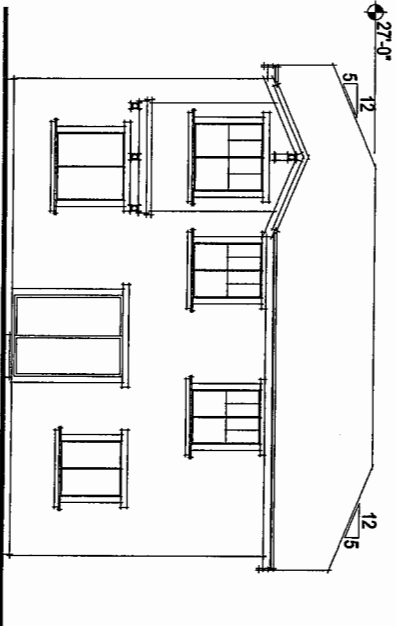
0' 7' 6" 0'



RIGHT ELEVATION 'C'



LEFT ELEVATION 'C'



REAR ELEVATION 'C'

0' 7' 6" 0'

PLAN 2
STANDARD LOT PRODUCT
ENHANCED ELEVATIONS WHERE
LOT IS ADJACENT TO ROADWAY

R O L I I N G W O O D

V A I L E J O , C A L I F O R N I A



8 0 0 A R C H I T E C T S , I N C.
9 2 5 . 5 3 4 . 7 6 6 0
0 0 A u g u s t 0 7

EXTERIOR MATERIALS

A ELEVATIONS

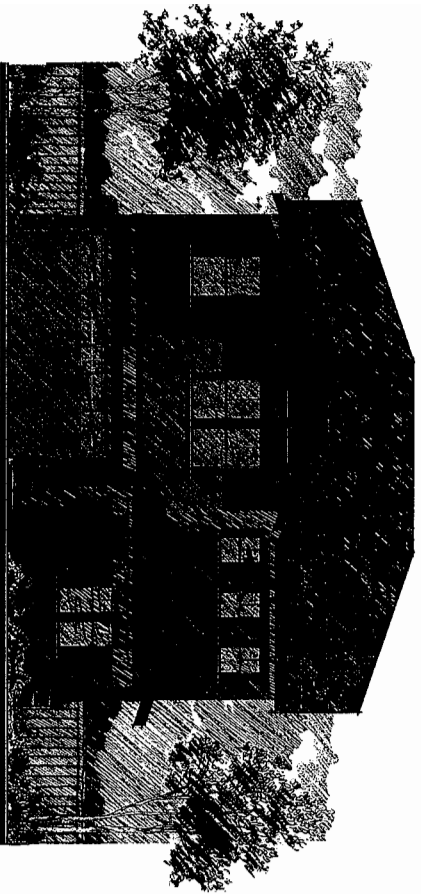
Flat concrete tile roofing
 Gable roofs
 Stone veneer
 Decorative stucco over foam concrete
 Roll-up garage doors with optional glass
 Stone veneer

B ELEVATIONS

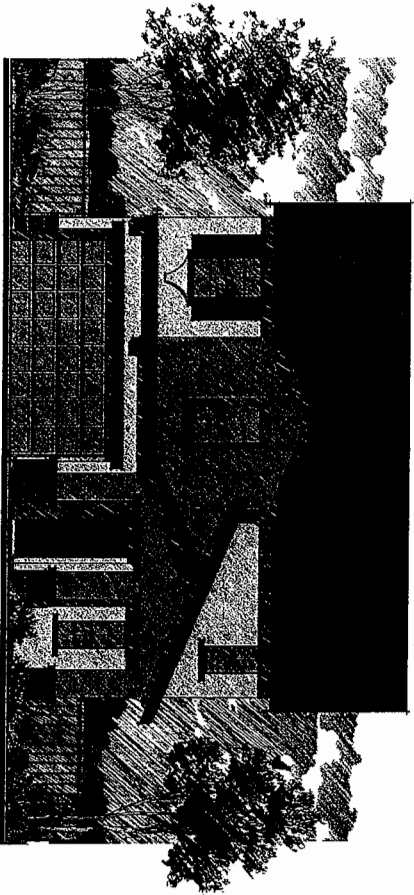
Flat concrete tile roofing
 Gable & shed roofs
 Stucco finish over foam concrete
 Decorative stucco over foam concrete
 Roll-up garage doors with optional glass
 Stone veneer

C ELEVATIONS

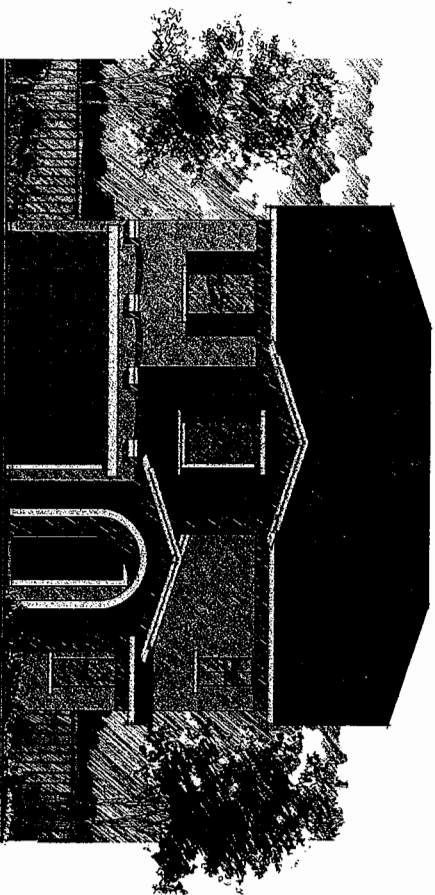
Flat concrete tile roofing
 Gable roofs
 Stucco exterior finish w/ wood siding accents
 Decorative stucco over foam concrete
 Roll-up garage doors with optional glass
 Stone veneer



Elevation C



Elevation A

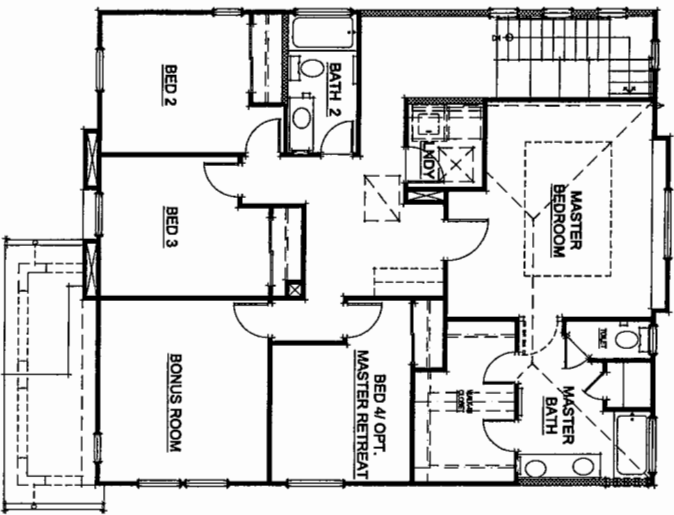


Elevation B

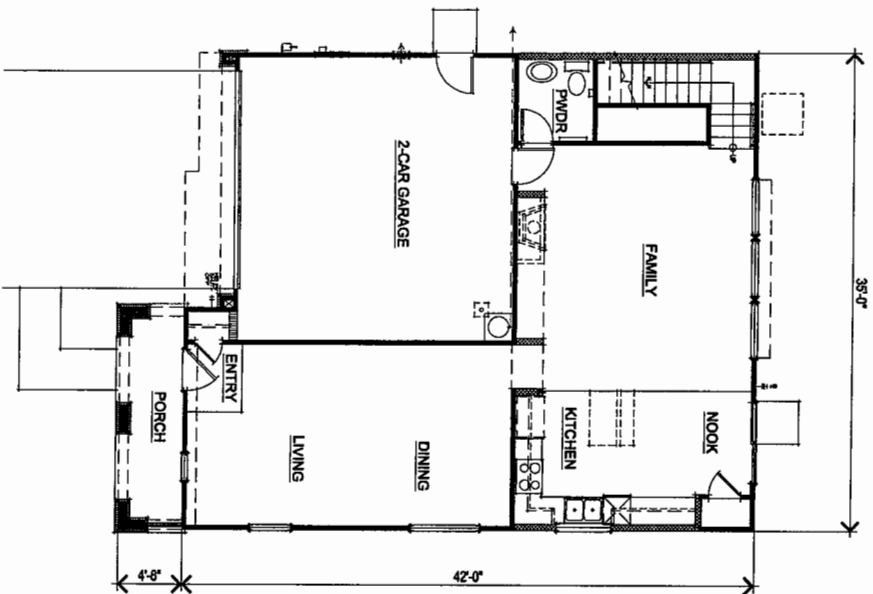
**PLAN 3
 STANDARD LOT PRODUCT**

**R O L L I N G W O O D
 V A L I E J O , C A L I F O R N I A**





SECOND FLOOR PLAN



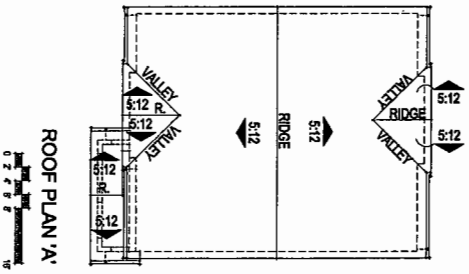
FIRST FLOOR PLAN

SQUARE FOOTAGE	
FIRST FLOOR:	974 SQ. FT.
SECOND FLOOR:	1488 SQ. FT.
TOTAL:	2462 SQ. FT.
2-CAR GARAGE:	439 SQ. FT.

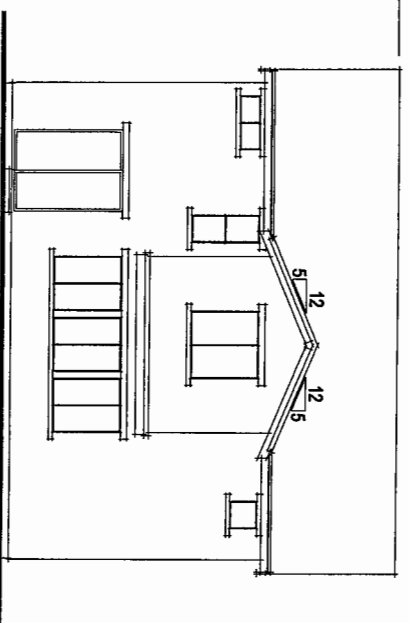
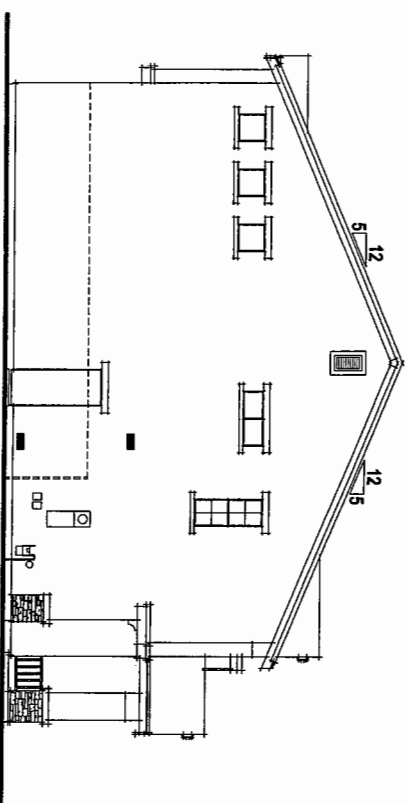
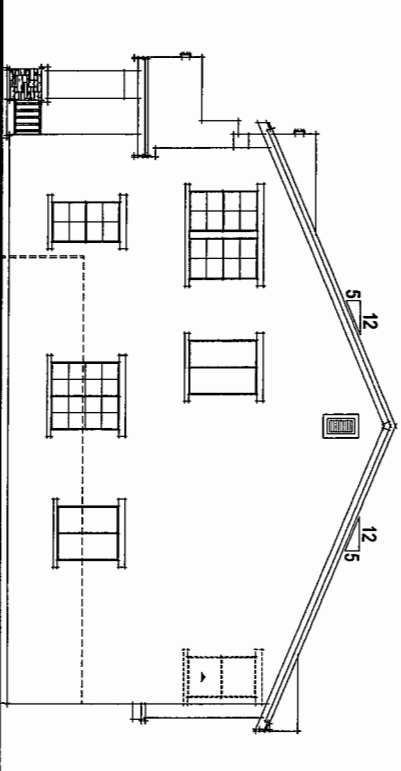
PLAN 3
STANDARD LOT PRODUCT

R o l l i n g w o o d
V a l i e j o , C a l i f o r n i a

S O G A ' S H I L L S C I S , I N C.
929.434.7988
08 August 07



0' 2' 4' 6' 8' 10'



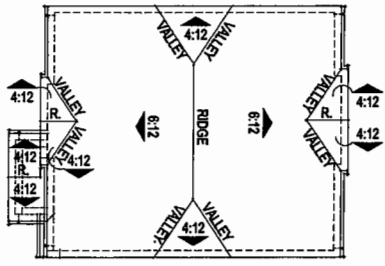
28'-11"

PLAN 3
STANDARD LOT PRODUCT

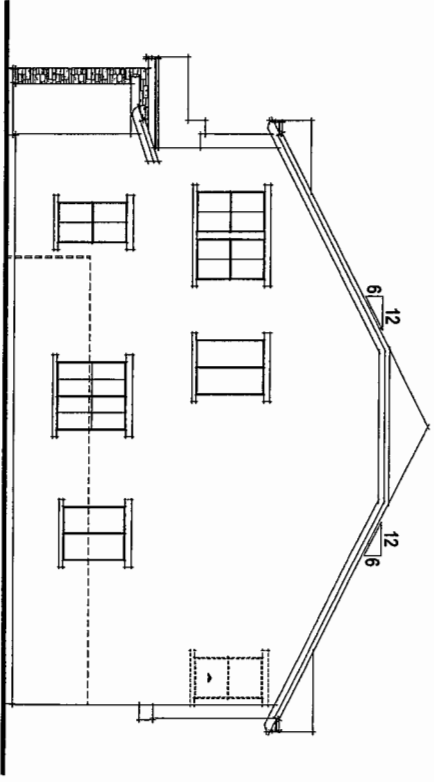
R O L I N G W O O D

V A R I E T Y O F C A L I F O R N I A

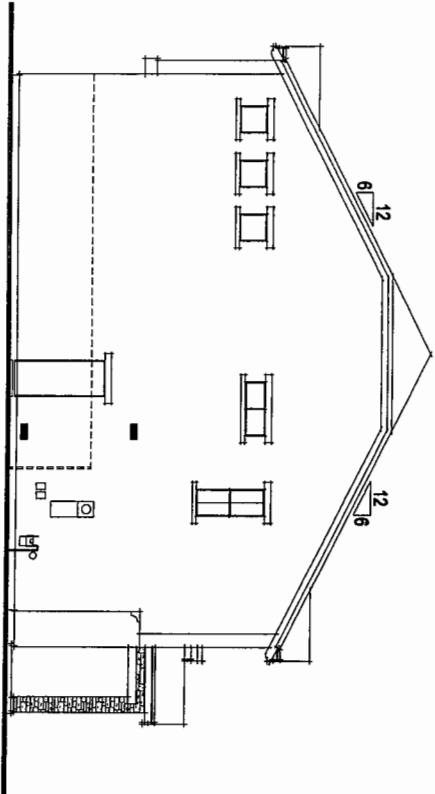
S D O A R C H I T E C T S, I N C.
925.834.7000
08 AUGUST 07



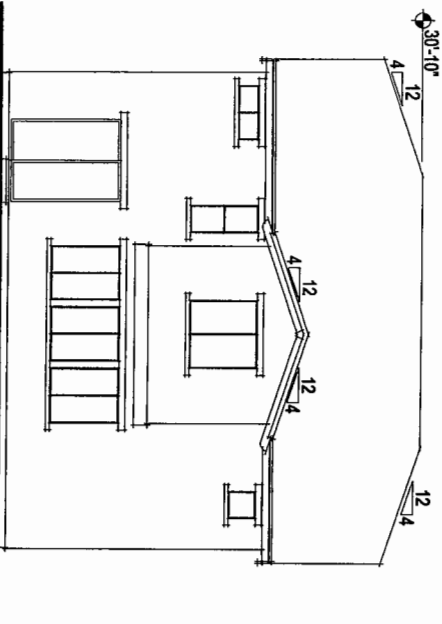
ROOF PLAN 'B'
 0' 2' 4' 6' 8'



RIGHT ELEVATION 'B'



LEFT ELEVATION 'B'



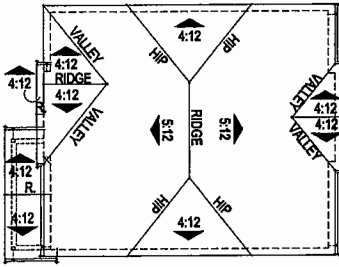
REAR ELEVATION 'B'

PLAN 3
 STANDARD LOT PRODUCT

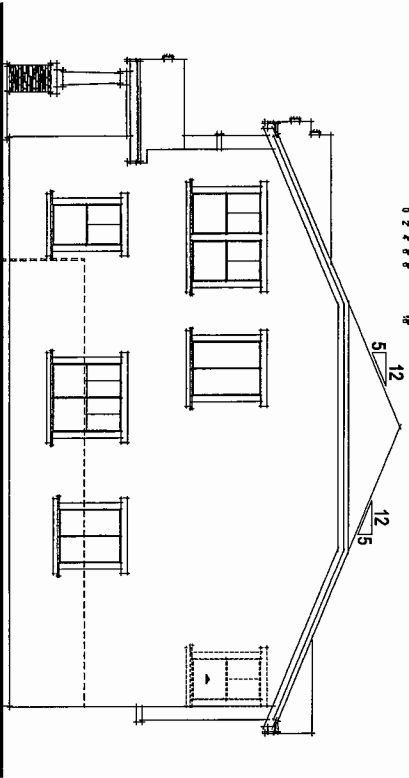
R O I L I N G W O O D

V A I L E J O , C A L I F O R N I A

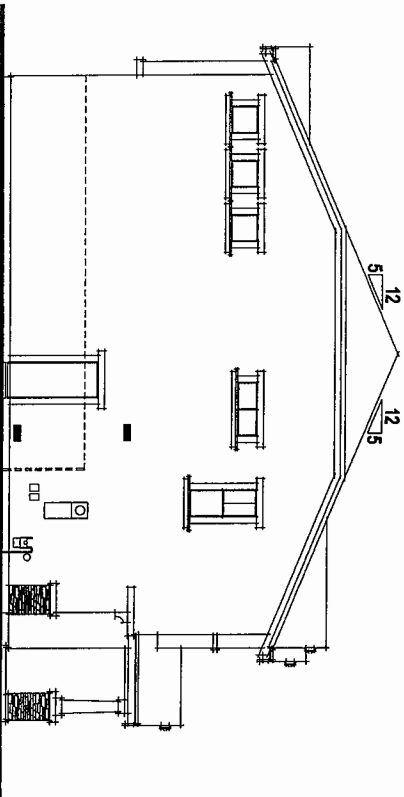
8 0 0 A R G O N I A C O I N C .
 # 2 5 - 8 3 4 - 7 0 0 0
 5 0 A S S O C I A T E S



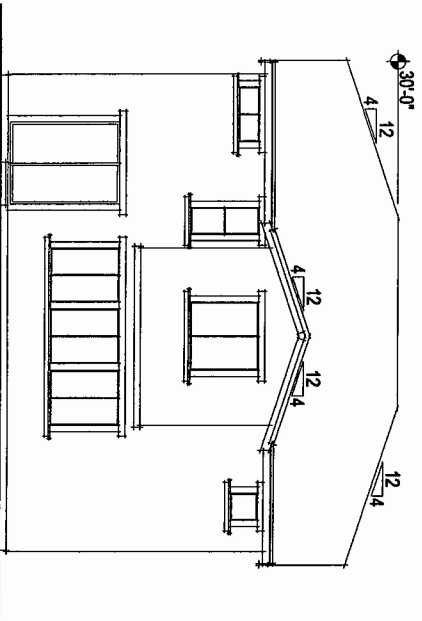
ROOF PLAN 'C'



RIGHT ELEVATION 'C'



LEFT ELEVATION 'C'



REAR ELEVATION 'C'



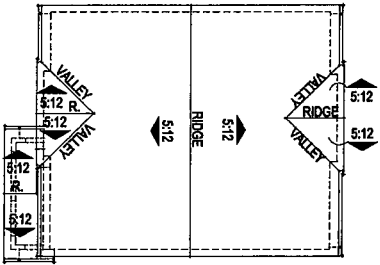
PLAN 3
STANDARD LOT PRODUCT

R O I L I N G W O O D

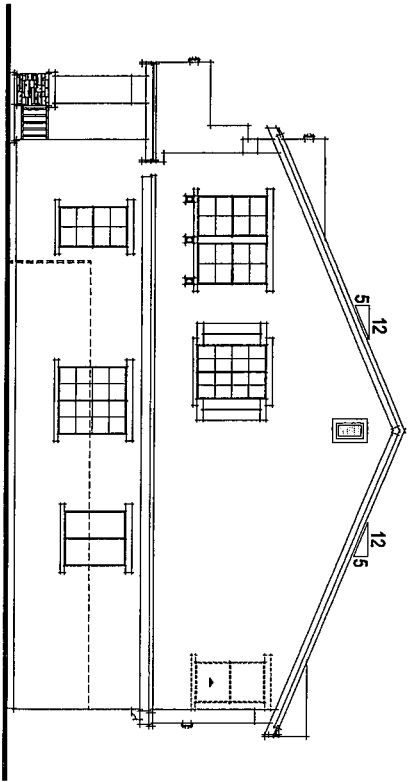
VALLEJO, CALIFORNIA



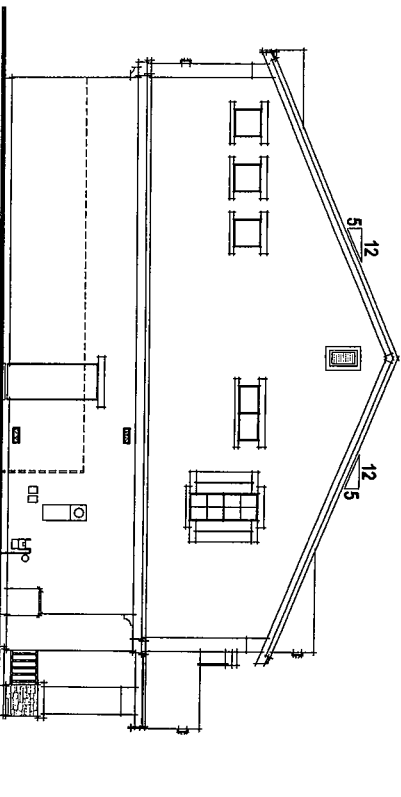
800 ARCHITECTURE, INC.
925.834.7000
08 August 07



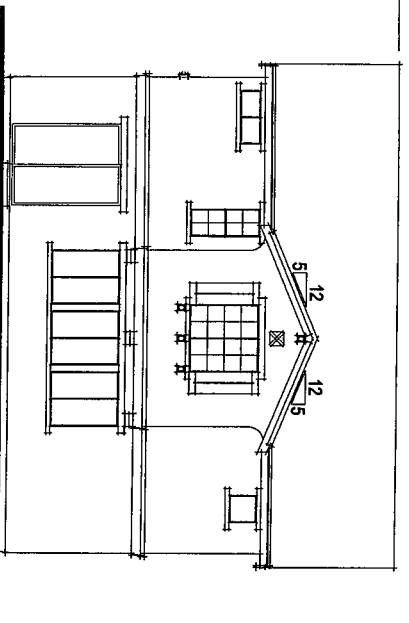
ROOF PLAN 'A'



RIGHT ELEVATION 'A'



LEFT ELEVATION 'A'



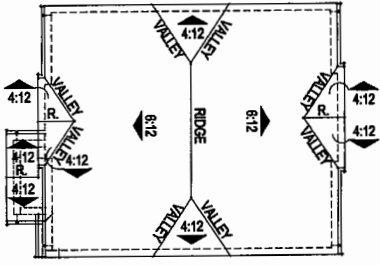
REAR ELEVATION 'A'

PLAN 3
STANDARD LOT PRODUCT
ENHANCED ELEVATIONS WHERE
LOTS ADJACENT TO ROADWAY

R O L I I N G W O O D

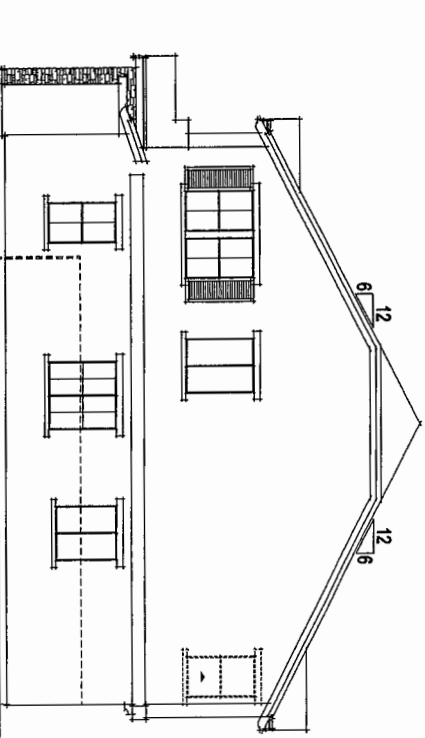
V A L I E J O , C A L I F O R N I A



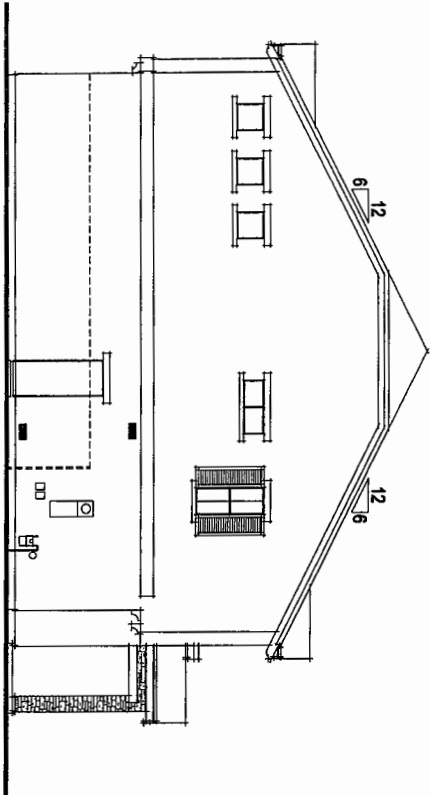


ROOF PLAN 'B'

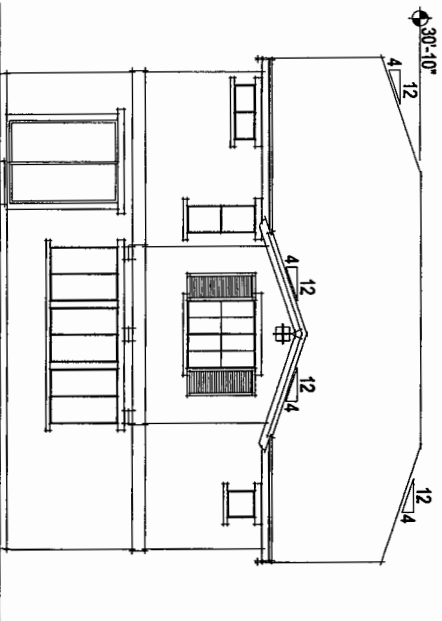
0 2 4 6 8



RIGHT ELEVATION 'B'



LEFT ELEVATION 'B'



REAR ELEVATION 'B'

0 2 4 6 8

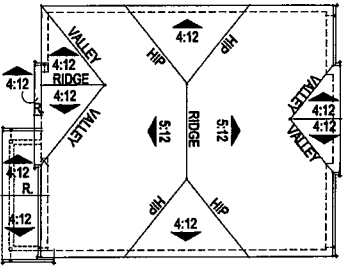
PLAN 3
STANDARD LOT PRODUCT
ENHANCED ELEVATIONS WHERE
LOT IS ADJACENT TO ROADWAY

R O L I N G W O O D

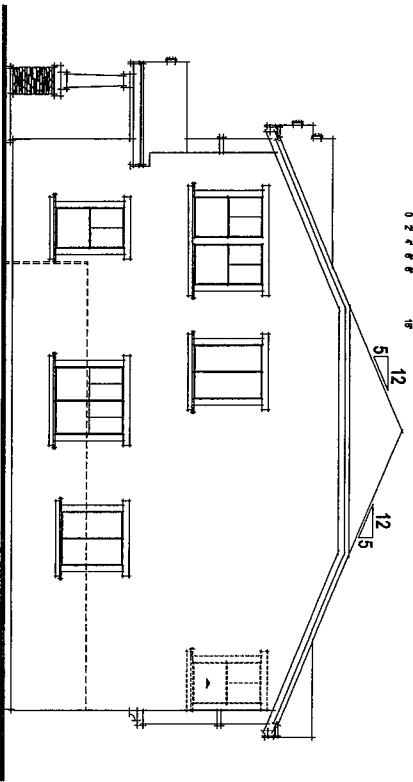
V A L I E J O, C A L I F O R N I A



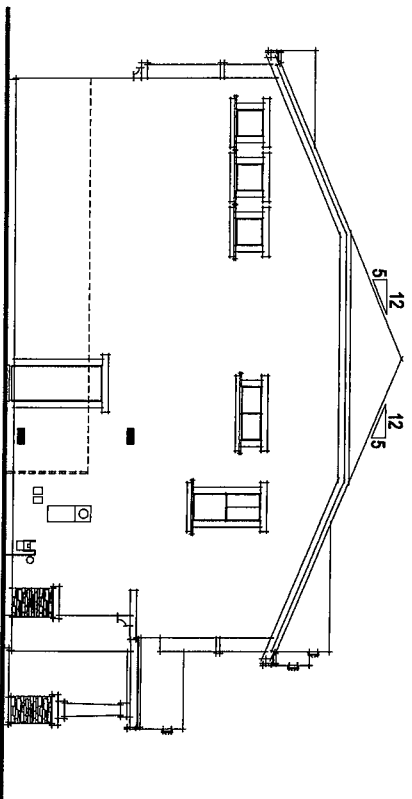
S D G A R T S H I L L S, I N C.
925-834-7988
90 ARDEN 97



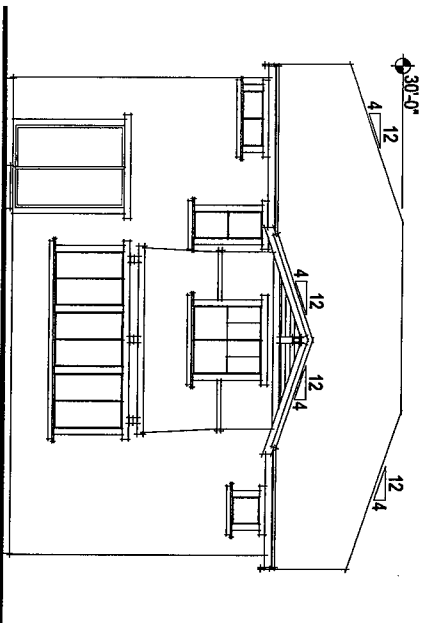
ROOF PLAN 'C'



RIGHT ELEVATION 'C'



LEFT ELEVATION 'C'



REAR ELEVATION 'C'

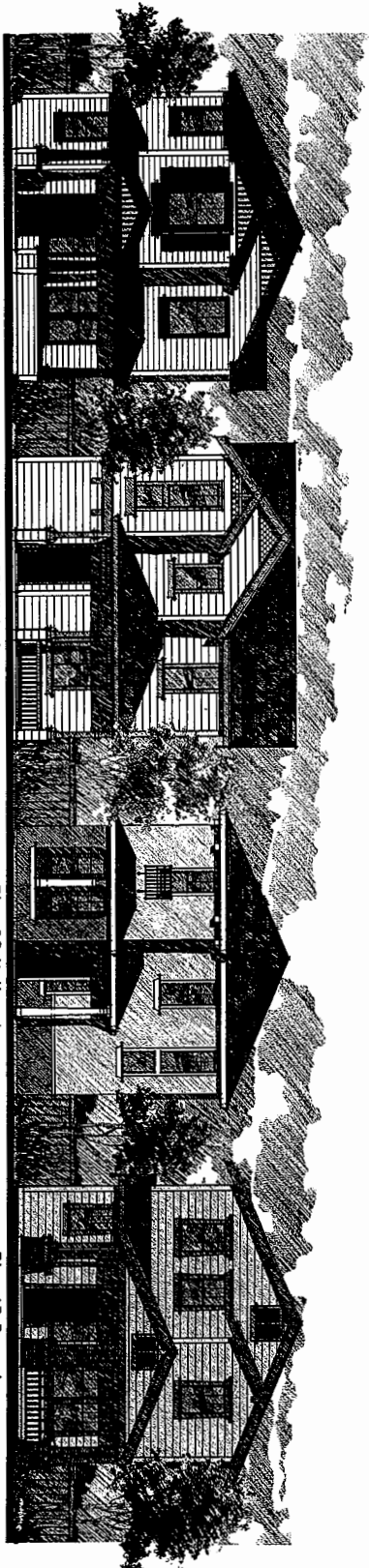
PLAN 3
 STANDARD LOT PRODUCT
 ENHANCED ELEVATIONS WHERE
 LOT IS ADJACENT TO ROADWAY

R O L I N G W O O D

V A L L E J O, C A L I F O R N I A



8 0 0 A R C H I T E C T S, I N C.
 929.834.7000
 08 August 07



Plan 1A Traditional
(Extended Porch Option Shown)

Plan 3D Victorian

Plan 2C Italianate

Plan 1B Bungalow

Note: Extended Porch Option to be Offered on Paseo Homes Plans 1 and 3 at Corner Lots only

Paseo Homes

Rollingwood

Vallejo, California



300 Architec, Inc.
925.934.7000
03 August 07

EXTERIOR MATERIALS

A ELEVATIONS

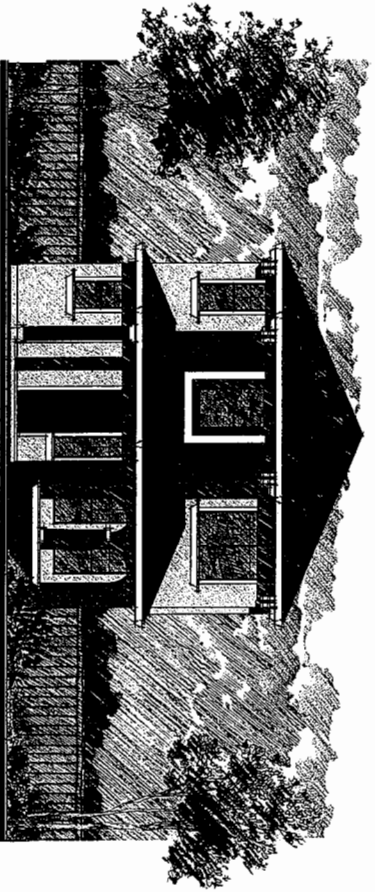
- Flat concrete tile roofing
- Gable & hip roofs
- Wood siding
- Decorative eels & trim
- Roll-up garage doors with optional glass

B ELEVATIONS

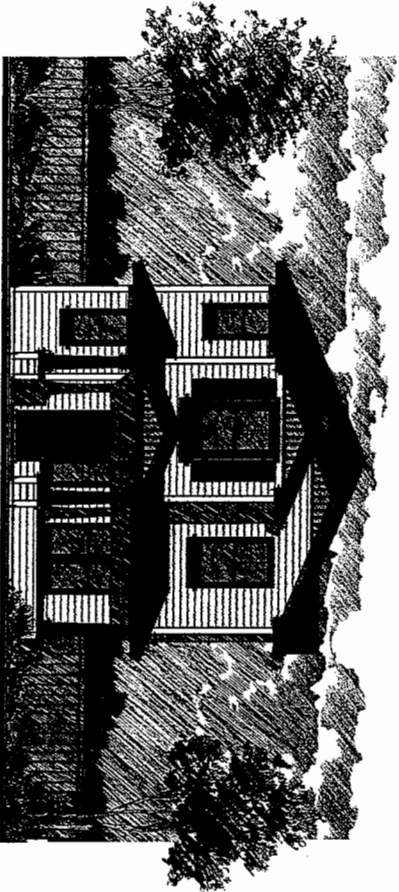
- Flat concrete tile roofing
- Gable & shed roofs
- Wood siding
- Wood trim
- Decorative eels & trim
- Roll-up garage doors with optional glass
- Stone veneer

C ELEVATIONS

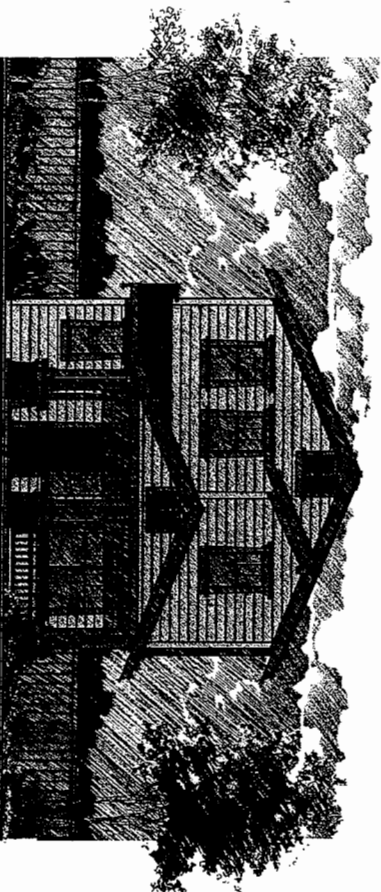
- Flat concrete tile roofing
- Hip roofs
- Shutter exterior finish
- Decorative eels & trim
- Roll-up garage doors with optional glass



Elevation C



Elevation A



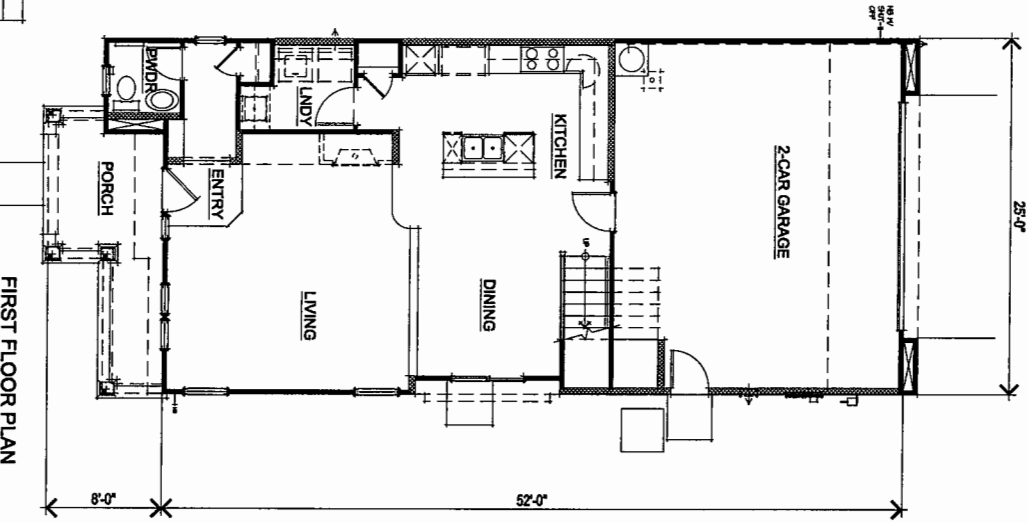
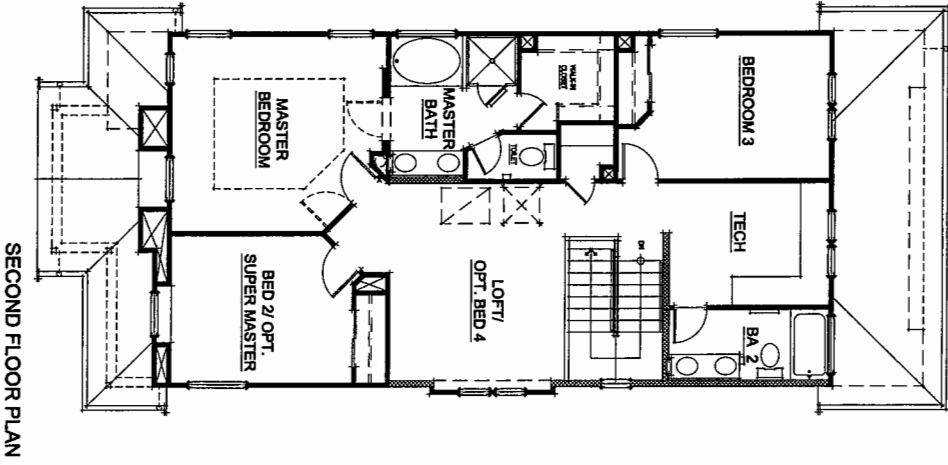
Elevation B



**PLAN 1
PASEO HOMES**

**R O L I I N G W O O D
V A I L E J O , C A L I F O R N I A**

300 Archili Pacific, Inc.
928-694-7666
68 August 07



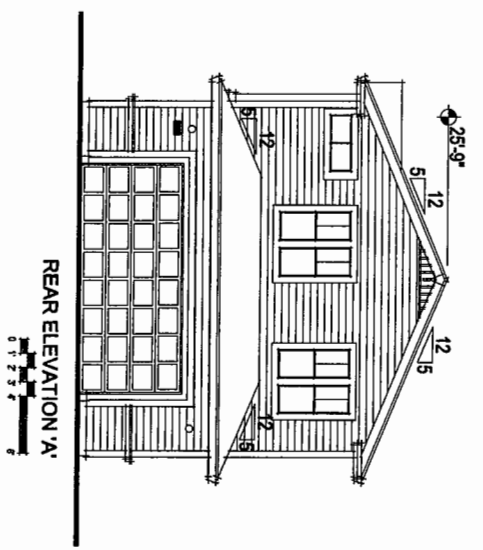
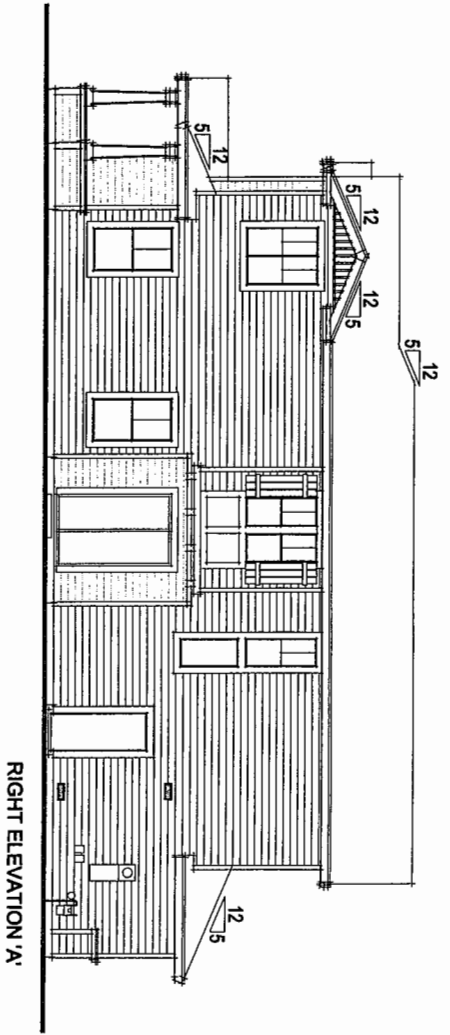
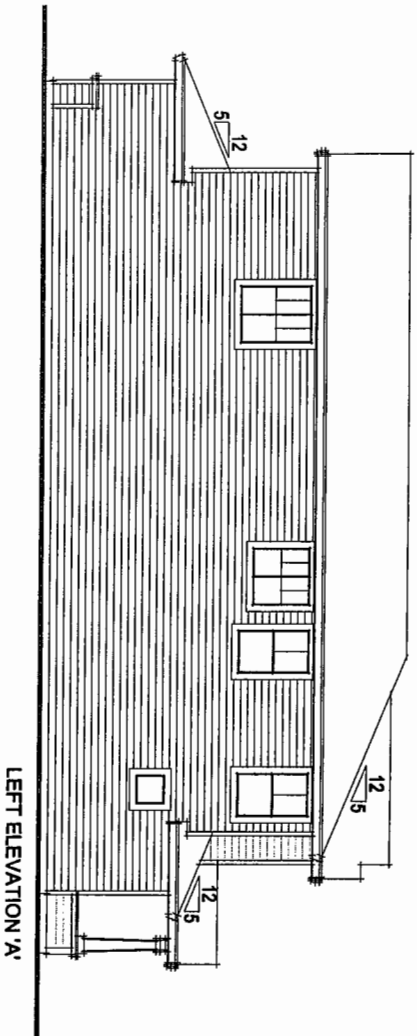
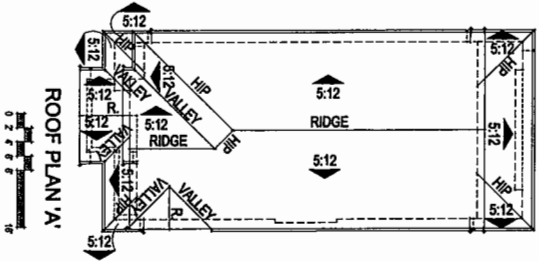
SQUARE FOOTAGE	
FIRST FLOOR:	895 SQ. FT.
SECOND FLOOR:	1137 SQ. FT.
TOTAL:	1942 SQ. FT.
2-CAR GARAGE:	599 SQ. FT.

PLAN 1
PASEO HOMES

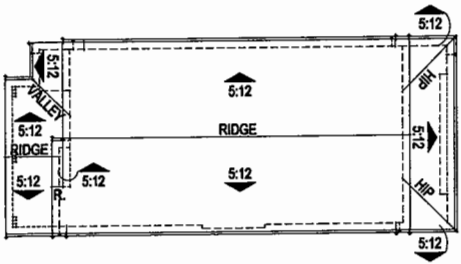
R O I I I n g W O O d
V a l l e j o, C a l i f o r n i a

S O D A R 1 9 8 7 1 1 9 8 7 S. I. N. C.
9 2 8 . 9 2 1 . 7 0 0 0

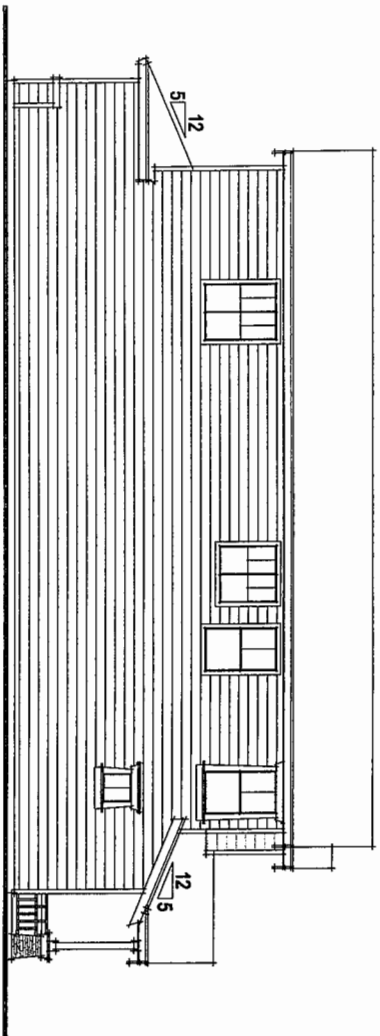




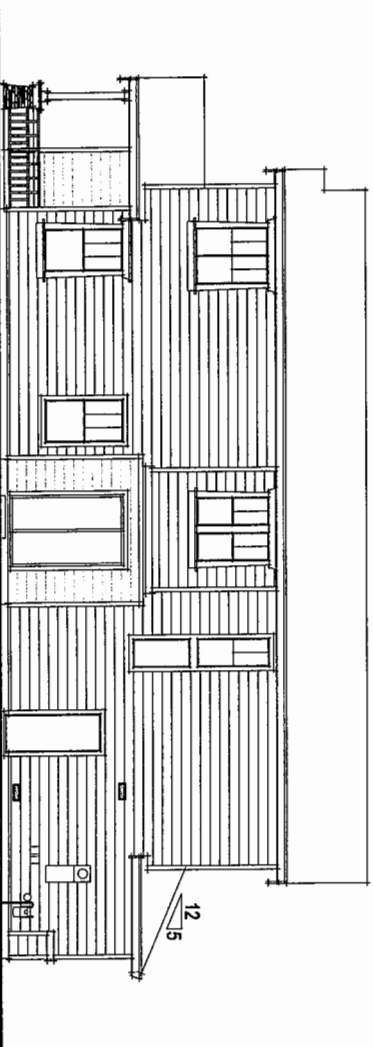
PLAN 1
 PASEO HOMES
 R o l i n g w o o d
 V a l l e j o , C a l i f o r n i a
 S D G A r c h i t e c t s , I n c .
 9 2 5 1 7 0 9 8



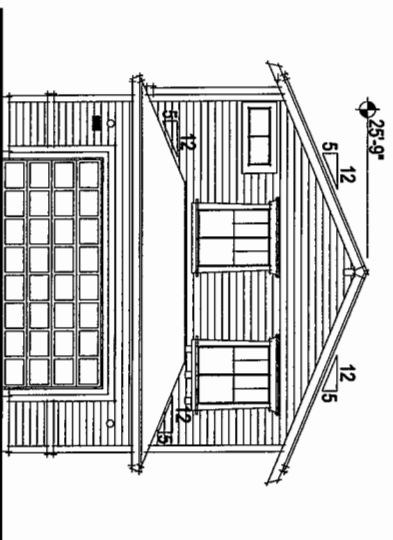
ROOF PLAN 'B'



LEFT ELEVATION 'B'



RIGHT ELEVATION 'B'



REAR ELEVATION 'B'

PLAN 1
PASEO HOMES

R o l l i n g W o o d

V a l l e j o, C a l i f o r n i a

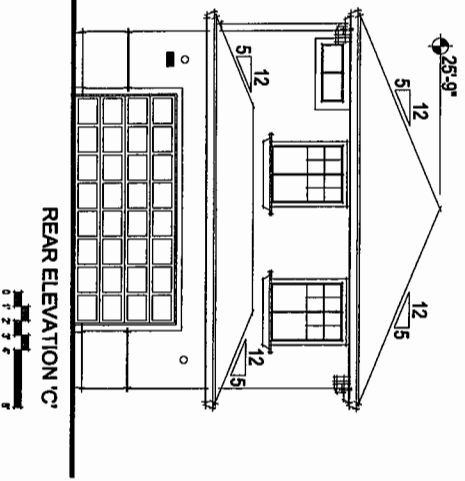
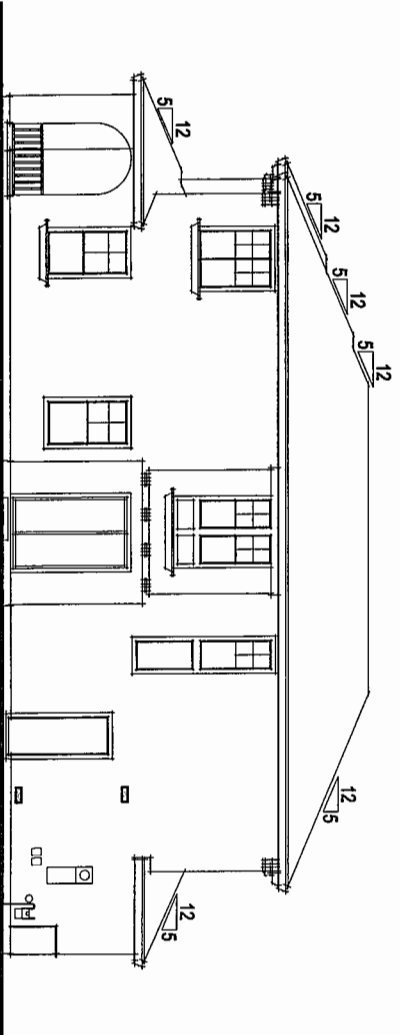
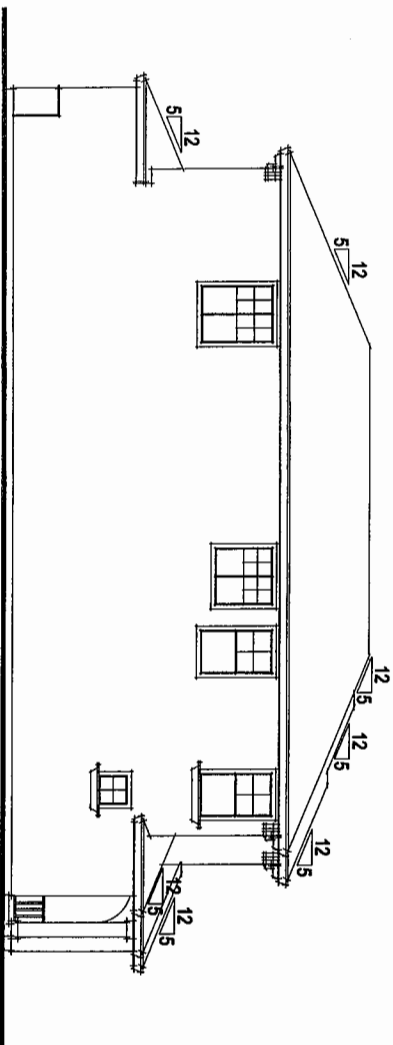
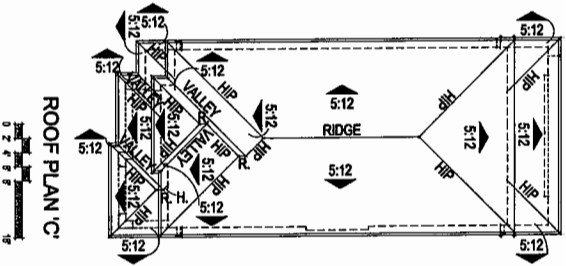


S D G A R C N I L P A C I S, I N C.

2008 ANDREWS ST

DAVIS, CA 94601

07



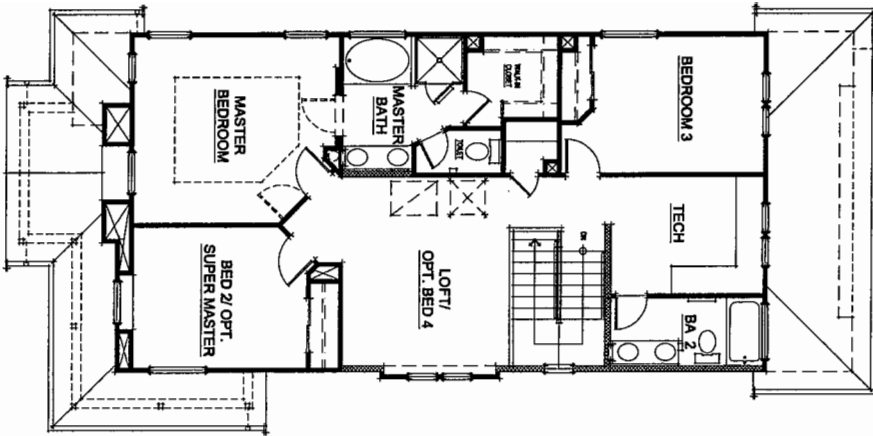
PLAN 1
PASSEO HOMES

R O L I I N G W O O D

V A L I E J O , C A L I F O R N I A



5 0 0 A R G O H I L L S, S A N J O S E, C A L I F O R N I A 9 5 1 2 8



SECOND FLOOR PLAN

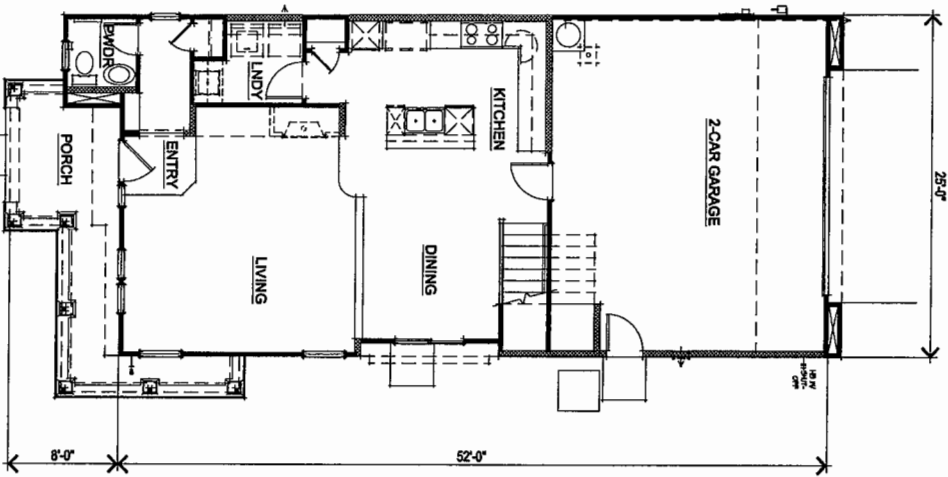
SQUARE FOOTAGE	
FIRST FLOOR:	695 SQ. FT.
SECOND FLOOR:	1137 SQ. FT.
LOFT:	411 SQ. FT.
2-CAR GARAGE:	599 SQ. FT.

PLAN 1

ALLEY LOAD @ EXTENDED PORCH PRODUCT

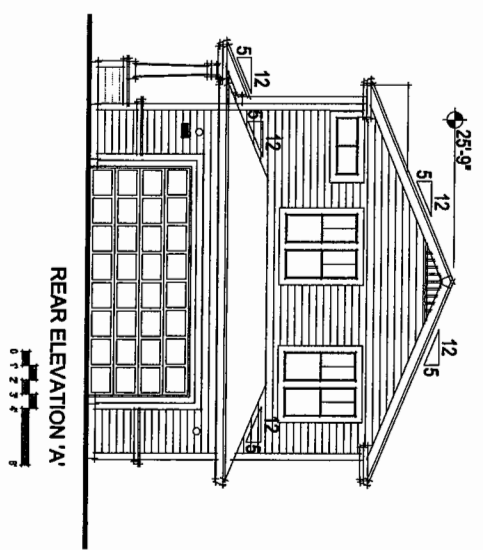
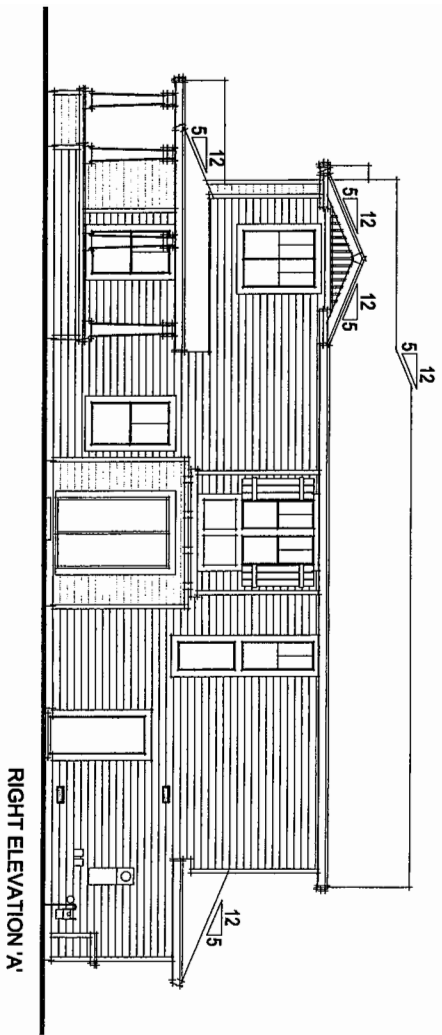
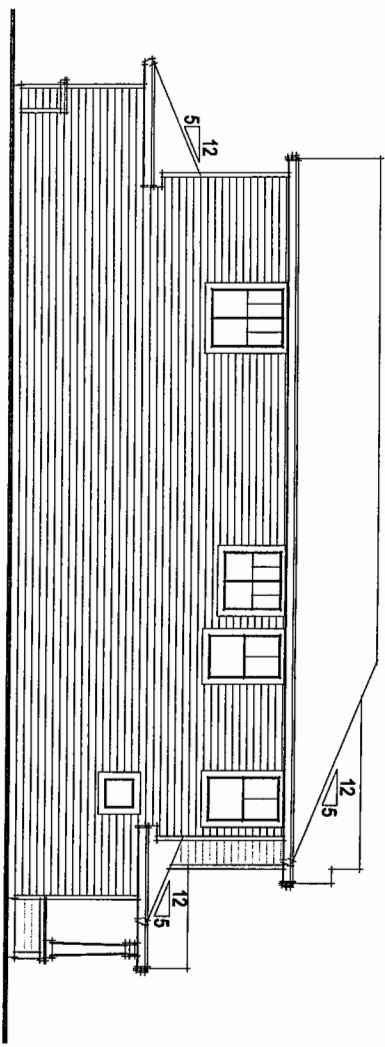
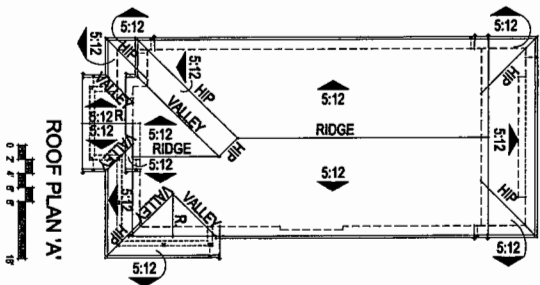
R o l l i n g w o o d

v a r i e t y o f c a l i f o r n i a



FIRST FLOOR PLAN



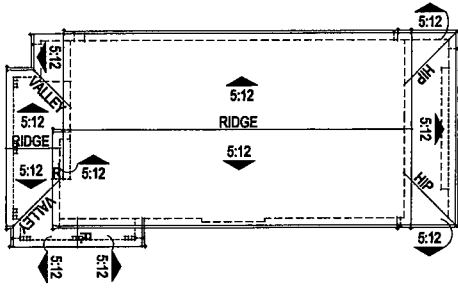


PLAN 1
ALLEY LOAD @ EXTENDED PORCH PRODUCT

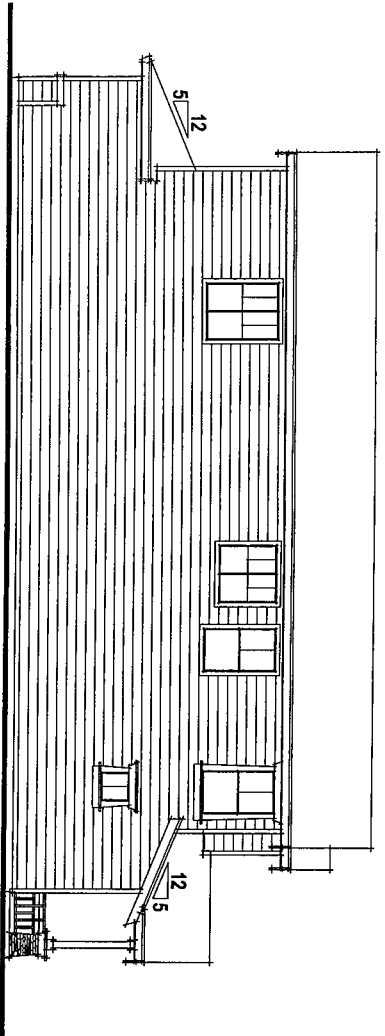
R O L I I N G W O O D
V A I L E J O C A L I F O R N I A

S D G A R C H I T E C T S, I N C.
P O B O X 1 1 7 5 0 1
S A N J O S E, C A L I F O R N I A 9 5 1 2 7

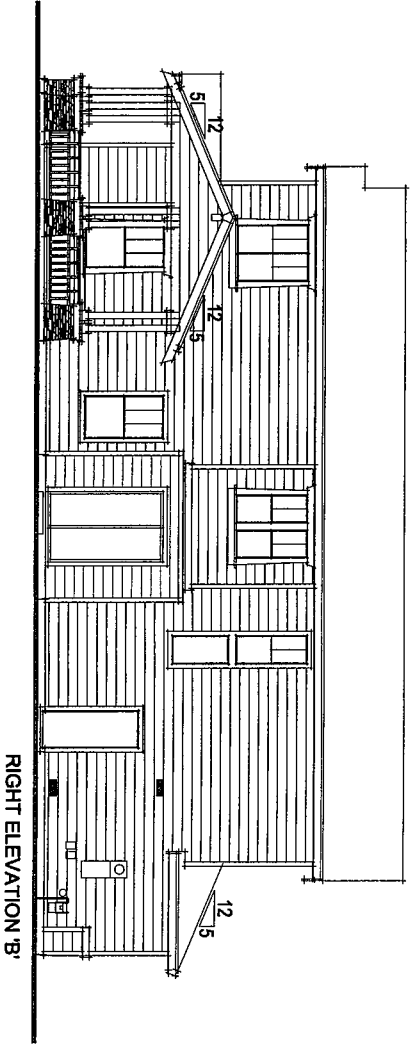




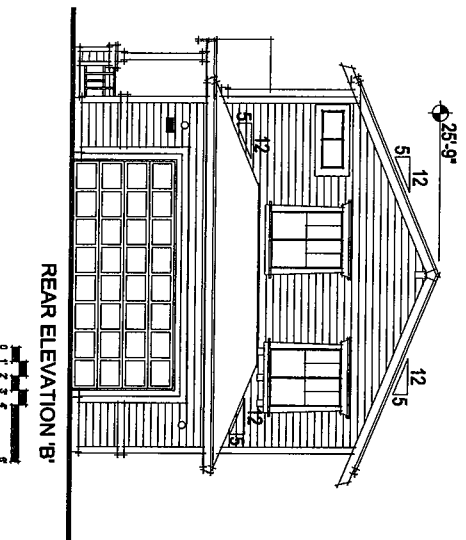
ROOF PLAN 'B'



LEFT ELEVATION 'B'



RIGHT ELEVATION 'B'



REAR ELEVATION 'B'

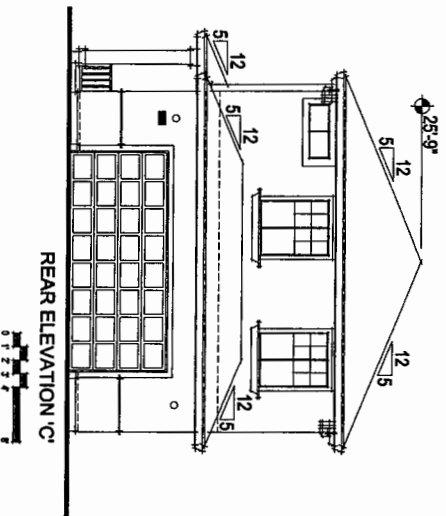
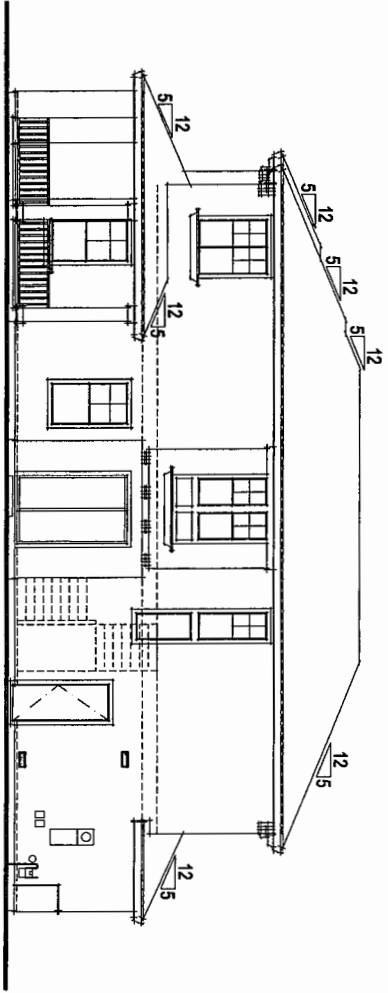
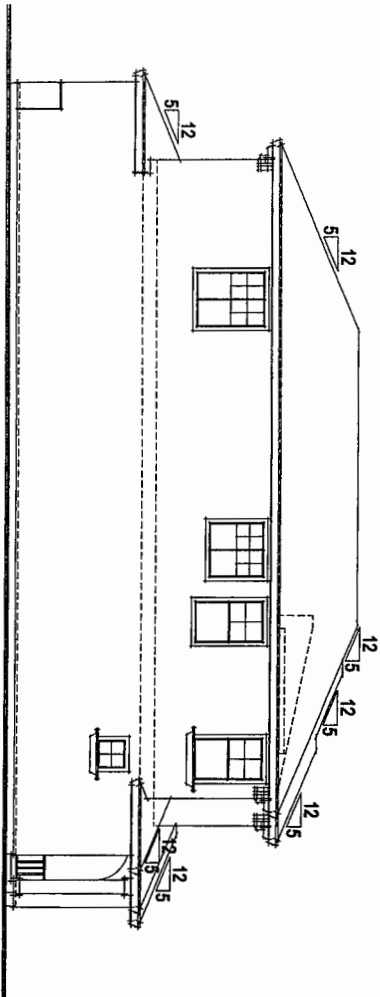
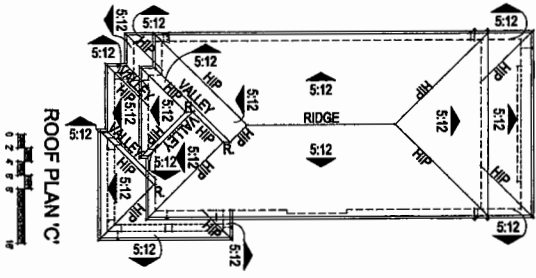
PLAN 1
ALLEY LOAD @ EXTENDED PORCH PRODUCT

R O L I I N S W O O D

V a l i e j o C a l i f o r n i a



S O G A R S H I P S I N C.
438.934.7099



PLAN 1
ALLEY LOAD @ EXTENDED PORCH PRODUCT

R O I L I N G W O O D
V A I L E J O C A L I F O R N I A

S O O A R C H I T E C T S I N C .
929.434.7000
08 August 07

EXTERIOR MATERIALS

A ELEVATIONS

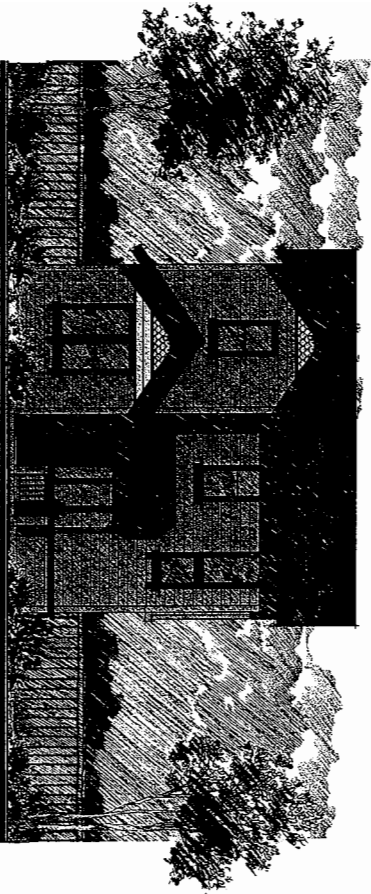
Flat concrete tile roofing
 Gable & hip roofs
 Vertical board and batten accents
 Decorative wood shutters
 Roll-up garage doors with optional glass

C ELEVATIONS

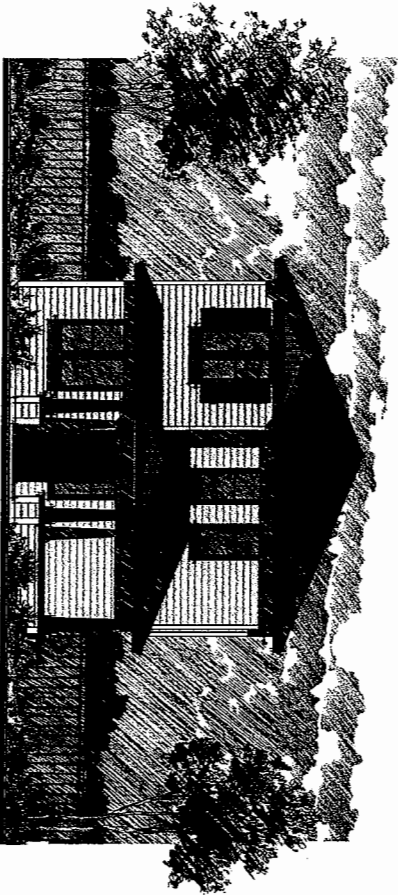
Flat concrete tile roofing
 Hip roofs
 Decorative stucco over foam concrete
 Decorative wood sills & trims
 Roll-up garage doors with optional glass
 Decorative wrought iron railing

D ELEVATIONS

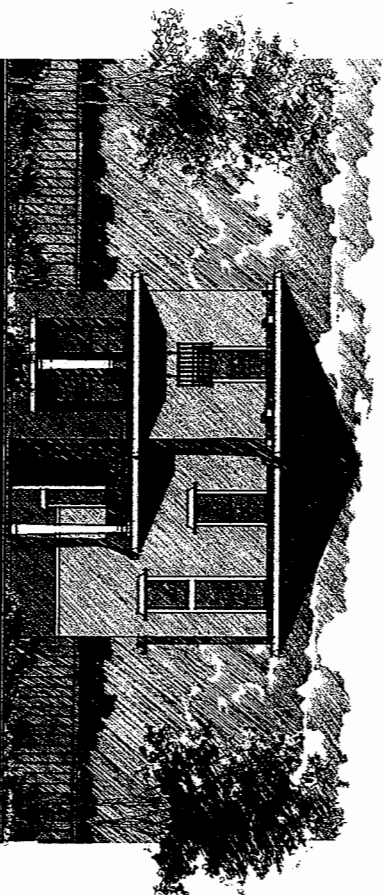
Flat concrete tile roofing
 Gable roofs
 Vertical siding w/ leaded alight siding accents
 Decorative wood sills & trims
 Roll-up garage doors with optional glass



Elevation D



Elevation A



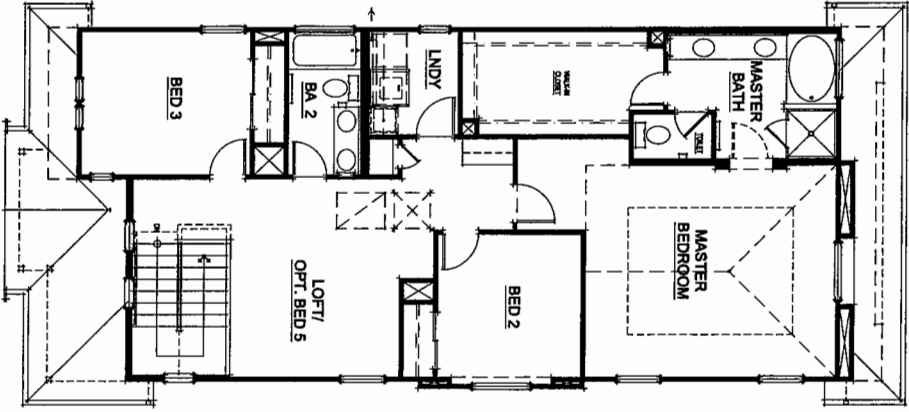
Elevation C



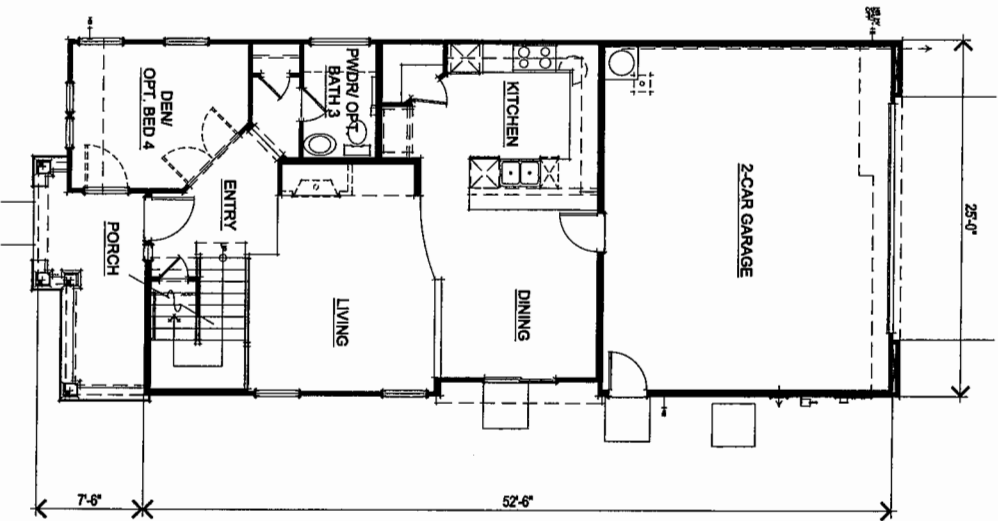
**PLAN 2
 PASEO HOMES**

R O I L I N G W O O D
 V A I L I O , C A L I F O R N I A

3 0 0 A F O R T L I L I O S I S , I N O .
 9 2 8 . 2 2 4 . 7 0 0 0
 0 8 A u g u s t , 0 7



SECOND FLOOR PLAN



FIRST FLOOR PLAN

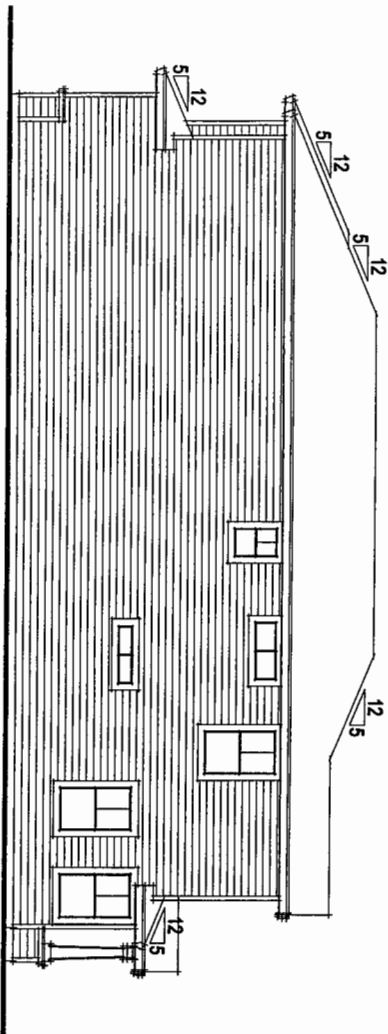
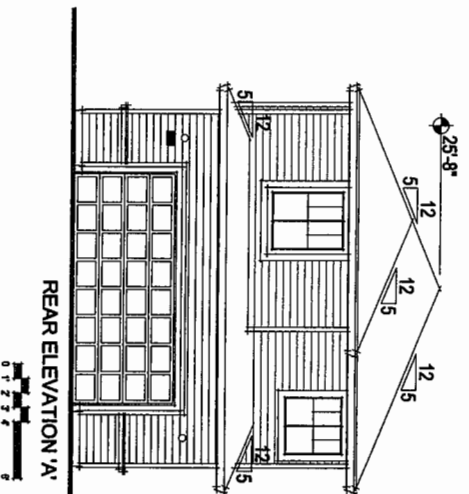
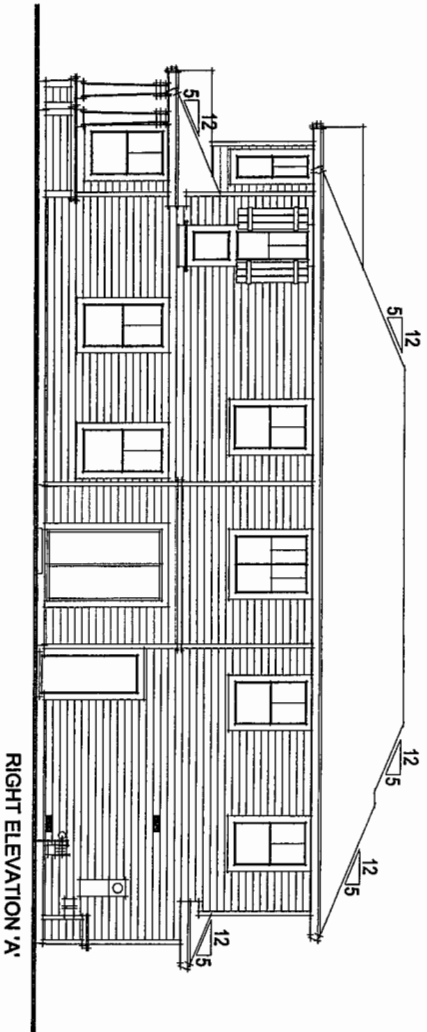
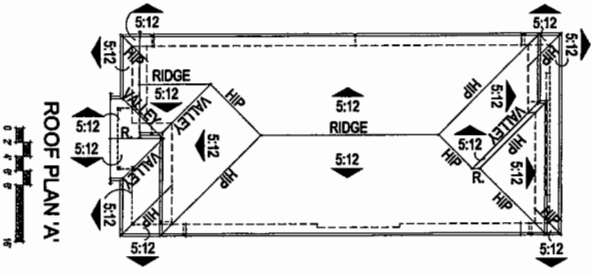
SQUARE FOOTAGE	
FIRST FLOOR:	861 SQ. FT.
SECOND FLOOR:	1226 SQ. FT.
2-CAR GARAGE:	708 SQ. FT.
	289 SQ. FT.

PLAN 2
PASEO HOMES

R O I I I n g W O O D
V A I L E J O, C A L I F O R N I A



S O G A T E H A U S I N G S, I N C.
1924 AVENUE 07
DA ANIMATED 07

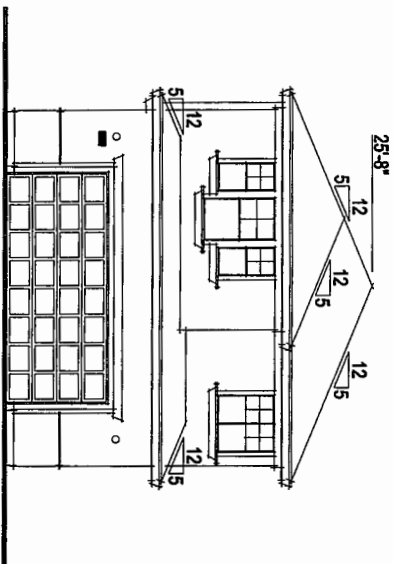
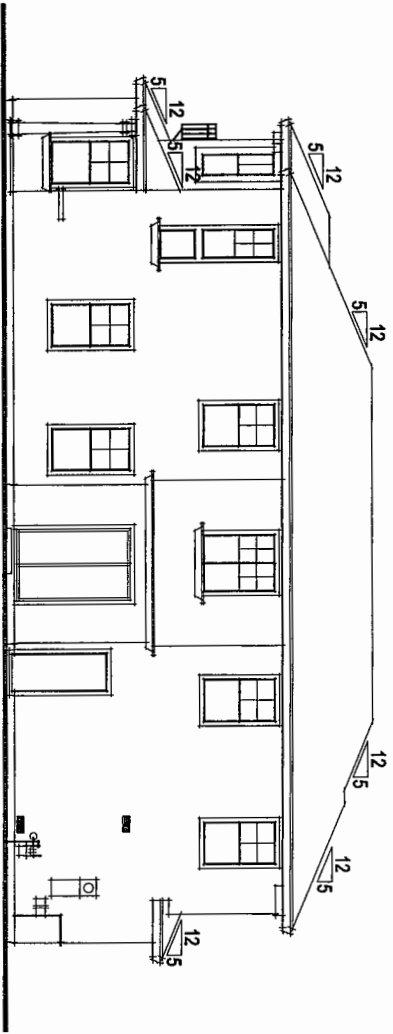
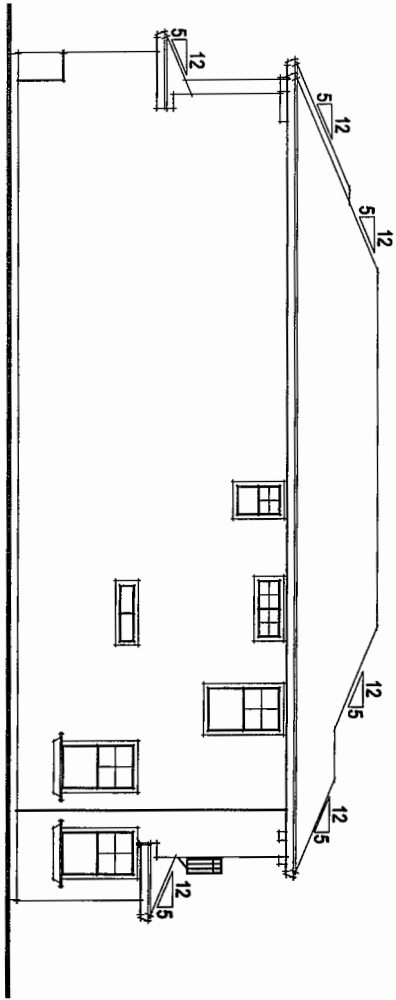
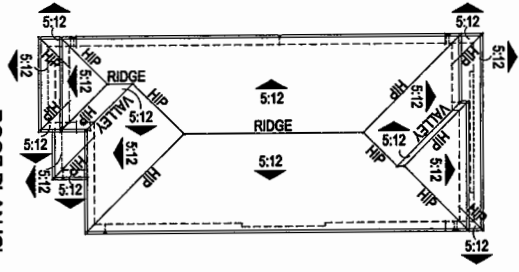


PLAN 2
PASEO HOMES

ROILING WOOD
Village, CALIFORNIA

300 A 15th Street, Inc.
Pasadena, California





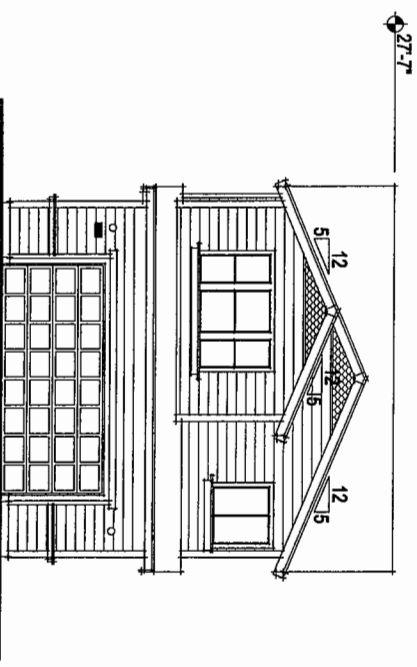
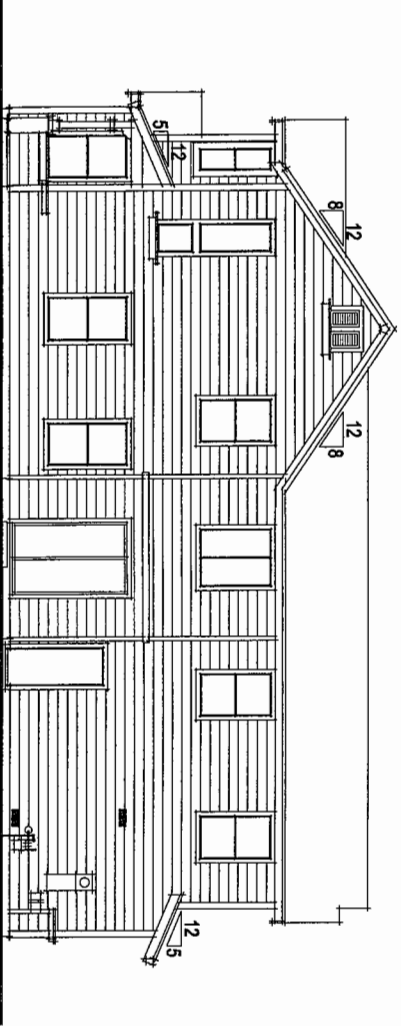
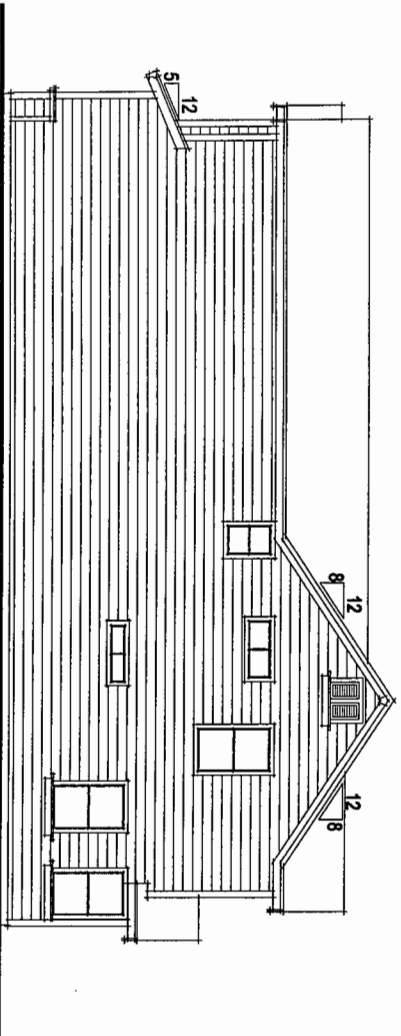
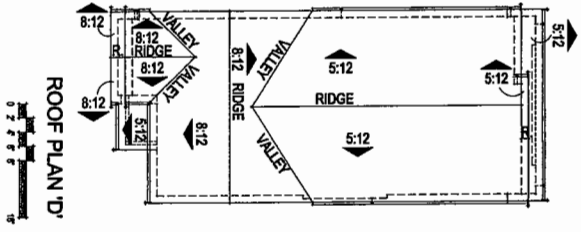
PLAN 2
PASEO HOMES

ROJILINWOOD

Vallejo, California



SOE ARCHITECTS, INC.
525.934.7000



PLAN 2
PASEO HOMES

R O I I I N G W O O D

Vallejo, California



800 A T S E N I T A S I N C.
225 S. MICHIGAN AV.

EXTERIOR MATERIALS

B ELEVATIONS

- Flat concrete tile roofing
- Gable & shed roofs
- Wood siding
- Stone veneer
- Wood corbels
- Decorative wood aille & trim
- Roll-up garage doors with optional glass

C ELEVATIONS

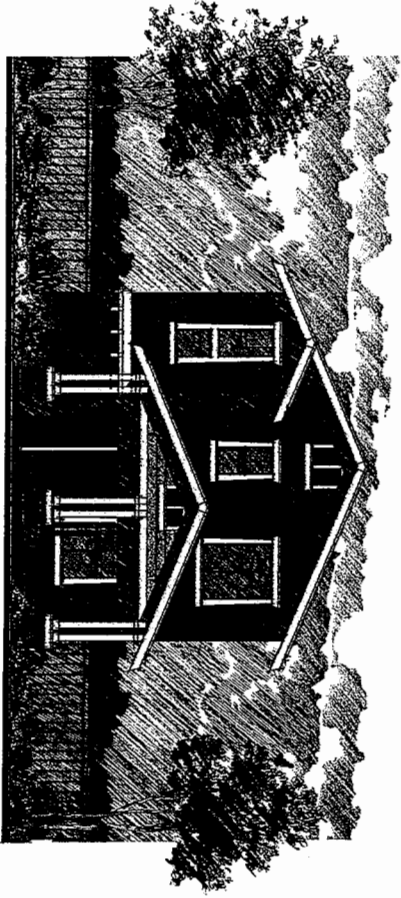
- Flat concrete tile roofing
- Hip roofs
- Stucco exterior finish
- Decorative stucco over foam corbels
- Decorative stucco over foam aille & trim
- Roll-up garage doors with optional glass
- Decorative wrought iron railing

D ELEVATIONS

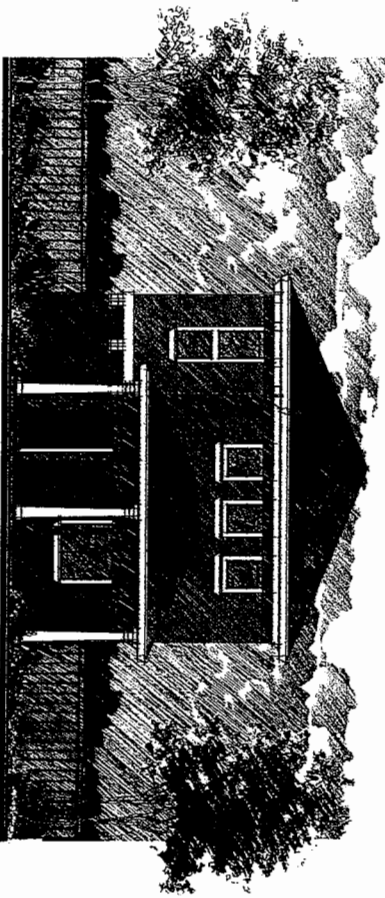
- Flat concrete tile roofing
- Gable roofs
- Wood siding w/ scalloped siding accents
- Decorative wood aille & trim
- Roll-up garage doors with optional glass



Elevation D



Elevation B



Elevation C

PLAN 3

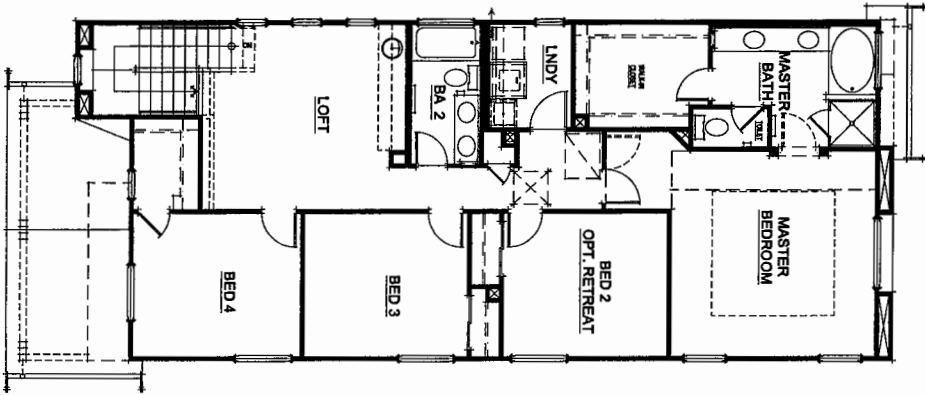
PASEO HOMES

R O L I I N G W O O D

V A L I E J O , C A L I F O R N I A

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 08 August '07

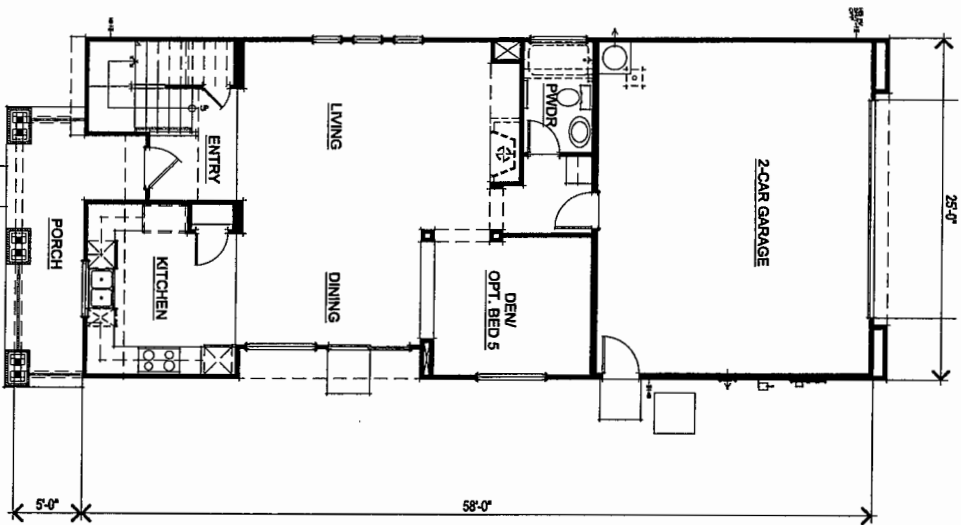




SECOND FLOOR PLAN

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TOTAL:	2278 SQ. FT.
2-CAR GARAGE:	599 SQ. FT.

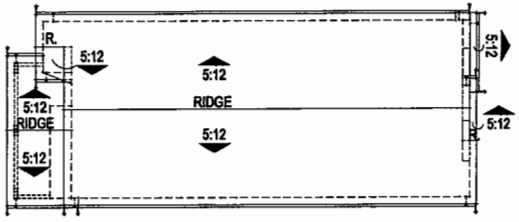
PLAN 3
PASEO HOMES



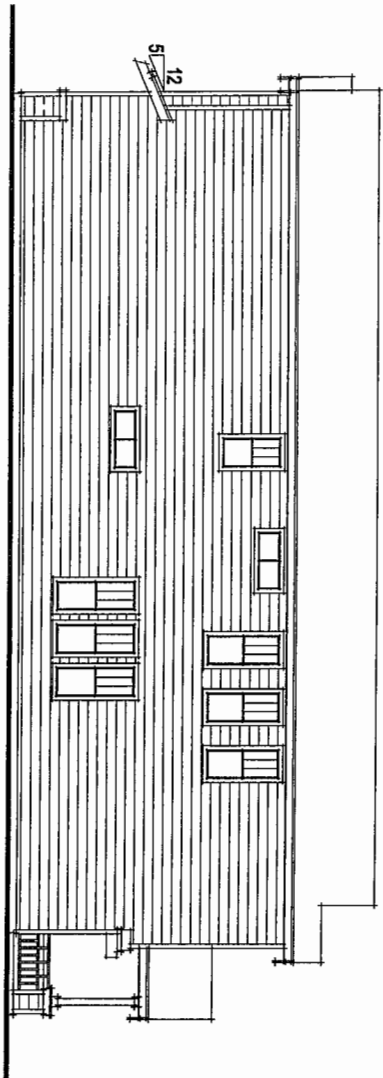
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R o l i n g w o o d
V a l i e j o, C a l i f o r n i a

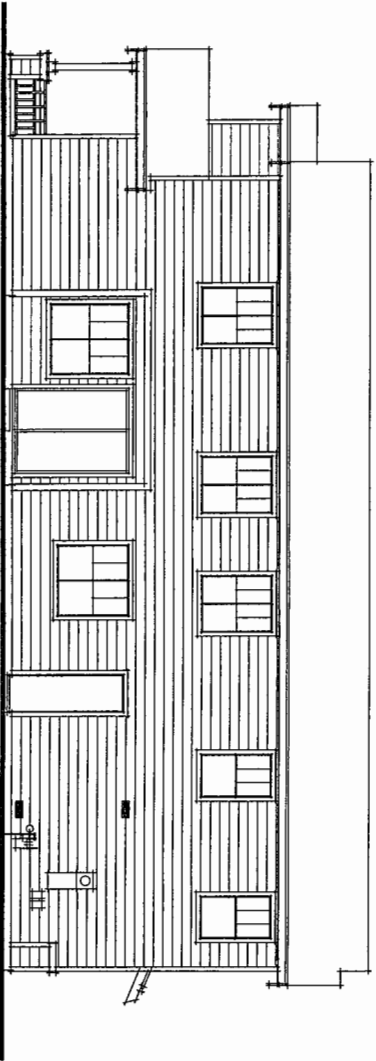
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08 August 07



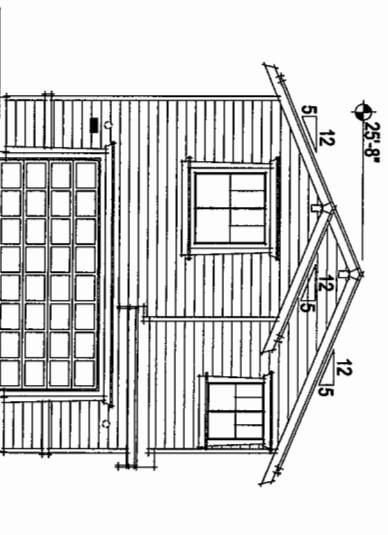
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 1/8" = 1'-0"
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LEFT ELEVATION 'B'



RIGHT ELEVATION 'B'



REAR ELEVATION 'B'



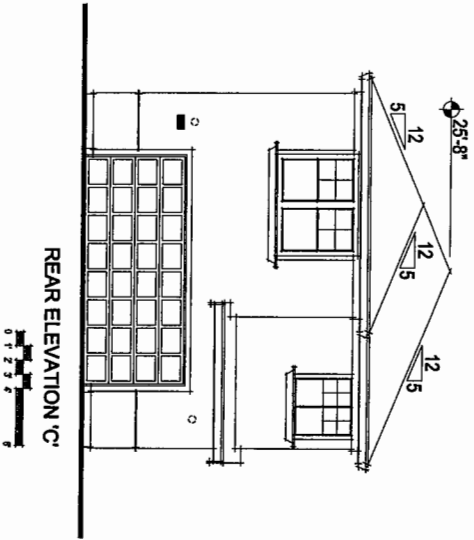
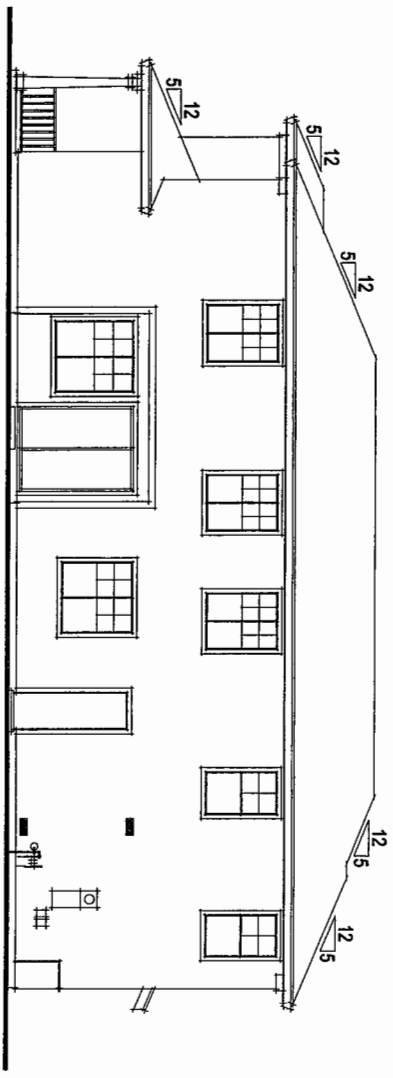
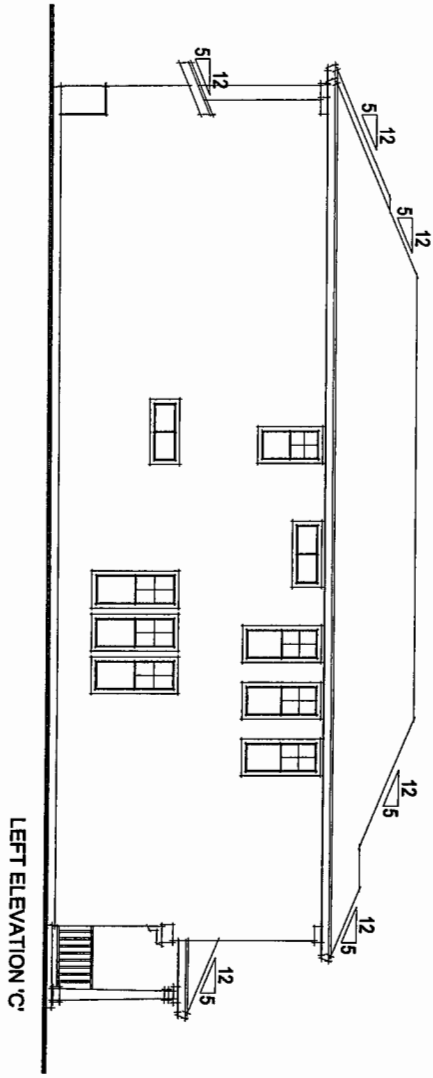
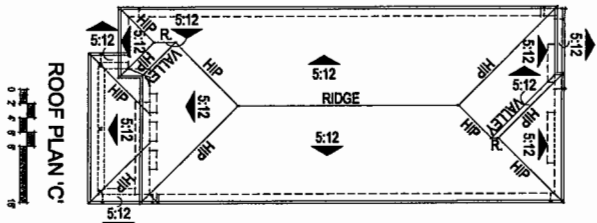
PLAN 3
 PASEO HOMES

R O L I I n g W O O D

V A I I A J O , C A L I F O R N I A



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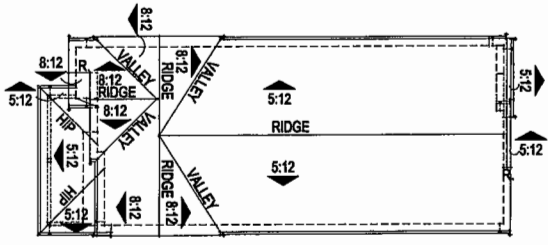


PLAN 3
PASEO HOMES

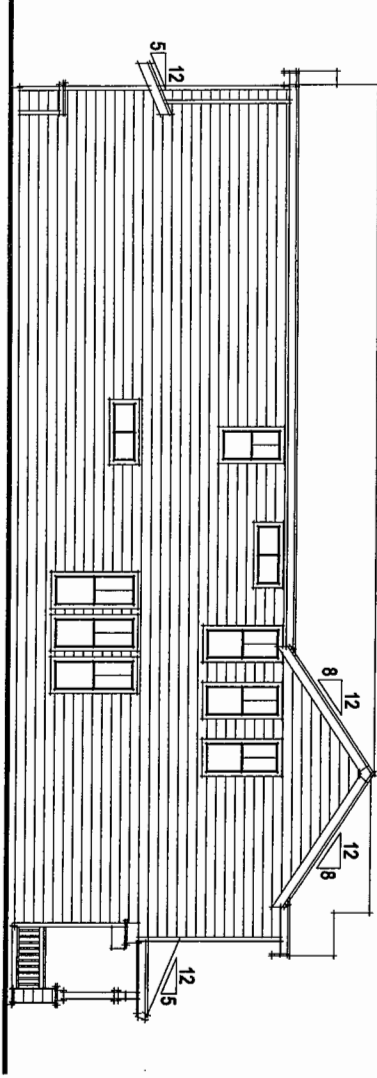
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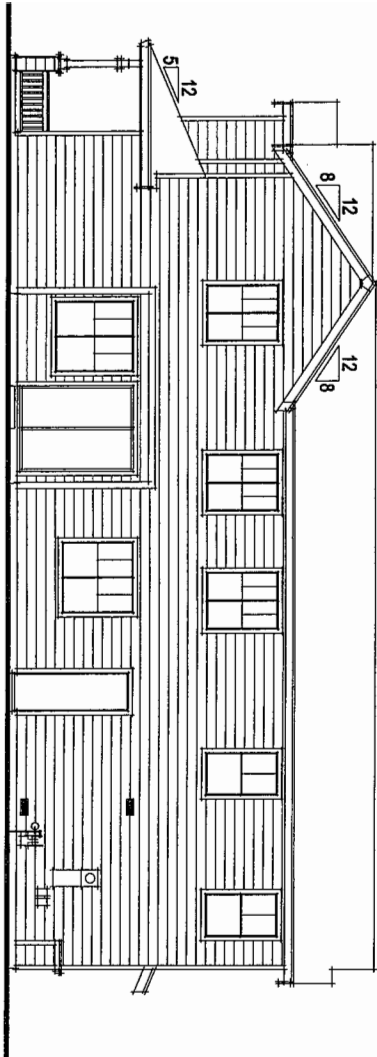
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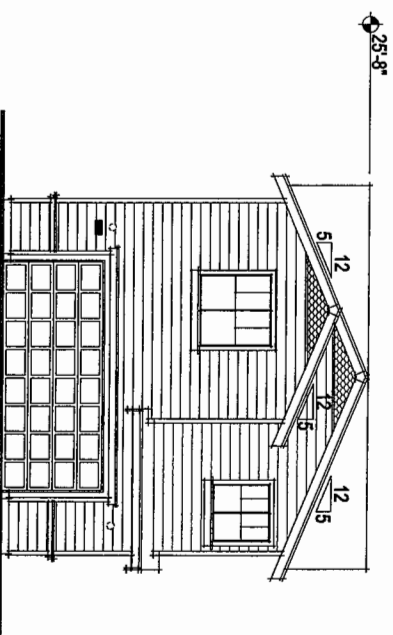
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LEFT ELEVATION 'D'



RIGHT ELEVATION 'D'



REAR ELEVATION 'D'

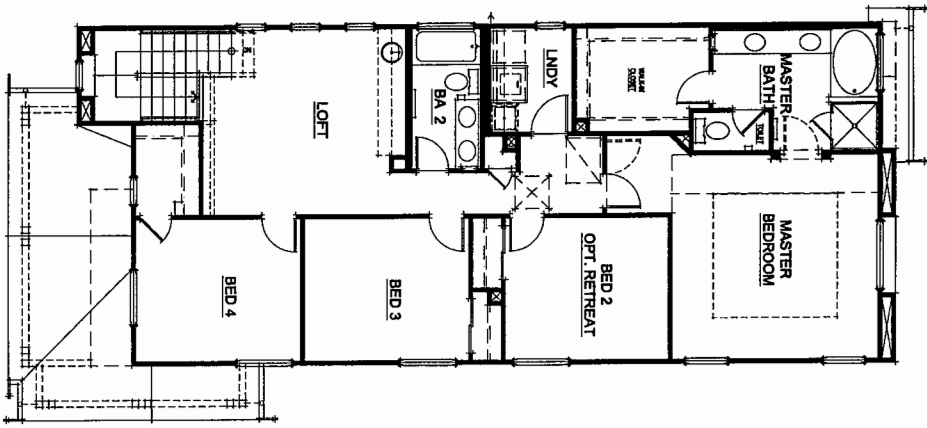
PLAN 3
PASEO HOMES

R O L I I n g W O O D

V A I L E J O , C A L I F O R N I A



S O G A R E S H I P S O I N C .
S A N A N T O N I O , C A



SECOND FLOOR PLAN

SQUARE FOOTAGE:	
FIRST FLOOR:	891 SQ. FT.
SECOND FLOOR:	1987 SQ. FT.
2-CAR GARAGE:	2288 SQ. FT.
LOFT:	259 SQ. FT.

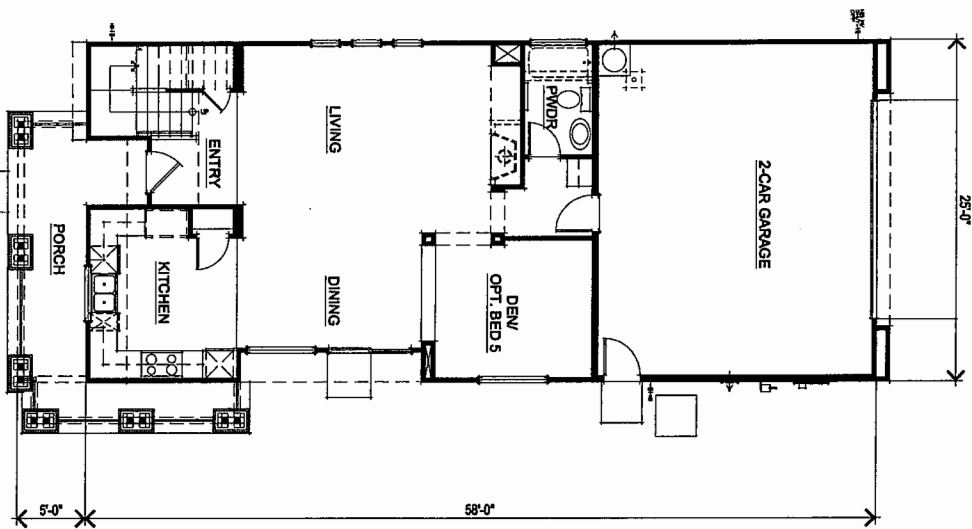
PLAN 3

ALLEY LOAD @ EXTENDED PORCH PRODUCT

R O L I I N G W O O D

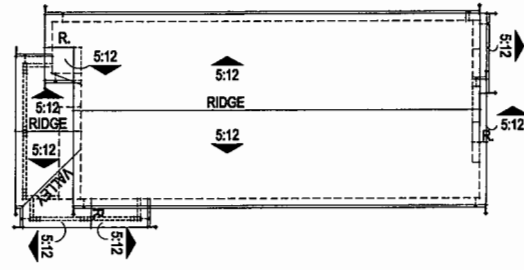
V A I L O C A L I F O R N I A

S D G A R C H I T E C T S, I N C.
 823.434.7609
 06 August 07

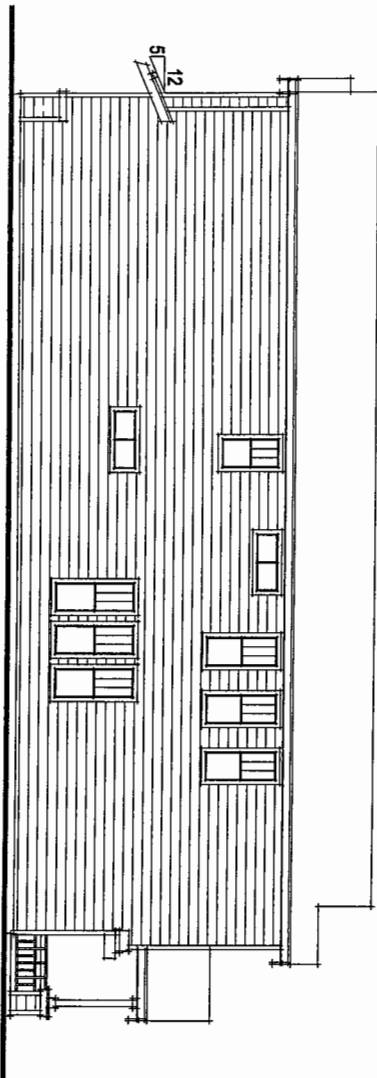


FIRST FLOOR PLAN

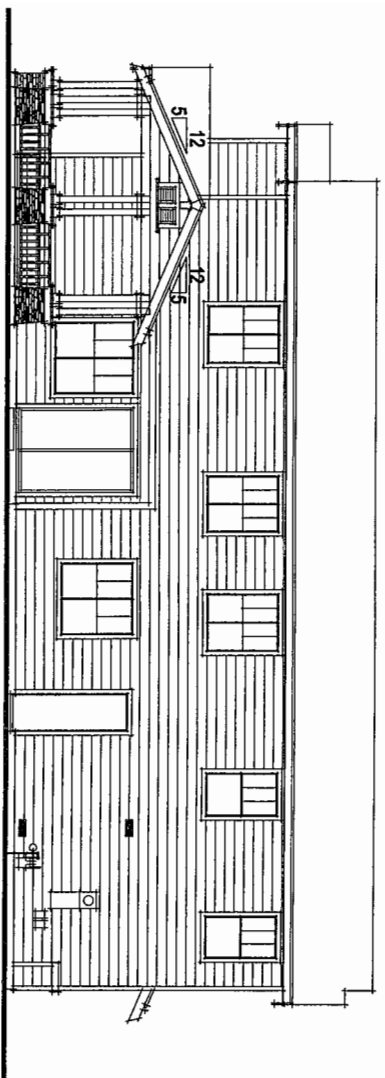




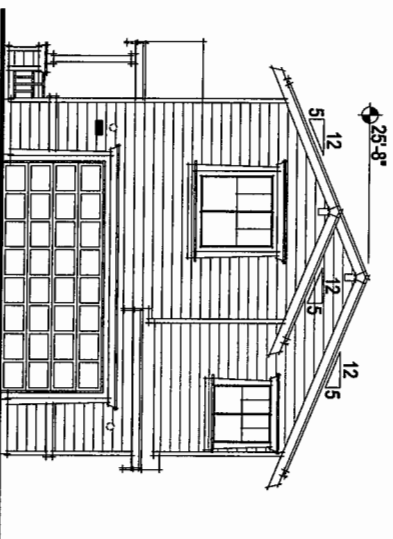
ROOF PLAN 'B'



LEFT ELEVATION 'B'



RIGHT ELEVATION 'B'



REAR ELEVATION 'B'

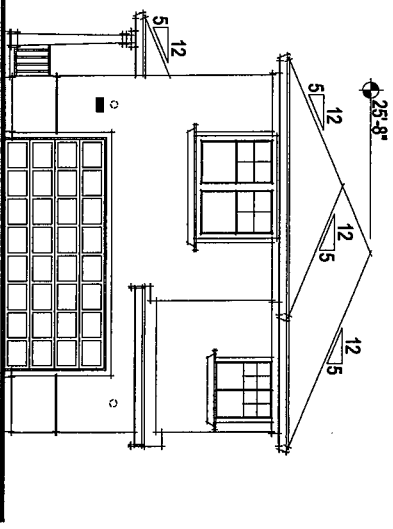
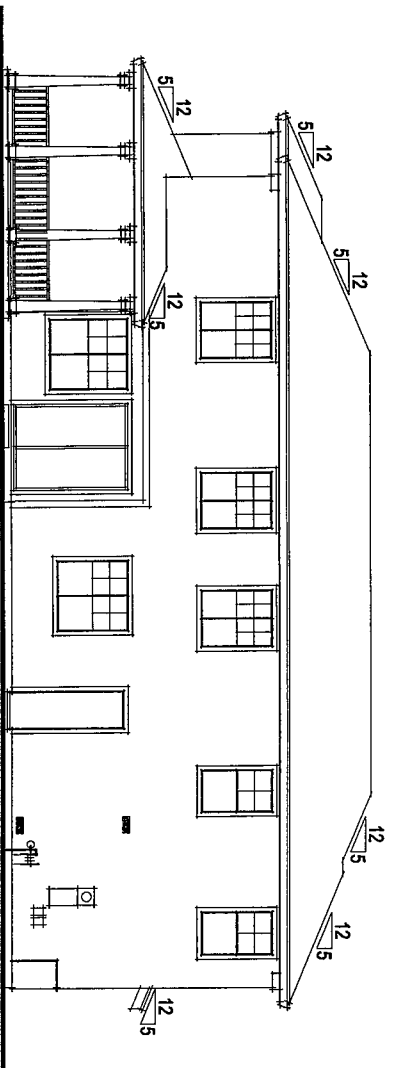
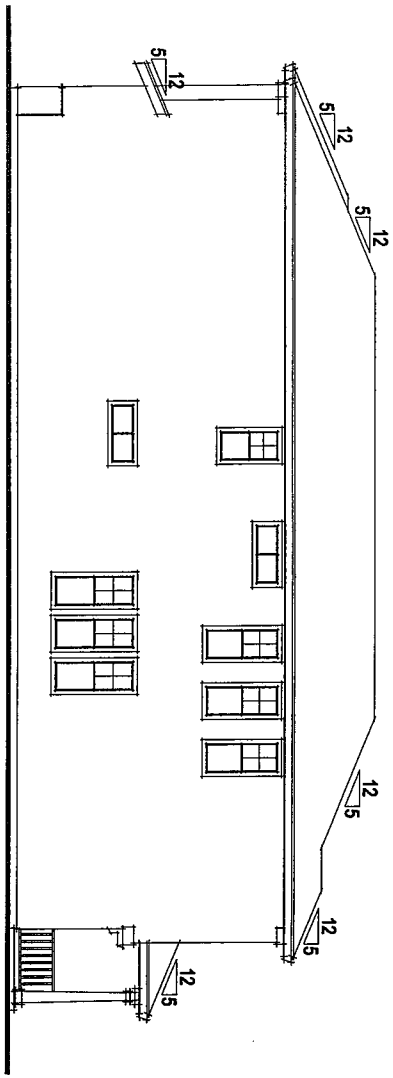
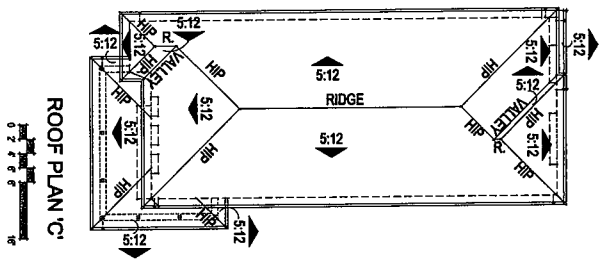
PLAN 3
ALLEY LOAD @ EXTENDED PORCH PRODUCT

R O L I I n G W O O D

V A I L E J O C A L I F O R N I A



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926.934.7000



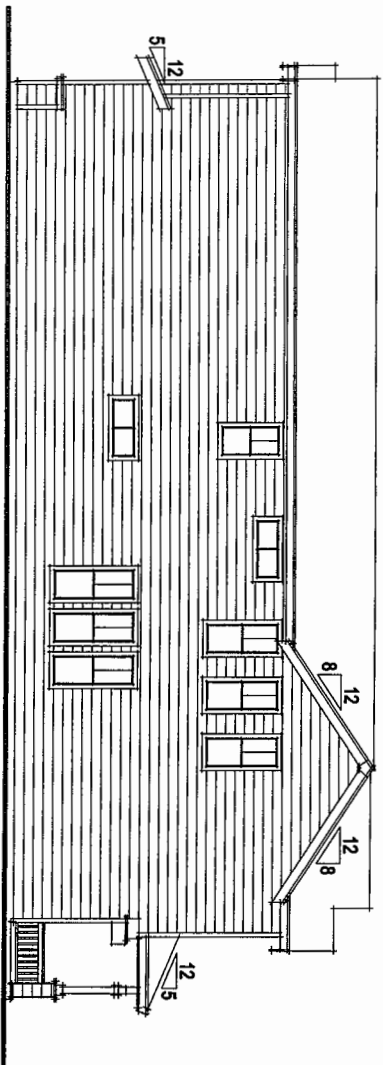
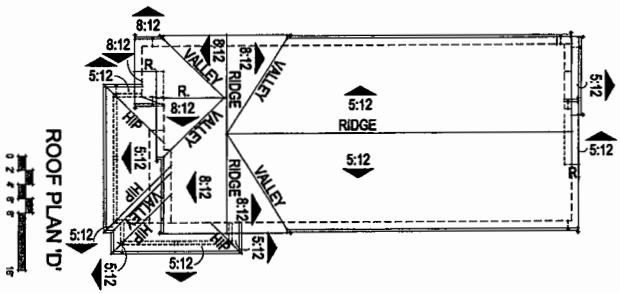
PLAN 3
ALLEY LOAD @ EXTENDED PORCH PRODUCT

R o l l i n g w o o d

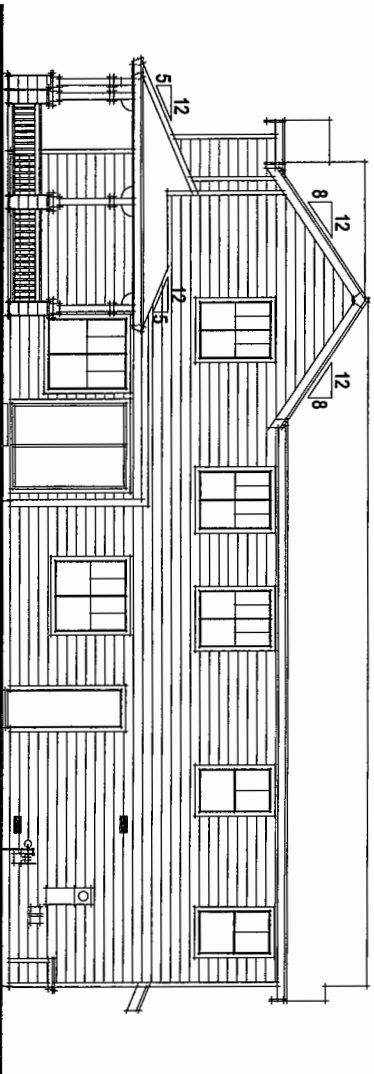
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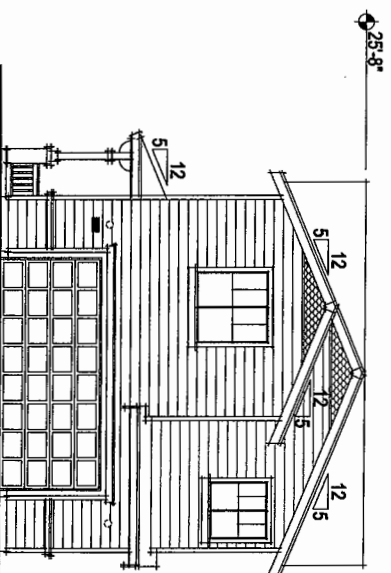
S O G A R E S H A I T S E I S . I N C.
855 N. H. 17900
Mesa, Arizona 85209



LEFT ELEVATION 'D'



RIGHT ELEVATION 'D'



REAR ELEVATION 'D'



PLAN 3
ALLEY LOAD @ EXTENDED PORCH PRODUCT

R O I L I n g W O O d

V a l i e j o C a l i f o r n i a



S O G A R T I S T S A N D A R C H I T E C T S I N C .

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Photo 2



Photo 4



Photo 1

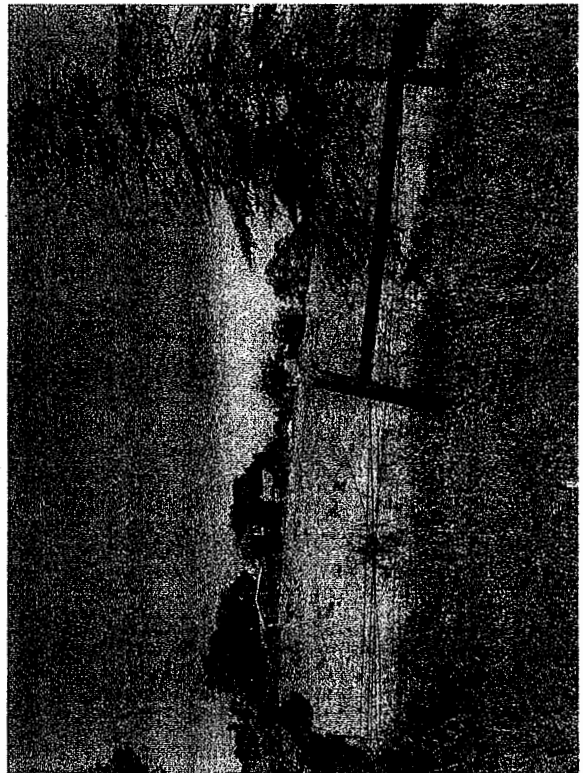


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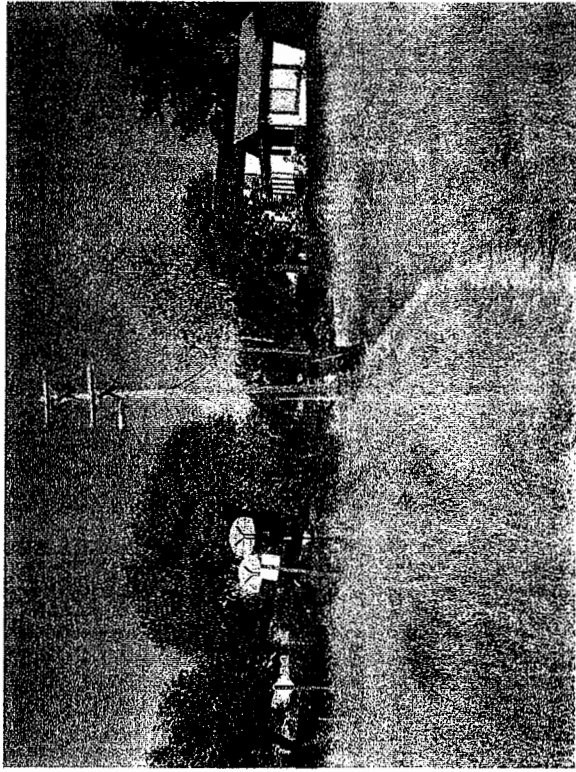


Photo 6



Photo 8



Photo 5

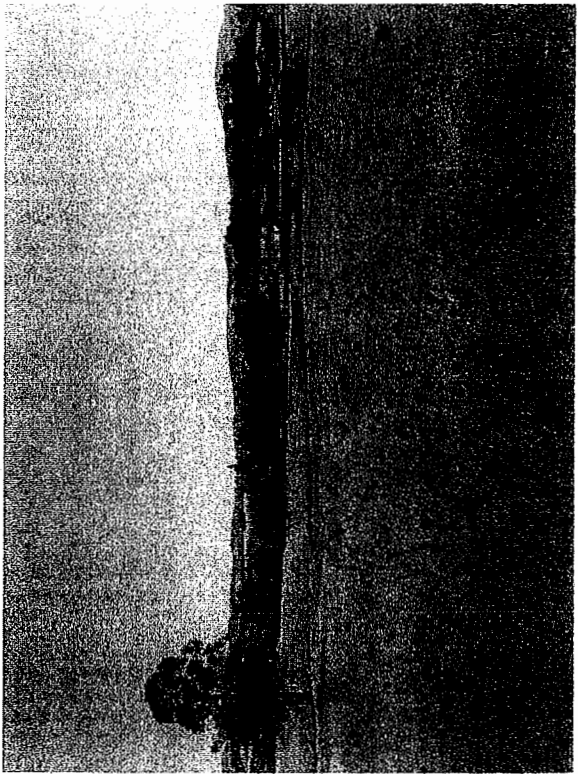


Photo 7



Photo 9



Photo 10

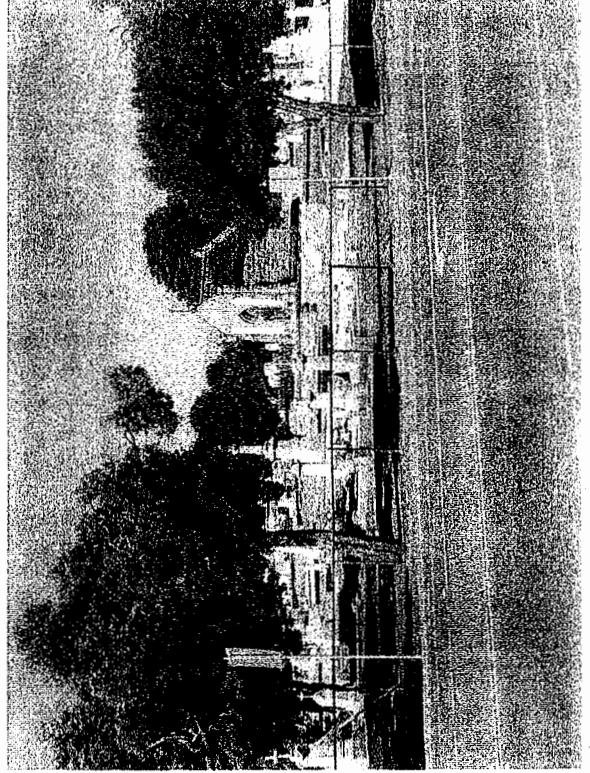


Photo 11



Photo 12

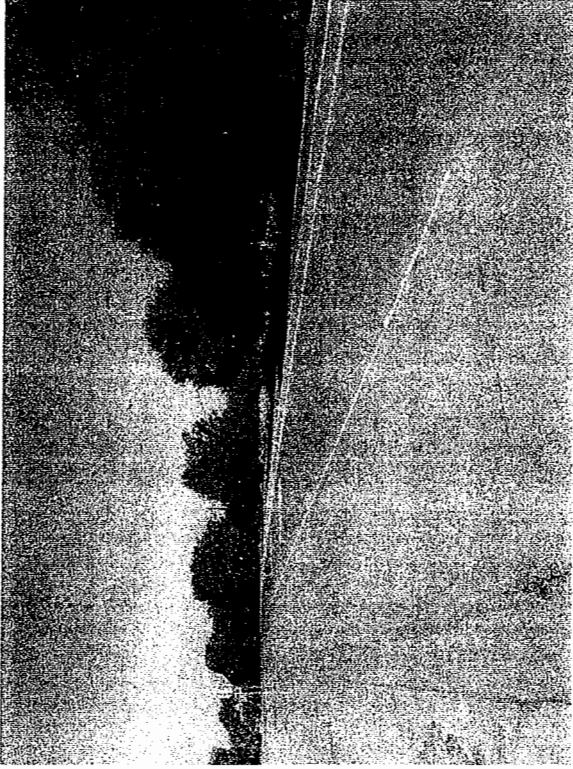


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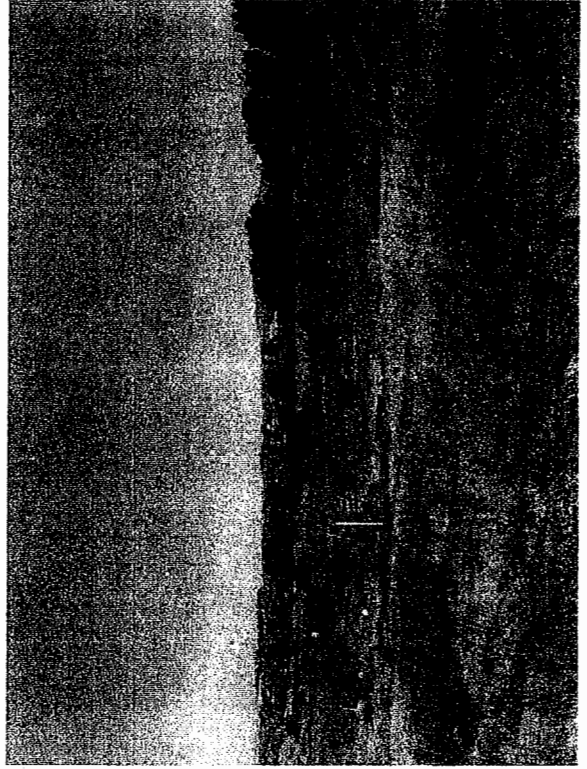


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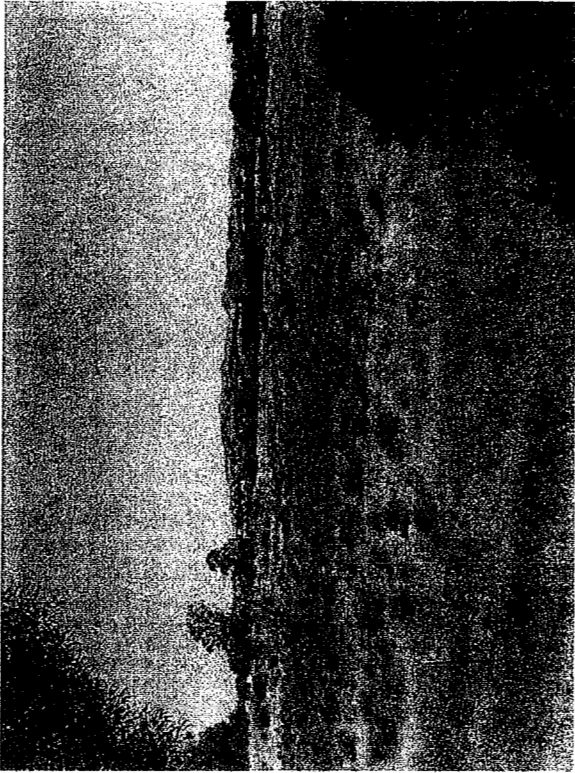


Photo 13



Photo 15



Photo 18

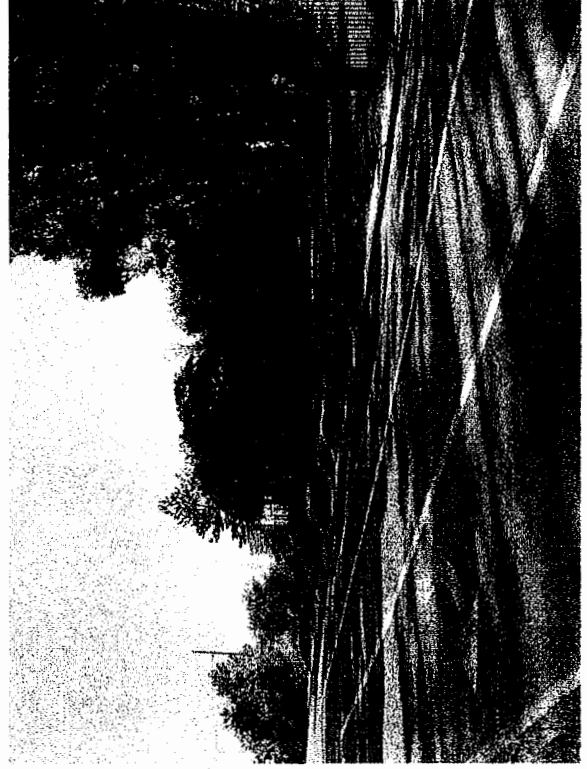


Photo 20



Photo 17



Photo 19

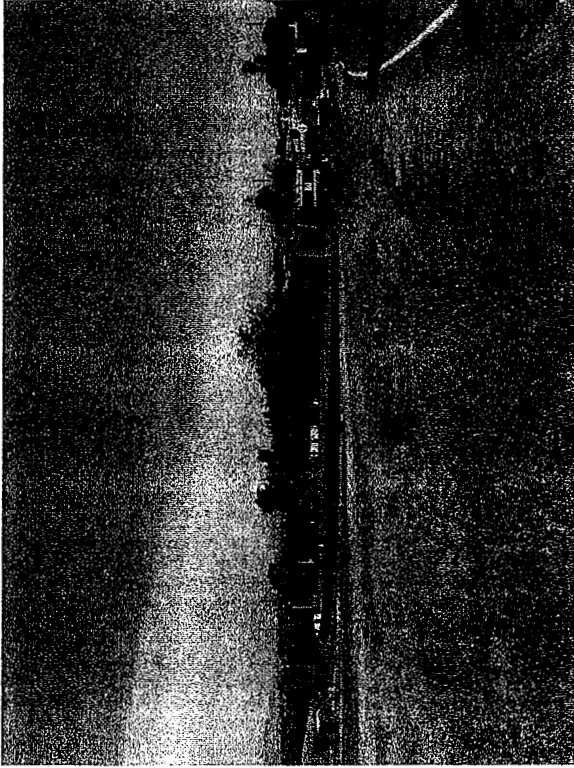


Photo 22



Photo 24

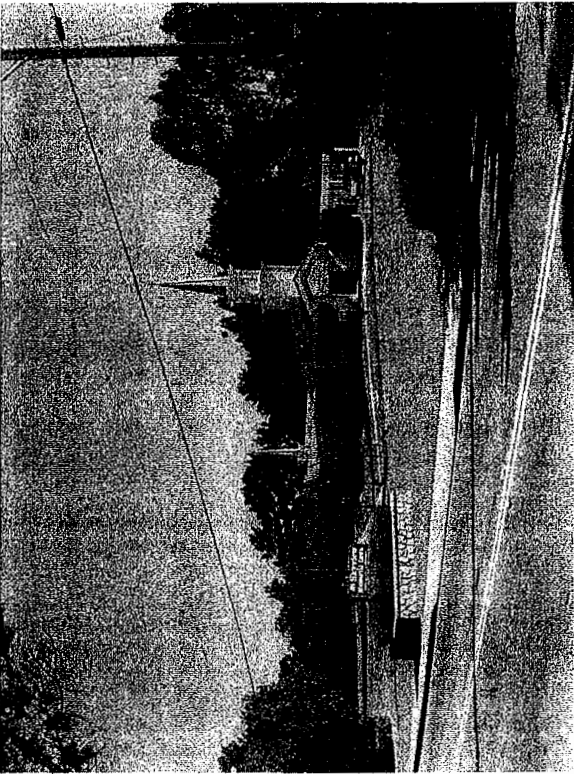


Photo 21



Photo 23

May 28, 2007
Job No. 061033

Project Description
Rollingwood
Vallejo, California

Existing Setting

The Rollingwood property is located at the northwest corner of Benicia Road and Rollingwood Avenue in southeastern Vallejo. The 31.8 acre site currently contains two functional baseball fields and ancillary buildings at the north end of the property. The southern end of the property is primarily vacant with the exception of a small one-story structure near the southeast corner of the site. The existing terrain is varied and the site has been previously graded. A portion of the site was formerly used as a drive-in movie theater. Elevations range from a low of about 140' at the northwest corner of the site to a high point of 200' along Rollingwood Avenue. This site generally drains from east to west. The vacant portions of the site are primarily covered in native grasses/shrubs. Benicia Road and Rollingwood Avenue are bordered by a row of trees. Photos of the site and surrounding area are shown on pages 7-13.

Surrounding Uses

Existing land uses surrounding the property include a single family housing development to the north, rural residential homes and scattered businesses to the west, and cemeteries to the south and east. There is a single family residential area along Pope Drive across from the northwest corner of the site. The zoning districts for the surrounding properties are as follows:

North:	Low Density Residential
East:	Low Density Residential and Public & Quasi-Public Facilities
South:	Public & Quasi-Public Facilities
West:	Rural Residential (Solano County)

Existing and Proposed Ownership

The project site contains 4 separate parcels owned by the Vallejo Unified School District, City of Vallejo, and Syufy Enterprises.

KB Home is under contract to purchase the lands owned by the School District and Syufy. The project includes utilizing the City of Vallejo's 4.3 acre portion of the site for a public park. The applicant plans to construct the proposed improvements and provide a turnkey park to GVRD. The City's portion of the site is an irregularly shaped parcel located along Rollingwood Drive. KB Home proposes to relocate the park site within the project to create a centrally located park with a rectangular shape that would allow for maximum efficiency of use.

Proposed Land Use

The current General Plan land use designation for the site is Retail. The existing zoning is PF – Public and Quasi-Public Facilities and CP – Pedestrian Commercial. KB Home proposes a General Plan Amendment to change the land use designation of the site from Retail to Low Density Residential. The site will also be rezoned from PF (Public Facilities) and CP (Pedestrian Commercial) to PDR (Planned Development Residential). The proposed PDR zoning will allow for flexibility in the site layout and design of the development. The proposed residential use would be compatible with the surrounding low density residential land uses and provide for new housing in the southwestern portion of the City.

Proposed Plan

The applicant proposes a planned residential development featuring 214 housing units and a 4.1 acre park. The proposed southerly entrance road from Rollingwood Avenue divides the project into two distinct neighborhoods: Conventional single family homes on 45'x75' lots to the north, and paseo homes to the south. Each neighborhood has its own design character and unit types. Three plan types are proposed for the single family neighborhood ranging from 2,114 - 2,424 s.f. The paseo homes will range from 1,942 – 2,248 s.f. The proposed Land Use Summary is presented below:

Land Use Summary				
Land Use	# Units	Acres	Density (du/ac)	% AC
Single Family Residential (45'x75')	104	15.6	6.7	49.1
Paseo Homes	110	12.1	9.1	38.0
Park	-	4.1	-	12.9
Project Total	214	31.8	7.7*	100%

*Gross density less park

The proposed single family neighborhood is located along the site's northern and eastern boundaries, adjacent to existing single family neighborhoods. The higher density paseo homes are located along Benicia Road.

The paseo homes consist of 2 story homes with garages at the rear of the homes that are accessed by private alleys. The entry to these homes is provided either off of Street D, Street G, Rollingwood Avenue or from a landscaped paseo on the opposite side of the home from the garages. The landscaped paseo is minimum of 24' wide between buildings and features a pathway for pedestrian circulation. Additional paseo's are oriented in a north/south direction to provide pedestrian access to Street D and the neighborhood park. Each home has a private patio at the front of the house as well as useable side yard space including private yard easements on the side yards. Shared open spaces are scattered throughout the development.

The proposed 4.1 acre park is centrally located within the site. The types of recreational uses to be included in the park are still being formulated by the City, the applicant and GVRD. The site could include a sports field(s), picnic areas, and play structures.

Circulation

Vehicular and Pedestrian access to the site will be provided from Benicia Road and Rollingwood Avenue. Benicia Road is currently a 2-lane arterial road connecting the cities of Vallejo and Benicia. Rollingwood Avenue is a collector road extending northward from Benicia Road and serves the residential areas to the north. South of Benicia Road, Rollingwood Avenue becomes Glen Cove Road which provides access to Highway 780. The Benicia Road/Rollingwood Avenue intersection is currently signalized.

Vehicle and pedestrian access to the site is available from both Benicia Road and Rollingwood Avenue. Access to the project will be provided by 3 entrances along Rollingwood Avenue and one entrance intersecting Benicia Road. The middle street (Street E) intersecting Rollingwood Avenue will provide direct vehicular and pedestrian access to the proposed park. The northern entrance will align with Pope Drive to the east. All of the internal streets provide convenient access to the park.

Two public street sections are proposed within the proposed development. Street E is 36' curb-to-curb with parking on both sides of the street. This street includes a detached 5' sidewalk with a 5' planter strip between the sidewalk and the curb. The remainder of the public streets conforms to the City standard 50' ROW with a 36' curb-to-curb dimension with attached sidewalks.

The paseo homes are served by private 22' wide alleys providing access to the garages at the rear of the homes. 65 guest parking spaces are provided in open parking bays and 56 on-street spaces are available on Streets D and G.

Phasing

The project will be developed in one phase.

Water Supply

The project will be served by the City of Vallejo water system. The current system is inadequate for the amount of development currently planned therefore the project will be required to contribute to the over-all system as well as to construct limited project specific off-site improvements. The Rollingwood project will contribute a fee to be used by the City towards the construction of an 18" water line from the Benicia/Rollingwood intersection to the Keats/Devlin intersection and/or the demolition of two existing 0.5 million gallon water tanks and the construction of a new approximately 2.5 million gallon water tank.

In addition, the project will be required to install approximately 1,150 lineal feet of 12" water line along the project frontage in Benicia Road connecting to a newly installed 12"

water line that currently ends near the western property line. The existing 6" line in this location will be abandoned in place. Also, roughly 350 lineal feet of 12" water line will be installed in Hargus Avenue connecting the existing system and abandoning an existing 6" line in place.

On-site improvements include a 12" water line from Hargus Avenue, along the west and then south side of the park and connecting to both Rollingwood Drive and Benicia Road. The remainder of the site will be served with 8" mains.

The single family homes will have standard water services and meters. The paseo homes will have a main running through the common driveways. The mains will have master meters as they leave the City right of way for Water Division billing purposes and each paseo home will be sub-metered to be read by the Home Owner's Association.

Sanitary Sewer

Sewage will be collected and conveyed in 8" main lines from the subdivision and taken to the existing system in existing easements on the Second Baptist Church property via Hargus Avenue. The existing system between Hargus Avenue and the connection point on the Church property will be removed and replaced at a lower elevation to allow gravity flow. The existing line between the tract boundary and the most down stream manhole on Hargus Avenue will be protected in place while a new, lower, main line is installed to serve the site. Existing services between the project and the most down stream manhole on Hargus Avenue will remain connected to the higher existing line.

Storm Drainage

Of the 31.8+/- project acres, approximately 6 acres currently drain to a swale that straddles the northern boundary of the project. The swale drains to an existing storm drain system in Dryden Drive. The project proposes to reduce the tributary area to this swale to 2.1+/- acres thus decreasing the flow to Dryden Drive.

Storm runoff from the rest of the site will be collected in standard inlets and piped to the park to travel through landscaping for treatment. From the park, storm water will drain via gravity flow to Benicia Road. In order to accomplish gravity flow approximately 450 lineal feet of existing storm drain will be removed and replaced at a lower elevation.

Project Architecture

Two neighborhoods are proposed for the Rollingwood community, each of which utilizes a distinct housing type. There are a total of seven architectural elevation styles between the two neighborhoods: French, Tuscan, Craftsman, Bungalow, Traditional, Italianate, and Victorian. These elevation styles are articulated through the use of such materials as Hardi Plank siding, stucco, and wrought iron.

The community is also characterized by two housing types, Traditional Lot Homes and Paseo homes.

Traditional Lot Home – Single family detached homes consisting of three floor plans on traditionally sized lots with front porches and attached garages oriented to small residential streets. Each plan also has a private fenced back yard. Elevation styles for these homes are French, Tuscan, and Craftsman.

Paseo Homes – Single family detached homes consisting of three floor plans on smaller lots with attached garages loading from a private rear ally, and front porches and entries fronting a common pedestrian-only “paseo.” In addition, each plan has a private fenced courtyard patio oriented to the side yard of each home. The side yards between homes will be in a “zero-lot line” configuration using side yard easements. This will allow each home to have an 8’ useable side yard space.

Project Landscaping

The landscape at Rollingwood has been designed to enhance both housing types within the project. The landscaping for the single family lots consists of front yard landscaping, including lawns, shrubs, groundcovers and street trees with individual homeowner maintained irrigation systems. Paseo home landscaping shall consist of landscaped paseos with connecting walks, seating areas, gazebos, special accented entries, lawns, trees, shrub and groundcover areas. All planting areas will be irrigated with a common irrigation system which will be maintained by future homeowners association.

A 40’+ landscape buffer separates Benicia Road and Rollingwood Avenue from the paseo homes. The landscape buffer includes meandering sidewalks, retaining walls, benches, lawn areas, trees, shrubs and groundcovers. All improved landscape areas will be irrigated with an automatic irrigation system.

Development Standards

Residential development regulations have been established to ensure that the proposed buildings and site plan achieve the desired character and development quality. The regulations set forth the minimum requirements necessary for planning of the property. Refer to the City of Vallejo Zoning Ordinance for any regulations not specified below.

	Single Family (45'x75')	Paseo Homes
Minimum Lot Area	3,375 S.F.	2,359 S.F.
Setbacks:		
Front Yard	15' minimum to bldg. 10' minimum to porch	9.5' minimum to bldg. 4' minimum to porch
Rear Yard (Garage)	10' min.	4' min. to face of garage
Side Yard	5' min.	4' min. to the property-line -- 8' between buildings with additional yard easement
Minimum Building Separation	10' min.	8' min.
Max. Height Limit	32' 1 1/2"	25' 7 1/2"
Max Lot Coverage	46.5%	65.2%

Accessory Structures

Allowed in single family neighborhood per the requirements of the Low Density Residential District. Accessory structures are not permitted in the paseo home neighborhood.

Permitted and Conditional Land Use

Allowed per the requirements of the Low Density Residential District.

Parking/Garages

Single Family: Each unit shall be provided with a two-car garage and full driveway apron. 166 on-street guest parking spaces will be available.

Paseo Homes: Each unit shall be provided with a two-car garage. 65 guest spaces are provided in open parking bays. Guest parking is provided at a ratio of .6 spaces per unit. On-street guest parking is also available on Streets D and G.

ROLLINGWOOD DRIVE

INITIAL STUDY AND MITIGATED NEGATIVE DECLARATION



City of Vallejo | October 11, 2007



DESIGN, COMMUNITY & ENVIRONMENT

Draft

ROLLINGWOOD DRIVE

INITIAL STUDY AND MITIGATED NEGATIVE DECLARATION

Submitted to

City of Vallejo

October 11, 2007



DESIGN, COMMUNITY & ENVIRONMENT

1625 SHATTUCK AVENUE, SUITE 300
BERKELEY, CALIFORNIA 94709

TEL: 510 848 3815
FAX: 510 848 4315

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Appendix A Traffic Analysis

CITY OF VALLEJO
ROLLINGWOOD DRIVE DRAFT INITIAL STUDY AND
MITIGATED NEGATIVE DECLARATION
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MITIGATED NEGATIVE DECLARATION

The following proposed project has been reviewed pursuant to the provisions of the California Environmental Quality Act (CEQA) for the purpose of determining the likelihood of a significant adverse environmental impact occurring as a result of project completion.

LEAD AGENCY

City of Vallejo, Community Development Department
555 Santa Clara Street, Vallejo, CA 94590

CONTACT PERSON AND PHONE NUMBER

Katherine Donovan, (707) 648-4327

CONSULTING FIRM

Design, Community & Environment (DC&E)
1625 Shattuck Avenue, Suite 300
Berkeley, CA 94709

PROJECT TITLE

Rollingwood Drive Initial Study and Mitigated Negative Declaration

DESCRIPTION OF PROJECT

See attached project description

PROJECT LOCATION

Northwest corner of Benicia Road and Rollingwood Drive in southeastern Vallejo

APN: 0072-170-050, 0072-170-060, 0072-170-200, 0072-170-210

Please see Figure 2-2 of this Initial Study.

DETERMINATION

The proposed project would not have a significant effect on the environment. As described in the attached Initial Study, all environmental issue areas have been considered, and any potentially significant impacts would be mitigated as part of the mitigation measures specified in the Initial Study. The envi-

ronmental analysis results in less-than-significant impacts regarding the issues of most concern: biological resources, cultural resources, hydrology, noise, traffic, and geology. Air quality is a regional concern, which the project would not impact significantly.

See attached Initial Study for documentation to support the findings.

MITIGATION MEASURES

The following mitigation measures are included in the Initial Study to avoid potentially significant impacts.

1. Mitigation Measure AQ-1: To ensure incorporation of appropriate Clean Air Plan TCMs, the features listed below shall be included in the project. Implementation of the measures listed below along with the City's General Plan requirements would reduce vehicle use and vehicle miles traveled consistent with the current Clean Air Plan TCMs:
 - ◆ Bicycle amenities should be provided. This would include appropriate bike lane connections and bicycle parking at the planned park.
 - ◆ Provide appropriate transit amenities in coordination with the City and Vallejo Transit (Route 6), such as sheltered bus stops and bus pullouts.
 - ◆ Implement a landscape plan that provides shade trees along adjacent sidewalks and at bus stops.
2. Mitigation Measure AQ-2: The applicant shall include the following measures to control dust and exhaust emissions:
 - ◆ Water all active construction areas at least twice daily and more often during windy periods. Active areas adjacent to residences should be kept damp at all times.
 - ◆ Cover all hauling trucks or maintain at least two feet of freeboard. Dust-proof chutes shall be used as appropriate to load debris onto trucks during demolition.

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- ◆ Pave, apply water at least twice daily, or apply (non-toxic) soil stabilizers on all unpaved access roads, parking areas, and staging areas.
- ◆ Sweep daily (with water sweepers) all paved access roads, parking areas, and staging areas and sweep streets daily (with water sweepers) if visible soil material is deposited onto the adjacent roads.
- ◆ Hydroseed or apply (non-toxic) soil stabilizers to inactive construction areas (i.e., previously-graded areas that are inactive for 10 days or more).
- ◆ Enclose, cover, water twice daily, or apply (non-toxic) soil binders to exposed stockpiles.
- ◆ Limit traffic speeds on any unpaved roads to 15 mph.
- ◆ Replant vegetation in disturbed areas as quickly as possible.
- ◆ Suspend construction activities that cause visible dust plumes to extend beyond the construction site.
- ◆ During renovation and demolition activities, removal or disturbance of any materials containing asbestos, lead paint or other hazardous pollutants will be conducted in accordance with BAAQMD rules and regulations.
- ◆ Prohibit use of “dirty” equipment. Equipment with noticeably dirty emissions shall be prohibited from operation at the site until proper maintenance has been performed to reduce the visible emissions to acceptable levels. If necessary, opacity shall be used as an indicator of exhaust particulate emissions from off-road diesel powered equipment, where the project shall ensure that emissions from all construction diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately.
- ◆ Signs shall be clearly posted at the construction site indicating that diesel equipment standing idle for more than five minutes shall be turned off. This would include trucks waiting to deliver or receive

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soil, aggregate, or other bulk materials. Rotating drum concrete trucks could keep their engines running continuously as long as they are onsite.

- ◆ Properly tune and maintain equipment in accordance with manufacturer specifications.
 - ◆ During grading, stage construction equipment at least 200 feet away from any sensitive uses.
 - ◆ The contractor shall install temporary electrical service whenever possible to avoid the need for independently powered equipment (e.g., compressors).
 - ◆ A Disturbance Coordinator will be assigned to the project when active construction is occurring within 200 feet of residences. This coordinator will ensure that all air quality construction period mitigation measures are enforced. In addition, the Disturbance Coordinator will respond to complaints from the public regarding air quality issues in a timely manner. The contact information for this Coordinator will be posted in plain view at the project site.
 - ◆ A Disturbance Coordinator shall suspend construction activities that cause visible dust plumes to extend beyond the construction site.
3. Mitigation Measure BIO-1: The applicant shall obtain all necessary permits from the Army Corps of Engineers (the Corps), U.S. Fish and Wildlife Service (USFWS), California Department of Fish and Game (CDFG), and the Regional Water Quality Control Board (RWQCB) as required by federal and State law to avoid, minimize or offset impacts to any species listed under either the State or federal Endangered Species Acts or protected under any other State or federal law as follows:
- ◆ Before project implementation, a delineation of waters of the United States, including wetlands that could be affected by development, shall be made by a qualified wetland specialist through the formal Clean Water Act (CWA) Section 404 process.

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- ◆ If based on the verified delineation, it is determined that fill of waters of the United States would result from project implementation, authorization for such fill shall be secured from the Corps through the Section 404 permitting process and from the RWQCB as part of the Section 401 water quality certification process.
 - ◆ Consultation or incidental take permitting may be required under the Endangered Species Act (ESA). The applicant shall obtain all legally-required permits from the USFWS for the “take” of protected species under the ESA.
 - ◆ Evidence that the applicant has secured any required authorization from these agencies shall be submitted to the Vallejo Planning Department prior to issuance of any grading or building permits for the project.
4. Mitigation Measure BIO-2: Any active raptor or other bird nests in the vicinity of proposed grading shall be avoided until young birds are able to leave the nest (i.e., fledged) and forage on their own. Avoidance may be accomplished either by scheduling grading and vegetation removal during the non-nesting period (September through February), or if this is not feasible, by conducting a pre-construction survey for active nests. Provisions of the pre-construction survey and nest avoidance, if necessary, shall include the following:
- ◆ If grading is scheduled during the active nesting period (March through August), a qualified wildlife biologist shall be retained by the applicant to conduct a pre-construction nesting survey no more than 30 days prior to initiation of grading to provide confirmation on presence or absence of active nests in the vicinity.
 - ◆ If active nests are encountered, species-specific measures shall be prepared by a qualified biologist in consultation with the CDFG and implemented to prevent nest abandonment. At a minimum, grading in the vicinity of the nest shall be deferred until the young birds have fledged. A nest-setback zone of at least 300 feet shall be established for raptors and 100 feet for loggerhead shrike and passerine birds

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within which all construction-related disturbances shall be prohibited. The perimeter of the nest-setback zone shall be fenced or adequately demarcated, and construction personnel restricted from the area.

- ◆ If permanent avoidance of the nest is not feasible, impacts shall be minimized by prohibiting disturbance within the nest-setback zone until a qualified biologist verifies that the birds have either a) not begun egg-laying and incubation, or b) that the juveniles from the nest are foraging independently and capable of independent survival at an earlier date. A survey report by the qualified biologist verifying that the young have fledged shall be submitted to the Vallejo Planning Department prior to initiation of grading in the nest-setback zone.
5. Mitigation Measure BIO-3: Focused surveys for the special-status plant species considered to have a remote potential for occurrence on the site shall be conducted during the flowering season by a qualified botanist to confirm absence of these species from the site. Species of concern identified in the BRA include: bent-flowered fiddleneck (*Amsinckia lunaris*), Congdon's tarplant (*Centromadia parryi* ssp. *congdonii*), dwarf downingia (*Downingia pusilla*), Santa Cruz tarplant (*Holocarpus macradenia*), Contra Costa goldfields (*Lasthenia conjugens*), and legenere (*Legenere limosa*). If populations of special-status plant species are found, then a detailed mitigation plan shall be prepared which includes specifics on the species, habitat and natural community preservation/conservation strategies designed to protect this species, and compensatory mitigation developed in accordance with any CDFG and USFWS requirements. If no special-status plants are found during the focused surveys, then no further mitigation would be required.
 6. Mitigation Measure BIO-4: Directed surveys shall be conducted for Monarch butterfly, vernal pool fairy shrimp, burrowing owl, and pallid bat as recommended in the BRA by the applicant's biological consultant. Appropriate mitigation shall be provided in the remote instance that any of these species are encountered on the site. Mitigation shall be coordi-

nated with representatives of the CDFG and/or USFWS as required by law, and may include on-site avoidance, compensatory mitigation, or other acceptable mitigation strategies. If none of these special-status animal species are found during the focused surveys, then no further mitigation would be required.

7. Mitigation Measure BIO-5: A Wetland Mitigation Program shall be prepared by a qualified wetland specialist to provide for the protection, replacement, and management of jurisdictional waters on the site affected by proposed development. The Wetland Mitigation Program shall include the following components and meet the following standards:
 - ◆ Proposed grading and development shall be redesigned to preferably avoid removal or adverse impacts to areas verified as jurisdictional wetlands. Where avoidance is infeasible, adequate mitigation for any direct or indirect impacts on jurisdictional waters shall be provided as coordinated with the Corps and/or RWQCB. Replacement wetlands shall be provided at a minimum 2:1 replacement ratio and shall be established in suitable locations within undeveloped open space areas, preferably on-site but possible off-site if an acceptable location is selected and approved by jurisdictional agencies. The wetlands replacement component of the Wetland Mitigation Program shall emphasize establishment of native freshwater marsh and seasonal wetlands to replace and enhance existing habitat values.
 - ◆ The wetland replacement component of the Wetland Mitigation Program shall specify performance criteria, maintenance and long-term management responsibilities, monitoring requirements, and contingency measures. Monitoring shall be conducted by the qualified wetland specialist for a minimum of five years and continue until the success criteria are met.
8. Mitigation Measure CUL-1. In the event that unique historical, archeological paleontological or geologic features are discovered during construction, a qualified archeologist or geologist shall be contacted immediately to determine the significance of the resource. Construction activi-

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ties shall be halted and not commence until the expert has issued an opinion about the resource and appropriate mitigation has been determined.

9. Mitigation Measure CUL-2. In the event that human remains are discovered on the project site during construction activities, the following steps shall be taken according to CEQA Guidelines Section 15064.5 (e)(1). First there shall be no further excavation of the site or any nearby area until the Solano County coroner is contacted to determine that no investigation of the cause of death is required. Second, if the coroner determines that the remains are of Native American decent, the coroner will contact the Native American Heritage Commission within 24 hours.
10. Mitigation Measure GEO-1: In portions of the site where clayey soils and fill material will not be removed during grading and where proposed improvements (new fill, pavements, slabs on grade) will be constructed, these existing sub-surface materials should be removed and recompacted. The process may consist of removing the upper two feet of these materials, and scarifying and re-compacting the lower one foot prior to placing new fill or improvements. The process will need to be confirmed during a future geotechnical investigation. Subsequent to this investigation and prior the issuance of a grading permit, the applicant shall submit a plan to the City for review and approval, which will identify the steps for removal or treatment of soils on-site.
11. Mitigation Measure GEO-2: To reduce the potential for post-construction distress to the proposed residential structures resulting from swelling and shrinkages of clayey soils, post-tensioned slabs should be used for foundation support over the engineered and compacted fill after the removal of the clay-rich overburden.
12. Mitigation Measure HAZ-1: Prior to soil sampling, the applicant shall review the Kleinfelder study at the DTSC Sacramento offices. Depending on the extent of the soil sampling completed to date and the determinations of the Kleinfelder study, additional sampling shall be conducted

to better define the extent of contamination on-site. Once sufficient sampling has been completed in accordance with DTSC guidelines, the samples shall be evaluated and mitigation measures developed, as appropriate.

13. Mitigation Measure HAZ-2: Before any soil can be exported off-site it shall be profiled for landfill disposal requirements. If harmful concentrations of pesticides, lead and arsenic are reported in the soil, per DTSC guidelines, the contaminated soil shall not be reused for landscaping. Rather, landscaping soils shall be imported from an off site source.

14. Mitigation Measure NOISE-1: Where noise-sensitive exterior use areas would be exposed to an Ldn greater than 60 dBA, mitigation would be required to reduce noise levels to meet exterior and interior noise level limits.
 - ◆ Construct noise barriers to reduce exterior noise levels to 60 dBA Ldn or less as shown in Figure 4-4. If the City determines that it is not economically or aesthetically reasonable to meet the 60 dBA Ldn standard, construct noise barriers as shown in Figure 4-5 to reduce noise levels to 65 dBA Ldn or less. Noise barriers shall be constructed out of materials with a minimum surface weight of 3 lbs./ft². Suitable materials include 1-in. thick wood fence boards, concrete, masonry, or metal provided that the barrier is solid over its face and at its base. In locations identified for noise barriers, the applicant shall examine the creation of earthen berms as an alternative option. Provided that berms can reduce potential impacts to a less than significant level and would not conflict with right-of-way constraints, the applicant shall coordinate with the City to identify the preferable option. The applicant shall also examine the option of combining noise barriers and berms, such as a four foot berm atop a four foot wall to achieve comparable noise reduction.

 - ◆ Retain a qualified Acoustical Specialist to prepare for City review and approval a detailed acoustical analysis of interior noise reduction requirements and specifications for all units proposed within the 60

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dBa Ldn contours of area roadways, in accordance with City standards. Interior noise levels must be maintained at or below 45 dBA Ldn. Building sound insulation requirements shall include forced air mechanical ventilation in noise environments exceeding 60 dBA Ldn. Special building construction techniques (e.g., sound-rated windows and building facade treatments) may be required where exterior noise levels exceed 65 dBA CNEL. Because the noise exposure is between 65 dBA Ldn and 70 dBA Ldn these treatments would be limited to sound rated windows and doors at the most affected rooms. Special sound rated exterior wall assemblies would not be required. The specific determination of required door and window sound transmission ratings shall be made on a unit-by-unit basis during project design. Results of the analysis, including the description of the necessary noise control treatments, shall be submitted to the City along with the building plans prior to issuance of a building permit.

15. Mitigation Measure TRAF-1: Prior to project approval, the project applicant shall revise the site plan to include turnaround space at the ends of the alleys for garbage trucks and emergency vehicles. The project applicant shall coordinate with the City of Vallejo to determine the specific requirements for these turnarounds.

I INITIAL STUDY SUMMARY

1. **Project title:** Rollingwood Drive Initial Study and Mitigated Negative Declaration
2. **Lead agency name and address:**
City of Vallejo, Community Development Department
555 Santa Clara Street
Vallejo, CA 94590
3. **Contact person and phone number:**
Katherine Donovan, (707) 648-4327
4. **Project location:**
Northwest corner of Benicia Road and Rollingwood Drive in southeastern Vallejo. Please see Figure 2-2 of this Initial Study.
APN: 0072-170-050, 0072-170-060, 0072-170-200, 0072-170-210
5. **Project sponsor's name and address:**
KB Home South Bay
6700 Koll Center Parkway
Pleasanton, CA 94566
6. **City of Vallejo General Plan and Zoning designations:**
Retail land use designation
PF- Public and Quasi-Public Facilities and CP – Pedestrian Commercial
7. **Description of project:**
See attached project description, Chapter 2 of this Initial Study.
8. **Surrounding land uses and setting:**
The site is surrounded by single family housing development to the north, rural residential homes and scattered businesses to the west, and cemeteries to the south and east.
9. **Other public agencies whose approval is required:**
 - ◆ Regional Water Quality Control Board
 - ◆ U.S. Fish and Wildlife Service
 - ◆ County of Solano
 - ◆ City of Vallejo
 - ◆ U.S. Army Corps of Engineers

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 INITIAL STUDY SUMMARY

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, as indicated by the checklist on the following pages, involving at least one impact that could have a significant impact, which has been reduced to less-than-significant impacts by incorporation of mitigation measures.

<input type="checkbox"/> Aesthetics	<input type="checkbox"/> Agriculture Resources	<input checked="" type="checkbox"/> Air Quality
<input checked="" type="checkbox"/> Biological Resources	<input checked="" type="checkbox"/> Cultural Resources	<input checked="" type="checkbox"/> Geology / Soils
<input checked="" type="checkbox"/> Hazards / Hazardous Materials	<input type="checkbox"/> Hydrology / Water Quality	<input type="checkbox"/> Land Use
<input type="checkbox"/> Mineral Resources	<input checked="" type="checkbox"/> Noise	<input type="checkbox"/> Population / Housing
<input type="checkbox"/> Public Services	<input type="checkbox"/> Recreation	<input checked="" type="checkbox"/> Transportation / Traffic
<input type="checkbox"/> Utilities / Service Systems	<input type="checkbox"/> Mandatory Findings of Significance	

DETERMINATION: (To be completed by the Lead Agency) On the basis of this initial evaluation:

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.

I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signature

Date

2 PROJECT DESCRIPTION

This Initial Study was prepared for the Rollingwood subdivision project (the Project) which includes 214 housing units and a 4.1-acre park. The Project is divided into two distinct neighborhoods that includes single-family homes to the north and paseo homes, single-family homes with garages at the rear, to the south.

This environmental document provides an assessment of the potential impacts that the development of 214 houses may have on the project site and its surrounding areas.

A. Local Setting

The 31.8-acre site is located at the northwest corner of Benicia Road and Rollingwood Drive in southeastern Vallejo. The site is surrounded by a single-family housing tract to the north, rural residential homes and scattered businesses to the west, and cemeteries to the south and east. The project's regional and local locations are illustrated in Figures 2-1 and 2-2.

Surrounding land uses are as follows:

- ◆ **North:** Single-family housing is located along Dryden Drive
- ◆ **South:** To the south of the site, the Carquinez Cemetery and St. Vincent's All Souls Cemetery are located across Benicia Road.
- ◆ **West:** Single-family housing located along Hargus Avenue, and large vacant lots and Second Baptist Church are located to the west of the project site.
- ◆ **East:** To the East of the site, the Skyview Memorial Lawn Cemetery is located across Rollingwood Drive.

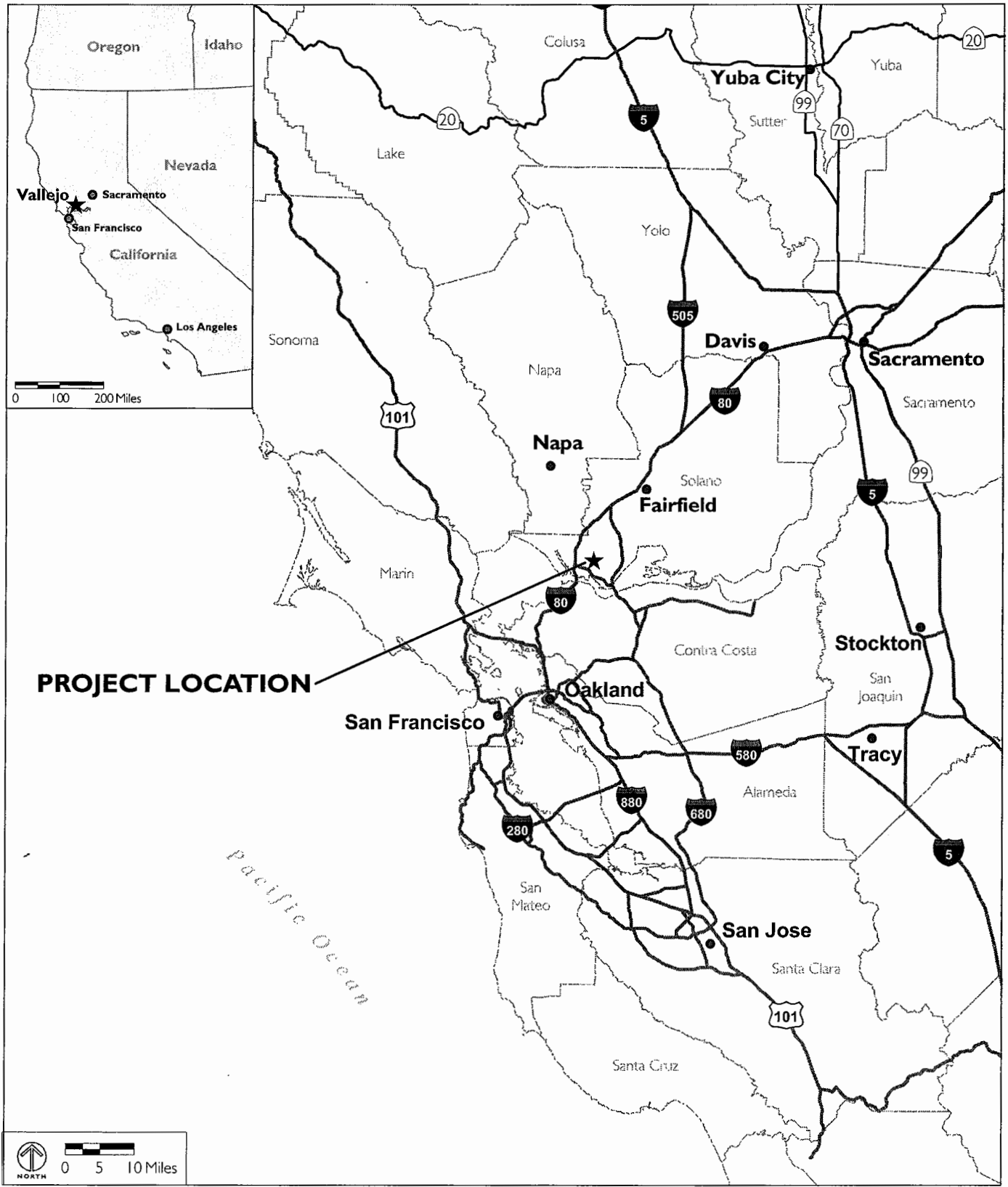


FIGURE 2-1
REGIONAL LOCATION

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Source: Google Earth, 2007

FIGURE 2-2
LOCAL LOCATION

B. Existing Site Character

The proposed site is comprised of 31.8 acres that have been previously graded. Four functional baseball fields and ancillary buildings are located at the north end of the property. One small one-story vacant residential structure is located on the southern end of the property. The existing terrain is varied as elevations range from 140 feet at the northwest corner of the site to a high point of 200 feet along Rollingwood Drive. As a result, site drainage generally flows from east to west.

1. Existing Vegetation and Animal Life

The site is largely undeveloped and supports a cover of disturbed ruderal grassland and mature trees. Four little league fields are located on the northeast corner of the project site. A large portion of the site is paved over in asphalt where a drive-in theatre operated. The asphalt has cracked in many areas and ruderal grasses and weeds are present. Figure 2-3 shows the heavily disturbed nature of the project site.

C. Existing Uses

The following provides a brief overview of existing uses (including vacant parcels) and discusses how each would be affected by the proposed project.

1. East Vallejo Little League Ball Fields

The northwest corner of the project site, contains four operating recreational little league baseball fields. The fields are owned by the Vallejo City Unified School District (VCUSD) and are operated by East Vallejo Little League (EVLL). The four little league fields are located within close proximity to one another. Several electrical sheds and dugouts are also located adjacent to the fields. A standalone structure along Rollingwood Drive serves as a snack-bar and restroom facility. This structure is adjacent to parking spaces.

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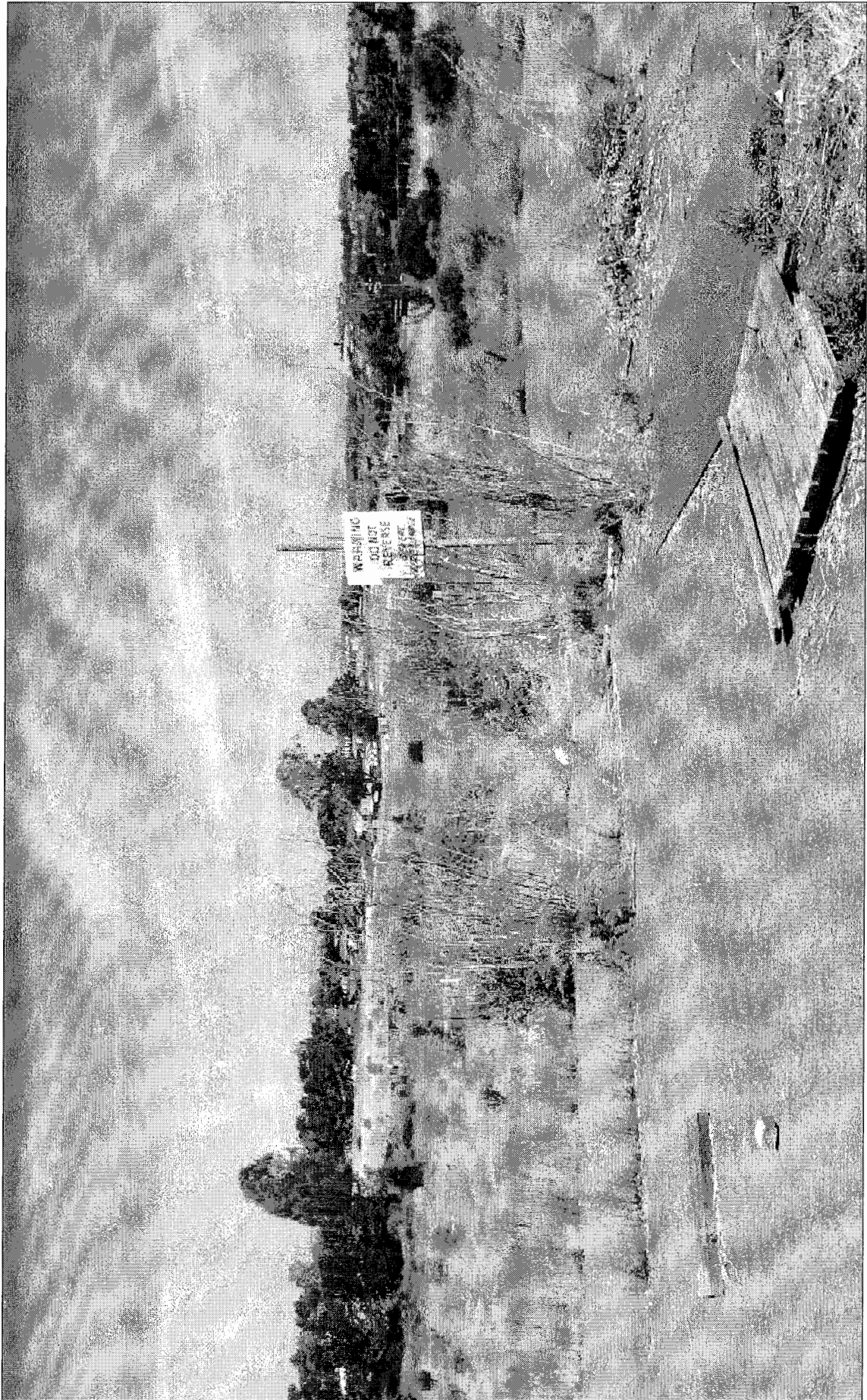


FIGURE 2-3
EXISTING SITE CONDITIONS

EVLL has operated the little league fields without a lease agreement with VCUSD. Little league activities on the project site will not continue past the 2007 season, however, VCUSD has arranged for a 2-year lease with EVLL at a nearby middle school in Vallejo to accommodate the same level of use.

2. City-owned Parcel

The city owns a vacant parcel south of the existing little league fields along Rollingwood Drive. The parcel is covered in ruderal vegetation.

3. Vacant Residential and Commercial Properties

A former drive-in theatre is located near the intersection of Rollingwood Drive and Benicia Road. A vacant residential unit is located near Benicia Road and was once the residence of the drive-in theatre caretaker. It is now abandoned, dilapidated, and surrounded by fencing and dense brush.

All that remains of the drive-in theatre is fencing, electrical boxes and a large area covered in asphalt pavement. Since the ending of commercial use, the asphalt pavement has since cracked and now supports ruderal vegetation over large portions of the former parking area and driveway.

D. Policy Setting

1. City of Vallejo General Plan and Zoning

The Project site is currently zoned as PF (Public and Quasi-Public Facilities) and CP (Pedestrian Commercial). The General Plan land use designation for the site is Retail.

2. Site Ownership

The project site is comprised of 4 separate parcels owned by the Vallejo Unified School District, City of Vallejo, and Syufy Enterprises.

E. Project Characteristics

As summarized in Tables 2-1 and 2-2, the planned residential development features 214 housing units and a 4.1-acre park. The proposed southerly entrance road from Rollingwood Avenue divides the project into two distinct neighborhoods. The north side of the site will feature single-family homes with differing design character from the southern side of the project. The southern side will feature paseo homes which include garages at the rear and access through private alleys.

The 4.1-acre park will be developed as a fully improved park in tandem with the project. Once constructed the park will be managed by the Greater Vallejo Recreation District.

1. Housing

The planned residential development features 214 residential units. 104 single-family residential units will be located on the northern portion of the site. These units will comprise 15.6 acres of the project site and will result in a density of 6.7 dwelling units per acre.

The southern half of the project site will be comprised of 110 paseo homes. Paseo homes are characterized as 2-story homes with garages at the rear of the structure that are accessed by private alleys. Entry to paseo units is provided from Street D, Street G, Rollingwood Avenue or from a landscaped paseo on the opposite side of the unit from the garage. See Figure 2-4 for the project site map. These units will comprise 21.1 acres of the project site and will result in a density of 9.1 dwelling units per acre.

Figures 2-5 and 2-6 are conceptual elevations of the single-family homes and paseo homes within the project, respectively. Several variations of each housing type exist materials and floor plans vary.

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TABLE 2-1 **SUMMARY OF PROJECT COMPONENTS: BUILDINGS**

Project Component	Number of Units	Acres	Density (du/acres)	% Acreage
Single-Family Residential	104	15.6	6.7	49.1
Paseo Residential	110	21.1	9.1	38.0
Total	214	36.7		87.1

Source: Rollingwood Site Plan, KB Home.

TABLE 2-2 **SUMMARY OF PROJECT COMPONENTS: PARKING**

Project Component	Parking Spaces Required	Project Component	Parking Spaces Provided
Residential		Covered Parking	428
Guest Parking	65		65
On-Street Parking	56		164
		Total Parking Provided	657

Source: Rollingwood Site Plan, KB Home.

2. Park

The Project will utilize the City of Vallejo's ownership of 4.3 acres of the project site for a public park. KB Home proposes to relocate the park site within the project to create a centrally located park with a rectangular shape that would allow for maximum efficient of use. Two community meetings were held on May 14, 2007 and July 30, 2007 to receive comments and discuss park plans. Upon completion of construction, the applicant will provide the fully improved park to the Greater Vallejo Recreation District.

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Source: Ruggieri, Jensen, Azar and Associates

FIGURE 2-4
 PROJECT SITE PLAN

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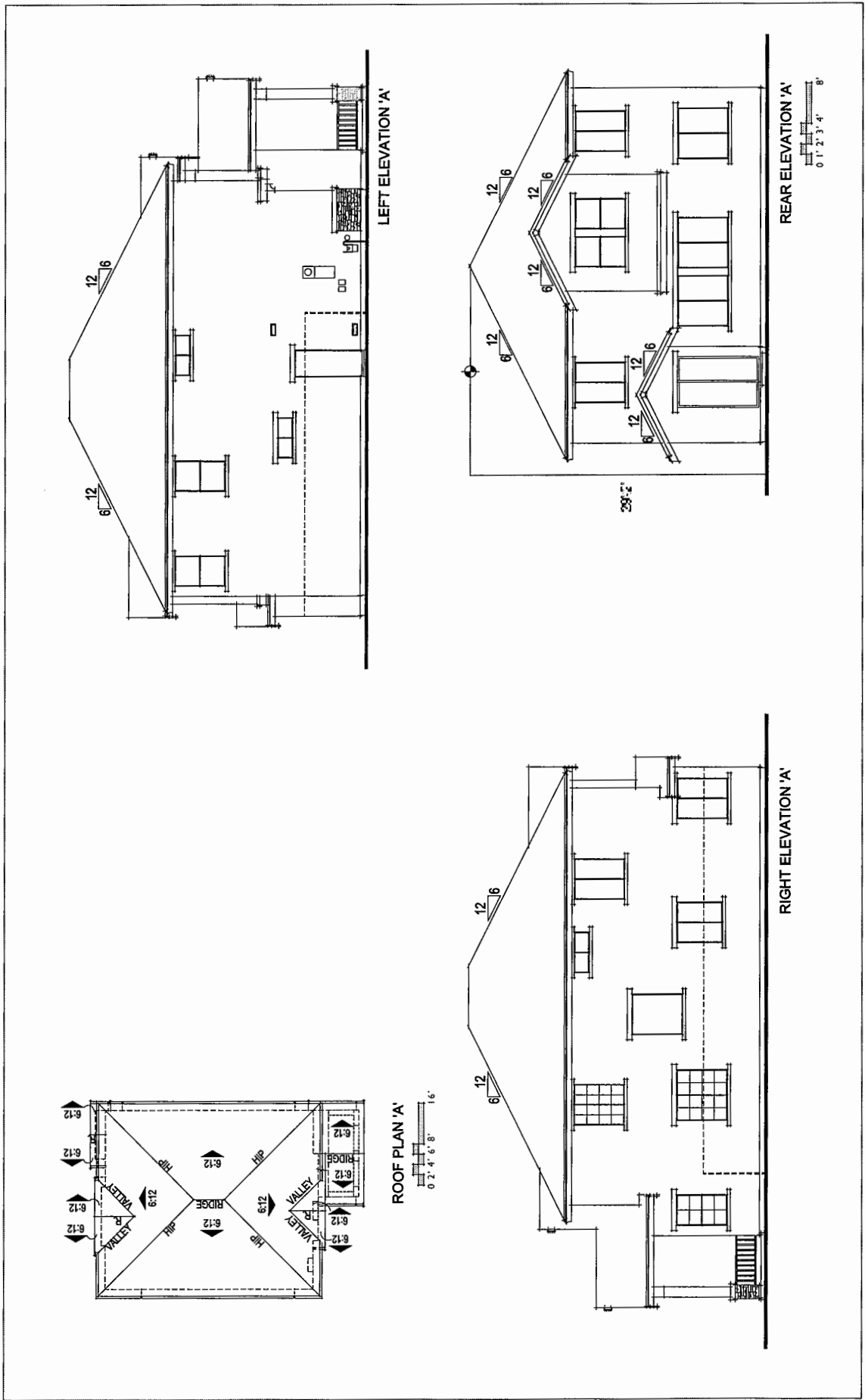
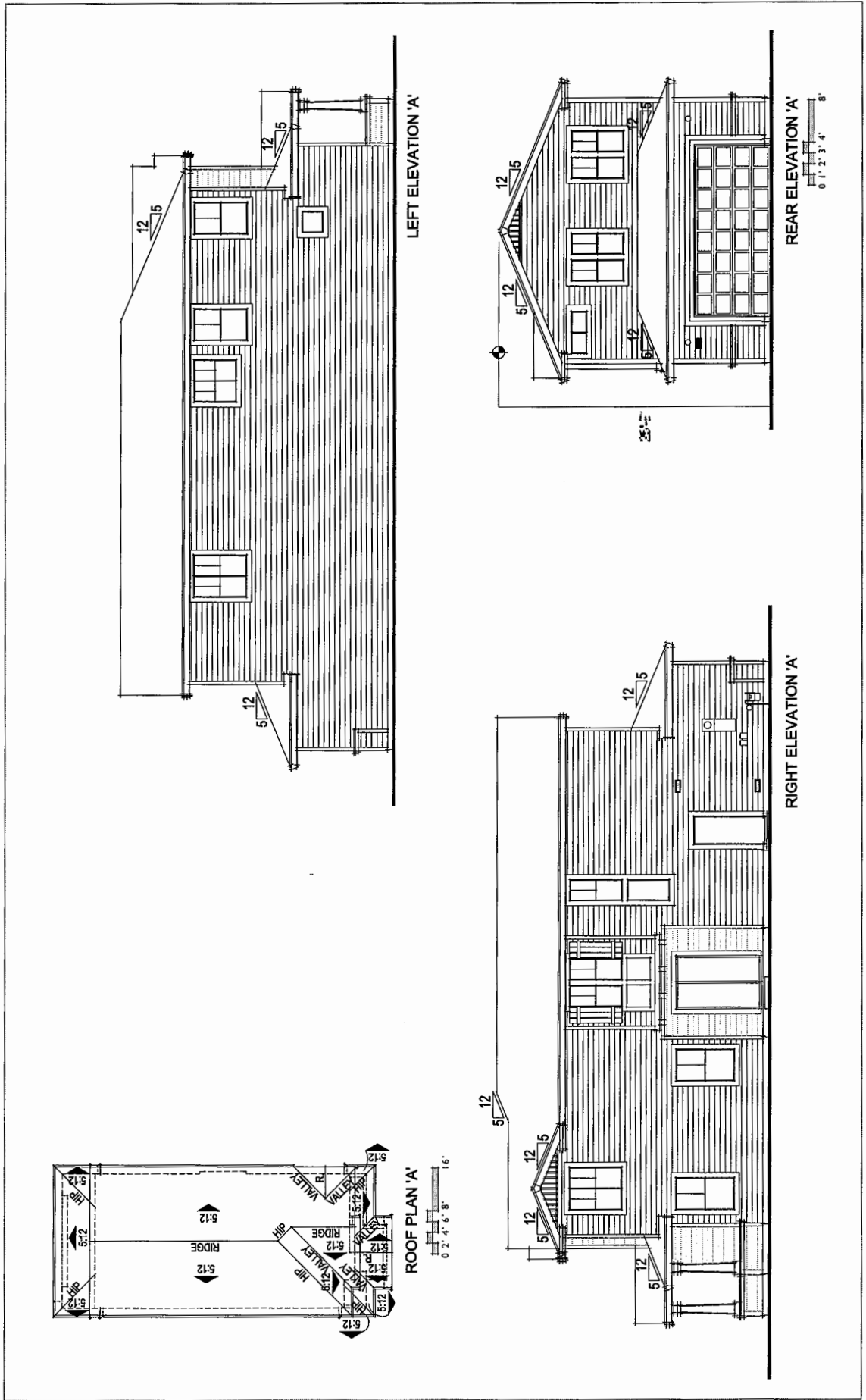


FIGURE 2-5
 CONCEPTUAL ELEVATION - SINGLE FAMILY HOME

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 MITIGATED NEGATIVE DECLARATION
 PROJECT DESCRIPTION



Note: As shown above, 5:12 and 12:5 indicate that the pitch of the roof will rise 5-inches for every 12-inches of horizontal distance.

FIGURE 2-6
 CONCEPTUAL ELEVATION - PASEO HOME

3. Policies

KB Home proposes a General Plan Amendment to change the land Use designation of the site from Retail to Low Density Residential. The site will also be rezoned from PF and CP to PDR (Planned Development Residential). The PDR zoning will allow for flexibility in the site layout and design of the development. The proposed residential use would be compatible with the surrounding low density residential land uses and provide for new housing in the southwestern portion of the City.

4. Other Improvements

a. Parking

Sixty-five guest parking spaces are provided in open parking bays and 166 on-street spaces on streets located within the project site.

b. Irrigation

Homeowner maintained irrigation systems will be used to maintain single-family lots consisting of front yard landscaping which includes lawns, shrubs, groundcovers, and street trees. Landscaping surrounding paseo homes, which will include lawns, trees, and groundcovers, will be irrigated with a common irrigation system maintained by the future homeowners association.

c. Storm Drainage

The majority storm runoff from the project site will be collected in standard inlets and piped to the park to travel through landscaping for treatment. Stormwater will then drain via gravity flow from the park to Benicia Road.

Approximately 6 acres of the project site currently drain to a swale that straddles the northern extent of the site. The swale drains to an existing storm drain system on Dryden Drive. The project proposes to reduce the tributary area to this swale to approximately 2.1 acres.

d. Sanitary Sewer

Wastewater will be collect and conveyed in 8-inch main lines from the subdivision and taken into the existing system through an existing easement on the Second Baptist Church property via Hargus Avenue. The existing system between Hargus Avenue and the connection point on the Church property will be removed and replaced at a lower elevation to allow gravity flow. An existing sewer line is located between the subdivision boundary and the most downstream manhole on Hargus Avenue. This line will be protected in place while a new, lower main line is installed to serve the needs of the project. The most downstream manhole on Hargus Avenue and the existing services will remain connected to the higher sewer line.

e. Little League Fields Replacement

As previously noted, East Vallejo Little League (EVLL) operates the recreational baseball fields within the project site without a lease agreement with Vallejo City Unified School District (VCUSD). Little league fields will be constructed at Springstowne Middle School and possibly Pennycook Elementary School to accommodate EVLL. The new fields will be operated by EVLL under a two-year lease.

f. Site Access and Circulation

As shown in Figure 2-4, the Project site will be accessed through three streets; two along Rollingwood Drive and one along Benicia Road. An emergency vehicle access road would be provided via Hargus Avenue.

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PROJECT DESCRIPTION

3 ENVIRONMENTAL CHECKLIST

This chapter contains the Environmental Checklist used by the City of Vallejo to evaluate impacts of the proposed project. An explanation of each checklist item is presented in Chapter 4.

Environmental Topic	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
1. Aesthetics				
Would the project:				
a. Substantially degrade the existing visual character or quality of the site and its surroundings?			X	
b. Have a substantial adverse effect on a scenic vista?				X
c. Substantially degrade the view from a scenic highway, including, but not limited to, trees, rock outcroppings and historic buildings?				X
d. Expose people on- or off-site to substantial light or glare?			X	
2. Agriculture Resources				
In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:				
a. Convert Prime Farmland, Unique Farmland or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?				X
b. Conflict with an existing Williamson Act contract?				X
c. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use?				X

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3. Air Quality				
Where available, the significance of criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?		X		
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X	
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under applicable federal or State ambient air quality standards (including releasing emissions that exceed quantitative thresholds for ozone precursors or other pollutants)?			X	
d. Expose sensitive receptors to substantial pollutant concentrations?		X		
e. Create objectionable odors affecting a substantial number of people?			X	
4. Biological Resources				
Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on a plant or animal population, or essential habitat, defined as a candidate, sensitive or special-status species?		X		
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community type?				X
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act, through direct removal, filling, hydrological interruption or other means?		X		

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d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species, their wildlife corridors or nursery sites?			X	
e. Conflict with any local ordinances or policies protecting biological resources?				X
f. Conflict with an adopted Habitat Conservation Plan, Natural Community Conservation Plan or other approved local, regional or State habitat conservation plan?				X
5. Cultural Resources				
Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource?		X		
b. Cause a substantial adverse change in the significance of an archaeological resource?		X		
c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?		X		
d. Disturb any human remains, including those interred outside of formal cemeteries?		X		
6. Geology and Soils.				
Would the project:				
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:				
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?				X

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ii. Strong seismic ground shaking?			X	
iii. Seismic-related ground failure, including liquefaction?			X	
iv. Landslides, mudslides or other similar hazards?				X
b. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?		X		
c. Result in substantial soil erosion or the loss of topsoil?			X	
d. Be located on expansive soil, creating substantial risks to life or property?		X		
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?				X
7. Hazards and Hazardous Materials				
Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?			X	
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X	
c. Emit hazardous emissions or handle hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?				X

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d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, create a significant hazard to the public or the environment?		X		
e. Expose people or structures to a significant risk of loss, injury or death involving wildland fires?			X	
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				X
g. For a project within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people living or working in the project area?				X
h. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people living or working in the project area?				X
8. Hydrology and Water Quality				
Would the project:				
a. Require or result in the construction of new stormwater drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
b. Violate any water quality standards or waste discharge requirements?			X	
c. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a significant lowering of the local groundwater table level?			X	

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Environmental Topic	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
d. Substantially alter the existing drainage pattern of the site or area in a manner which would result in substantial erosion, siltation or flooding on- or off-site?			X	
e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems?			X	
f. Provide substantial additional sources of polluted runoff, or otherwise substantially degrade water quality?				X
g. Place occupied development within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?				X
h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?				X
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?				X
j. Potentially be inundated by seiche, tsunami or mudflow?				X
9. Land Use				
Would the project:				
a. Physically divide an established community?				X
b. Create or exacerbate a conflict between land uses on the project site and in the surrounding area?				X

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c. Conflict with any applicable land use plan, policy or regulation of an agency with jurisdiction over the project (including, but not limited to, the general plan, specific plan, local coastal program or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?			X	
10. Mineral Resources				
Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region or the State, or of a locally-important mineral resource recovery site delineated on a mineral resource plan, local general plan, specific plan or other land use plan?				X
11. Noise				
Would the project:				
a. Expose people to or generate noise levels in excess of standards established in the local general plan or noise ordinance, or other applicable standards?		X		
b. Expose people to or generate excessive groundborne vibration or groundborne noise levels?			X	
c. Create a substantial temporary, periodic or permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
d. Expose people living or working in the project area to excessive noise from a public or private airport?			X	

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12. Population and Housing				
Would the project:				
a. Induce substantial unexpected population growth or growth for which inadequate planning has occurred, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?			X	
b. Displace substantial numbers of existing housing units, necessitating the construction of replacement housing elsewhere?				X
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?				X
13. Public Services				
Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, or result in the need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
a. Fire protection?			X	
b. Police protection?			X	
c. Schools?			X	
d. Parks?				X
e. Libraries?			X	
f. Other public facilities?				X

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14. Recreation				
Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?				X
b. Result in substantial adverse physical impacts associated with the provision of new or physically altered park and recreational facilities, or result in the need for new or physically altered park and recreational facilities, the construction of which could cause significant environmental impacts.			X	
15. Transportation/Traffic				
Would the project:				
a. Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system?			X	
b. Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?			X	
c. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?		X		
d. Result in inadequate emergency access?				X
e. Result in inadequate parking capacity?				X
f. Conflict with adopted policies, plans or programs supporting alternative transportation?				X

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g. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				X
16. Utilities and Services				
Would the project:				
a. Have insufficient water supplies available to serve the project from existing and identified entitlements and resources?			X	
b. Require or result in the construction of new water facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
c. Require or result in the construction of new wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
d. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X	
e. Have insufficient wastewater treatment capacity available to serve the project's projected demand in addition to existing demand?			X	
f. Require or result in the construction of new stormwater drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
g. Not be served by a landfill with sufficient permitted capacity to accommodate the buildout of the project's solid waste disposal needs?				X
h. Not comply with federal, State and local statutes and regulations related to solid waste and recycling?			X	

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Environmental Topic	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
17. Mandatory Findings of Significance				
Does the project:				
a. Have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			X	
b. Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects and the effects of probable future projects.)			X	
c. Have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?			X	

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4 EXPLANATION OF CHECKLIST FINDINGS

The following provides an explanation of the analysis completed to make the determination of significance contained in the previous checklist in Chapter 3.

1. AESTHETICS

- a. *Less than Significant.* The project site currently contains two structures; an abandoned residential unit is located near the intersection of Benicia Road and Rollingwood Drive, and a restroom/snack bar is located along Rollingwood Drive at the north end of the site. Other auxiliary sheds used for maintenance of the little league fields are also located on-site. Residential uses are found adjacent to the project site on the north and west sides of the site. The neighborhoods that define these areas generally consist of low-density single-family detached housing units that are one to two stories in height. The architectural styles of the homes in these areas vary to some degree, but generally consist of the California bungalow and ranch style home. These neighborhoods have views of the ruderal grassland and little league fields on site. Cemeteries are located across Rollingwood Drive to the east and across Benicia Road to the south of the site.

The introduction of 214 residential units, constructed as 2-story structures with articulated facades, would change the visual appearance of the project site from primarily open space to a suburban development pattern. However, the proposed project would include landscaping amenities, setbacks and architectural features designed to preserve and enhance the appearance of the structures, and to be compatible with the visual character and quality of its surroundings, such that long term aesthetic degradation will not occur. Therefore, implementation of the project would have a *less-than-significant* impact on the existing visual character and quality of the site and its surroundings.

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- b. *No Impact.* Nearby hills, which are visible from higher portions of the project site and surrounding areas, are located to the north and northeast of the project site? Existing vegetation and trees bordering the site, as well as topography of the site and surrounding area prevent expansive views and limit scenic vistas from adjacent public viewpoints. Therefore, the project would have *no impact* on scenic vistas.
- c. *No Impact.* The proposed project is not located near a State scenic highway; therefore, implementation of the Project would result in *no impact* to scenic highways.¹
- d. *Less than Significant.* There is potential for an increase in light and glare as a result of the project. Exterior lighting would be limited to new overhead street lighting and exterior lighting for individual residences. The field of light generated by overhead, exterior lights would be directed downward onto roadways and sidewalks within the development through the use of hooded lights. Spillover of excess light onto neighboring properties would be minimized through the use of hooded light poles and the appropriate positioning of the poles themselves in relation to neighboring parcels. The project could also cause an increase in the level of glare from the site due to the introduction of reflective surfaces on new homes and supporting exterior infrastructure (e.g. signage). However, the new landscaping that would be included as part of the site plan, especially the new trees, would reduce the amount of refracted glare that would escape from the site. As a result, a *less-than-significant* impact would occur in relation to excess light and glare.

¹ California Department of Transportation, website, <http://www.dot.ca.gov/hq/LandArch/scenic/schwy1.html>, accessed on July 24, 2007.

2. AGRICULTURAL RESOURCES

- a-c. *No Impact.* The site is not part of the Williamson Act contract, as the site's parcels are zoned as PF (Public and Quasi-Public Facilities) and CP (Pedestrian Commercial) in the City's zoning ordinance. Furthermore, based on criteria for Williamson Act farmland, namely the necessity of 100 acres of farmland, the project site would not qualify as Williamson Act farmland.² Based on the absence of Williamson Act contracts and any agricultural activity on the site, the project would have *no impact* on Prime Farmland, Unique Farmland, or farmland of Statewide Importance.

3. AIR QUALITY

- a. *Potentially Significant Unless Mitigation Incorporated.* The most current Clean Air Plan (CAP), the 2005 Bay Area Ozone Strategy, was adopted by BAAQMD in 2006. This plan is based on population projections through 2020 compiled by the Association of Bay Area Governments (ABAG). The project proposes new single-family residences on a site that is mostly unused, but designated for retail uses. The neighborhood surrounding the project site is predominately residential. The Project would increase the number of residential units in Vallejo by 214 and increase the population by approximately 600 residents by 2010. According to ABAG's projections, the City's population is expected to increase from 122,900 in 2005 to 129,800 in 2010, amounting in an increase of 6,900 people. During the same period, the number of households in the City is projected to increase by 2,040.³ Therefore, the projected increase in population and dwelling units associated with the construction of the Project through 2010 would not exceed ABAG's growth projections for the City of Vallejo. Therefore, the project would not affect

² State of California, Division of Land Resource Protection, "Williamson Act Basics."

³ Association of Bay Area Governments, *Projections 2007*, page 200.

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population or Vehicle Miles Traveled (VMT) forecasts that were used for in the development of the regional Clean Air Plan.

Determining consistency with the Clean Air Plan also involves assessing whether Transportation Control Measures (TCMs) contained in the 2005 Bay Area Ozone Strategy are implemented. The 2005 Ozone Strategy (i.e., BAAQMD's most recent Clean Air Plan) includes 20 transportation control measures, of which seven require participation at the local level. The latest set of adopted TCMs, which identify local governments as implementing agencies, is listed by the BAAQMD in their CEQA Guidelines. These TCMs that would apply to projects are designed to reduce motor vehicle travel by encouraging use of other transportation modes. For single-family residential projects, these would include amenities that would encourage transit, bicycle and pedestrian modes of transportation. Without appropriate implementation of project-level transportation measures, this would be a significant impact.

Mitigation Measure AQ-1: To ensure incorporation of appropriate Clean Air Plan TCMs, the features listed below shall be included in the project. Implementation of the measures listed below along with the City's General Plan requirements would reduce vehicle use and vehicle miles traveled consistent with the current Clean Air Plan TCMs:

- ◆ Bicycle amenities should be provided. This would include appropriate bike lane connections and bicycle parking at the planned park.
- ◆ Provide appropriate transit amenities in coordination with the City and Vallejo Transit (Route 6), such as sheltered bus stops and bus pullouts.
- ◆ Implement a landscape plan that provides shade trees along adjacent sidewalks and at bus stops.

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Significance after Mitigation: Less than significant.

- b. *Less than Significant.* Carbon monoxide emissions from traffic generated by the project would be the pollutant of greatest concern at the local level. Congested intersections with a large volume of traffic have the greatest potential to cause high-localized concentrations of carbon monoxide. Air pollutant monitoring data indicate that carbon monoxide levels have been at healthy levels (i.e., below State and Federal standards) in the Bay Area since the early 1990s. As a result, the region has been designated as attainment for the standard. There is an ambient air quality monitoring station in Vallejo that measures carbon monoxide concentrations. The highest measured level over any 8-hour averaging period during the last 3 years is 3.4 parts per million (ppm). The contribution of project-generated traffic to these levels was predicted using the BAAQMD-recommended screening methods based on the Caline4 Line-Source Dispersion Model. Carbon monoxide concentrations were predicted for existing, existing plus project and cumulative conditions. The intersection of Rollingwood Drive and Benicia Road is considered the worst intersection (in terms of elevated carbon monoxide levels from traffic) that would be affected by project-generated traffic. This is due to a combination of higher traffic volumes and congestion. Future carbon monoxide levels were predicted near this intersection with the project in place using traffic projections provided by DMJM Harris/AECOM. Emission factors used were calculated using the EMFAC2007 model, developed by the California Air Resources Board, with default assumptions for the Bay Area during winter that include a temperature of 45 deg. F. The highest 8-hour concentration with the project in place (in 2010) is predicted to be 4.4 ppm over an 8-hour averaging period. The results of modeling indicate that levels would be below the California ambient air quality standards (used to judge the significance of the impact) or 9.0 ppm; therefore the impact is *considered less-than-significant*.

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- c. *Less than Significant.* The Bay Area is considered a non-attainment area for ground-level ozone under both the Federal Clean Air Act and the California Clean Air Act. The area is also considered non-attainment for respirable particulates or particulate matter with a diameter of less than 10 micrometers (PM₁₀), and particulate matter with a diameter of less than 2.5 micrometers (PM_{2.5}) under the California Clean Air Act, but not the federal act. As explained in the response to criteria b), the area has attained both State and federal ambient air quality standards for carbon monoxide. As part of an effort to attain and maintain ambient air quality standards for ozone and PM₁₀, the BAAQMD has established thresholds of significance for air pollutants. These thresholds are for ozone precursor pollutants (reactive organic gases and nitrogen oxides) and PM₁₀.

The project would add new traffic trips, which would lead to increased emissions of air pollutants. Emissions of project-related air pollutants were predicted using the URBEMIS2007 model (Version 9.2), distributed by Rimpo Associates and recommended for use by the BAAQMD. This model predicts daily emissions associated with land use developments. The model combines predicted daily traffic activity, associated with the different land use types, with emission factors from the State's mobile emission factor model (i.e., EM-FAC2007). The model also predicts area source emissions associated with the proposed project, which are minor compared to emissions associated with traffic. Area sources for residential projects are associated with water and space heating and landscape maintenance. Vehicle mix, travel characteristics and temperature inputs were based on URBEMIS2007 defaults for Solano County. Since the City of Vallejo does not have a residential wood smoke ordinance, emissions of PM₁₀ from residential wood burning were predicted by the model for winter conditions. Daily summer and winter emissions predicted with full build out of the project are shown in Table 4-1.

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Emissions associated with the project would not exceed BAAQMD thresholds; as a result, the project would have a *less-than-significant* cumulative impact on regional air quality.

- d. *Potentially Significant Unless Mitigation Incorporated.* Operation of the project is not expected to cause any localized emissions that could expose sensitive receptors to unhealthy air pollutant levels. Construction activities would result in localized emissions of dust and diesel exhaust that could result in temporary impacts to adjacent land uses.

During grading and construction activities, dust would be generated. Most of the dust would result during site grading activities. The amount of dust generated would be highly variable and is dependent on the size of the area disturbed, amount of activity, soil conditions, and meteorological conditions. Typical winds during late spring through summer are from the southwest. Residential uses are located near the site, especially to the north and northeast. These areas could be adversely affected by dust generated during construction activities.

Although grading and construction activities would be temporary, they would have the potential to cause both nuisance and health air quality impacts. PM_{10} is the pollutant of greatest concern associated with dust. If uncontrolled, PM_{10} levels downwind of actively disturbed areas could possibly exceed State standards. In addition, dust fall on adjacent properties could be a nuisance. If uncontrolled, dust generated by demolition, grading and construction activities represents a potentially significant impact.

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TABLE 4-1 **EMISSIONS AT PROJECT BUILDOUT**

Scenario	Emissions of ROG ^a	Emissions of NO _x ^b	Emissions of PM ₁₀ ^c
Daily Summer Emissions	35 pounds/day	28 pounds/day	31 pounds/day
Daily Winter Emissions (includes wood burning)	-- ^d	-- ^d	49 pounds/day
<i>BAAQMD Thresholds</i>	<i>80 pounds/day</i>	<i>80 pounds/day</i>	<i>80 pounds/day</i>

^a ROG = Reactive Organic Gases

^b NO_x = Nitrogen Oxide

^c PM₁₀ = Particulate Matter 10 (particulate matter with a diameter of less than 10 micrometers)

^d Not applicable. Ozone season runs from late spring through early fall.

Construction equipment and associated heavy-duty truck traffic generates diesel exhaust, which is a known Toxic Air Contaminant. Diesel exhaust poses both a health and nuisance impact to nearby receptors. Diesel exhaust is also a substantial source of NO_x emissions that affect regional ozone levels. These construction activities would be near sensitive receptors, but only for relatively short time periods. Therefore, the impacts are considered to be less than significant if reasonable available control measures are applied.

Mitigation Measure AQ-2: The applicant shall include the following measures to control dust and exhaust emissions:

- ◆ Water all active construction areas at least twice daily and more often during windy periods. Active areas adjacent to residences should be kept damp at all times.
- ◆ Cover all hauling trucks or maintain at least two feet of free-board. Dust-proof chutes shall be used as appropriate to load debris onto trucks during demolition.

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- ◆ Pave, apply water at least twice daily, or apply (non-toxic) soil stabilizers on all unpaved access roads, parking areas, and staging areas.
- ◆ Sweep daily (with water sweepers) all paved access roads, parking areas, and staging areas and sweep streets daily (with water sweepers) if visible soil material is deposited onto the adjacent roads.
- ◆ Hydroseed or apply (non-toxic) soil stabilizers to inactive construction areas (i.e., previously-graded areas that are inactive for 10 days or more).
- ◆ Enclose, cover, water twice daily, or apply (non-toxic) soil binders to exposed stockpiles.
- ◆ Limit traffic speeds on any unpaved roads to 15 mph.
- ◆ Replant vegetation in disturbed areas as quickly as possible.
- ◆ Suspend construction activities that cause visible dust plumes to extend beyond the construction site.
- ◆ During renovation and demolition activities, removal or disturbance of any materials containing asbestos, lead paint or other hazardous pollutants will be conducted in accordance with BAAQMD rules and regulations.
- ◆ Prohibit use of “dirty” equipment. Equipment with noticeably dirty emissions shall be prohibited from operation at the site until proper maintenance has been performed to reduce the visible emissions to acceptable levels. If necessary, opacity shall be used as an indicator of exhaust particulate emissions from off-road diesel powered equipment, where the project shall ensure that emissions from all construction diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40

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percent opacity (or Ringelmann 2.0) shall be repaired immediately.

- ◆ Signs shall be clearly posted at the construction site indicating that diesel equipment standing idle for more than five minutes shall be turned off. This would include trucks waiting to deliver or receive soil, aggregate, or other bulk materials. Rotating drum concrete trucks could keep their engines running continuously as long as they are onsite.
- ◆ Properly tune and maintain equipment in accordance with manufacturer specifications.
- ◆ During grading, stage construction equipment at least 200 feet away from any sensitive uses.
- ◆ The contractor shall install temporary electrical service whenever possible to avoid the need for independently powered equipment (e.g., compressors).
- ◆ A Disturbance Coordinator will be assigned to the project when active construction is occurring within 200 feet of residences. This coordinator will ensure that all air quality construction period mitigation measures are enforced. In addition, the Disturbance Coordinator will respond to complaints from the public regarding air quality issues in a timely manner. The contact information for this Coordinator will be posted in plain view at the project site.
- ◆ A Disturbance Coordinator shall suspend construction activities that cause visible dust plumes to extend beyond the construction site.

Significance after Mitigation: Less than significant.

- e. *Less than Significant.* During construction the various diesel powered vehicles and equipment in use on the site would create localized odors. These odors would be temporary and not likely to be notice-

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able for extended periods of time much beyond the project's site boundaries. The potential for diesel odor impacts is therefore less-than-significant. The proposed residential uses that would be constructed are not likely to produce any offensive odors in the long-term; therefore this would be a *less-than-significant* impact.

4. BIOLOGICAL RESOURCES

Biological resources were identified through the review and compilation of existing information and a field reconnaissance survey. The review provided information on general resources in the area, potential jurisdictional wetlands and other waters, and the distribution and habitat requirements of special-status species which have been recorded from or are suspected to occur in the Vallejo vicinity. This included a preliminary wetland delineation (Olberding Environmental, 2006) a *Preliminary Tree Report* (Hortscience, 2006), and a *Biological Resources Analysis* (Olberding Environmental, 2006), all of which were prepared for the applicant. The preliminary wetland delineation provides a detailed assessment and mapping of the extent of potential jurisdictional wetlands based on the opinion of the applicant's biological consultant. The U.S. Army Corps of Engineers (Corps) conducted a field visit with the applicant's biological consultant as part of the verification in March 2007, but the verification process has not been completed at this point. The *Biological Resources Analysis* (BRA) provides information on vegetation and wildlife habitat, potential for occurrence of special-status species, summary of potential jurisdictional wetlands and other waters, conclusions regarding presence or absence of sensitive resources, and recommendations for additional surveys and mitigation. The *Preliminary Tree Report* provides a summary of the location and condition of each tree on the site at the time of the survey. A peer review of the applicant's studies was conducted by Environmental Collaborative, which involved a field reconnaissance survey of the site on August 7, 2007, followed by preparation of the following discussion. (Note: The background information provided in the BRA, it is available for review at Vallejo Planning Division.)

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- a. *Potentially Significant Unless Mitigation Incorporated.* The potential for occurrence of any special-status species on the site is considered remote, given the existing site conditions and previous uses. However, there remains the potential for nesting birds, including burrowing owl, loggerhead shrike, and other raptors and passerine birds to establish nests on the site in advance of construction. Grading and vegetation removal could result in the inadvertent destruction of any nests in active use, which would be a possible violation of the Migratory Bird Treaty Act. Preconstruction surveys would be necessary to confirm absence of any nesting bird species, particularly burrowing owl and other raptors. Mitigation Measure BIO-2 below would serve to ensure that adequate avoidance of possible bird nesting activity occurs as part of the project, and would serve to mitigate this potentially significant impact to a less-than-significant level.

In addition to the potential for nesting raptors on the site, there remains a remote possibility that other important plant and animal species may be present on the site. These species include six special-status plant species, vernal pool fairy shrimp, Monarch butterfly, and the pallid bat. The BRA recommends directed surveys to confirm absence of these species, and identifies mitigation if they are encountered. If one or more of these species is in fact present on the site, grading and construction could destroy the existing habitat and could result in inadvertent take of individuals, which would be a potentially significant impact of the project. Completion of the recommended additional surveys, implementation of any required mitigation, and compliance with applicable State and federal laws would serve to mitigate this potentially significant impact to a *less-than-significant* level.

Mitigation Measure BIO-1: The applicant shall obtain all necessary permits from the Army Corps of Engineers (the Corps), U.S. Fish and Wildlife Service (USFWS), California Department of Fish and Game (CDFG), and the Regional Water Quality Control Board (RWQCB) as required by federal and State law to

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avoid, minimize or offset impacts to any species listed under either the State or federal Endangered Species Acts or protected under any other State or federal law as follows:

- ◆ Before project implementation, a delineation of waters of the United States, including wetlands that could be affected by development, shall be made by a qualified wetland specialist through the formal Clean Water Act (CWA) Section 404 process.
- ◆ If based on the verified delineation, it is determined that fill of waters of the United States would result from project implementation, authorization for such fill shall be secured from the Corps through the Section 404 permitting process and from the RWQCB as part of the Section 401 water quality certification process.
- ◆ Consultation or incidental take permitting may be required under the Endangered Species Act (ESA). The applicant shall obtain all legally-required permits from the USFWS for the “take” of protected species under the ESA.
- ◆ Evidence that the applicant has secured any required authorization from these agencies shall be submitted to the Vallejo Planning Division prior to issuance of any grading or building permits for the project.

Mitigation Measure BIO-2: Any active raptor or other bird nests in the vicinity of proposed grading shall be avoided until young birds are able to leave the nest (i.e., fledged) and forage on their own. Avoidance may be accomplished either by scheduling grading and vegetation removal during the non-nesting period (September through February), or if this is not feasible, by conducting a pre-construction survey for active nests. Provisions of the pre-construction survey and nest avoidance, if necessary, shall include the following:

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- ◆ If grading is scheduled during the active nesting period (March through August), a qualified wildlife biologist shall be retained by the applicant to conduct a pre-construction nesting survey no more than 30 days prior to initiation of grading to provide confirmation on presence or absence of active nests in the vicinity.
- ◆ If active nests are encountered, species-specific measures shall be prepared by a qualified biologist in consultation with the CDFG and implemented to prevent nest abandonment. At a minimum, grading in the vicinity of the nest shall be deferred until the young birds have fledged. A nest-setback zone of at least 300 feet shall be established for raptors and 100 feet for loggerhead shrike and passerine birds within which all construction-related disturbances shall be prohibited. The perimeter of the nest-setback zone shall be fenced or adequately demarcated, and construction personnel restricted from the area.
- ◆ If permanent avoidance of the nest is not feasible, impacts shall be minimized by prohibiting disturbance within the nest-setback zone until a qualified biologist verifies that the birds have either a) not begun egg-laying and incubation, or b) that the juveniles from the nest are foraging independently and capable of independent survival at an earlier date. A survey report by the qualified biologist verifying that the young have fledged shall be submitted to the Vallejo Planning Division prior to initiation of grading in the nest-setback zone.

Mitigation Measure BIO-3: Focused surveys for the special-status plant species considered to have a remote potential for occurrence on the site shall be conducted during the flowering season by a qualified botanist to confirm absence of these species from the site. Species of concern identified in the BRA include: bent-flowered fiddleneck (*Amsinckia lunaris*), Congdon's tarplant (*Centromadia parryi* ssp. *congdonii*), dwarf downingia

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(*Downingia pusilla*), Santa Cruz tarplant (*Holocarpha macradenia*), Contra Costa goldfields (*Lasthenia conjugens*), and legenere (*Legenere limosa*). If populations of special-status plant species are found, then a detailed mitigation plan shall be prepared which includes specifics on the species, habitat and natural community preservation/conservation strategies designed to protect this species, and compensatory mitigation developed in accordance with any CDFG and USFWS requirements. If no special-status plants are found during the focused surveys, then no further mitigation would be required.

Mitigation Measure BIO-4: Directed surveys shall be conducted for Monarch butterfly, vernal pool fairy shrimp, burrowing owl, and pallid bat as recommended in the BRA by the applicant's biological consultant. Appropriate mitigation shall be provided in the remote instance that any of these species are encountered on the site. Mitigation shall be coordinated with representatives of the CDFG and/or USFWS as required by law, and may include on-site avoidance, compensatory mitigation, or other acceptable mitigation strategies. If none of these special-status animal species are found during the focused surveys, then no further mitigation would be required.

Significance after Mitigation: Less than significant.

- b. *No Impact.* Due to the extent of past disturbance, there are no sensitive natural community types on the site, including riparian habitat. The potential jurisdictional wetlands and non-jurisdictional waters are generally the result of man-made conditions, such as irrigation runoff from the baseball fields, residential yard areas, and the parking lot of the former drive-in theater. These features are dominated by non-native species and are they are not considered sensitive natural community types. However, some or all of these potential wetlands and waters may be regulated by the Corps, regardless of whether

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they are man-made or support native vegetation. *No impact* on sensitive natural communities is anticipated and no mitigation is therefore required.

- c. *Potentially Significant Unless Mitigation Incorporated.* As currently proposed, the project would eliminate all of the potential jurisdictional and non-jurisdictional waters mapped in the preliminary wetland delineation prepared by the applicant's biological consultant. Collectively, this consists of up to 0.621 acres of potential waters, the extent of which has not yet been verified by the Corps. Of this total, the applicant's biological consultant estimates that 0.386 acre is potentially jurisdictional wetlands, consisting of the drainage swales along the northern and west-central portions of the site (see Figure 4-1). Although these potential jurisdictional and non-jurisdictional features have only limited habitat value and are dominated by non-native species, collectively this represents a significant impact. Implementation of an adequate mitigation plan and securing any necessary authorizations from jurisdictional agencies, as called for in Mitigation Measure BIO-5, would ensure that potential impacts on wetlands and waters are mitigated to a less-than-significant level.

The wetland replacement component of the Wetland Mitigation Program shall specify performance criteria, maintenance and long-term management responsibilities, monitoring requirements, and contingency measures. Monitoring shall be conducted by the qualified wetland specialist for a minimum of five years and continue until the success criteria are met.

Mitigation Measure BIO-5: A Wetland Mitigation Program shall be prepared by a qualified wetland specialist to provide for the protection, replacement, and management of jurisdictional waters on the site affected by proposed development. The Wetland

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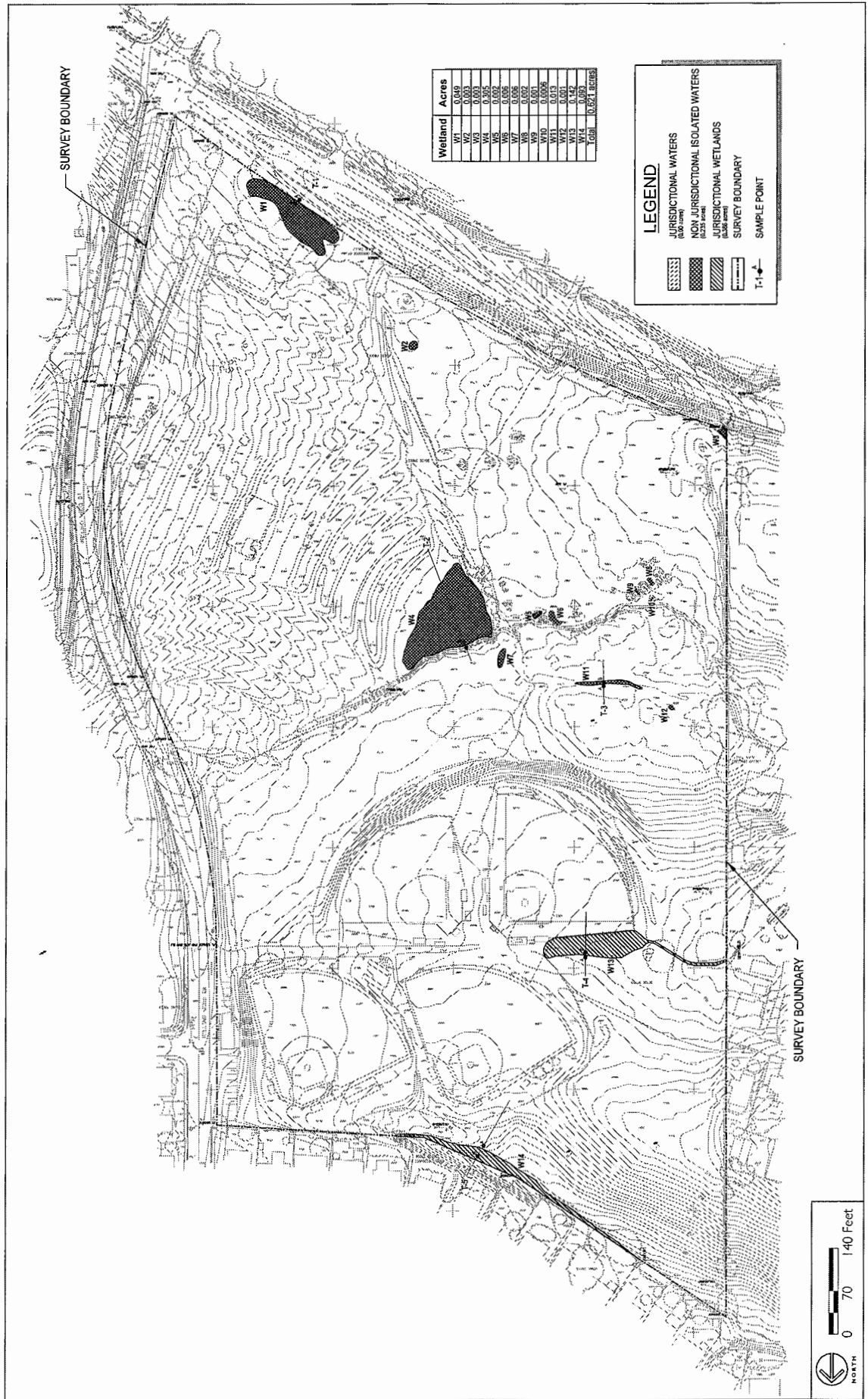


FIGURE 4-1
 WETLAND DELINEATION MAP

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Mitigation Program shall include the following components and meet the following standards:

- ◆ Proposed grading and development shall be redesigned to preferably avoid removal or adverse impacts to areas verified as jurisdictional wetlands. Where avoidance is infeasible, adequate mitigation for any direct or indirect impacts on jurisdictional waters shall be provided as coordinated with the Corps and/or RWQCB. Replacement wetlands shall be provided at a minimum 2:1 replacement ratio and shall be established in suitable locations within undeveloped open space areas, preferably on-site but possible off-site if an acceptable location is selected and approved by jurisdictional agencies. The wetlands replacement component of the Wetland Mitigation Program shall emphasize establishment of native freshwater marsh and seasonal wetlands to replace and enhance existing habitat values.
- ◆ The wetland replacement component of the Wetland Mitigation Program shall specify performance criteria, maintenance and long-term management responsibilities, monitoring requirements, and contingency measures. Monitoring shall be conducted by the qualified wetland specialist for a minimum of five years and continue until the success criteria are met.

Significance after Mitigation: Less than significant.

- d. *Less than Significant.* Proposed development would eliminate most of the existing habitat on the site, but the site is generally defined by degraded non-native, ruderal grasslands and areas of pavement or turf, with little habitat value. Development currently surrounds the entire site, limiting opportunities for movement and dispersal. Pre-construction surveys recommended in Mitigation Measure BIO-2 would ensure that any bird nests in active use are protected. Given the relatively low habitat value of the site, proposed development is

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not expected to have a significant impact on wildlife, and no mitigation is considered necessary.

- e. *No Impact.* The proposed project is not expected to conflict with any local ordinances or policies related to biological resources. Numerous non-native trees would be removed to accommodate proposed development, but new landscaping would include trees, shrubs, and groundcovers, that would serve to replace their existing habitat value. The City does not have a heritage-tree preservation ordinance that would need to be considered during tree removal.⁴ Mitigation measures recommended above would serve to address any potential adverse impacts on wetland or other sensitive resources on the site. *No impact* would occur.
- f. *No Impact.* There are no adopted Habitat Conservation Plans, Natural Community Conservation Plans, or other approved local, regional, or state habitat conservation plans encompassing the site and vicinity, so *no impact* would occur. No mitigation measures are required.

5. CULTURAL RESOURCES

- a.-d. *Potentially Significant Unless Mitigation Incorporated.* A record search by William Self Associates indicates that four structures existed on the project site by 1863. A visual inspection of the site was conducted by William Self Associates in Fall 2006 and there were no visual indicators of significant historical structures. Although a large portion of the site remains covered in asphalt, compacted gravel, dried grasses and weeds, the possibility exists that potentially significant historic resources are buried, and could be discovered during construction. These resources may include, but are not necessarily limited to, the foundations, of the previously existing structures.

⁴ Chapter 10.12 of the City of Vallejo Municipal Code (Trees). <http://municipalcodes.lexisnexis.com/codes/vallejo/>. Accessed August 23, 2007.

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According to the William Self Associates study, no archeological or paleontological resources, geologic features, or human remains are known to exist at the project site. However, the possibility remains that such resources may exist and be encountered during construction activities.⁵

In the event that such resources, features or human remains are discovered during construction, the following mitigation measures would reduce potential impacts to a *less-than-significant* level.

Mitigation Measure CUL-1. In the event that unique historical, archeological paleontological or geologic features are discovered during construction, a qualified archeologist or geologist shall be contacted immediately to determine the significance of the resource. Construction activities shall be halted and not commence until the expert has issued an opinion about the resource and appropriate mitigation has been determined.

Mitigation Measure CUL-2. In the event that human remains are discovered on the project site during construction activities, the following steps shall be taken according to CEQA Guidelines Section 15064.5 (e)(1). First there shall be no further excavation of the site or any nearby area until the Solano County coroner is contacted to determine that no investigation of the cause of death is required. Second, if the coroner determines that the remains are of Native American decent, the coroner will contact the Native American Heritage Commission within 24 hours.

Significance after Mitigation: Less than significant.

⁵ William Self Associates, *Archaeological Survey and Cultural Resources Assessment for 1300 Benicia Road Vallejo, Solano County, California*, October 30, 2006.

6. GEOLOGY AND SOILS

a.

i. No Impact. The property is not located within the Alquist-Priolo Earthquake Fault Zone as defined by the California Geologic Survey.⁶ The project site is therefore is not susceptible to ground surface rupture during an earthquake.⁷ *No impact* would occur.

ii. Less than Significant. As noted in the 2006 Geotechnical Feasibility study (Stevens, Ferrone, and Bailey; 2006), the site is located in a seismically active region, and within 25 miles of three active fault zones: the Green Valley (South) Fault Zone, West Napa Fault Zone, and Rodgers Creek Fault Zone. Due to the close proximity of these zones, the site will probably be subjected to at least one moderate to severe earthquake that will cause strong ground shaking and has a 10 percent probability within the next 50 years of exceeding a peak ground acceleration of about 0.43g.⁸ Although potential ground shaking at a site cannot be mitigated, the project would be required to adhere to the specifications of the California Building Code as a condition of approval. Because the site is located in Seismic Zone 4, the specifications for construction within Zone 4 would be followed and no mitigation is required. As a result, a *less-than-significant* impact would occur.

iii. Less than Significant. Soil liquefaction is a phenomenon primarily associated with saturated, cohesionless soil layers located close to the

⁶ The City of Vallejo is not listed as a city affected by the Alquist-Priolo Earthquake Fault Zones as listed by the California Geologic Survey, <http://www.consrv.ca.gov/CGS/rghm/ap/affected.htm>, accessed August 22, 2007.

⁷ The Alquist-Priolo Earthquake Fault Zone is a 500 foot zone on either side of an active fault trace where, among other things, the possibility of ground-rupture from fault movement is considered to exist.

⁸ This measurement is 0.43 the acceleration due to gravity or 43% of the acceleration of gravity.

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ground surface. Soils that are most susceptible to liquefaction are loose, sandy soils subject to water intrusion. The potential risk of liquefaction at the site location is low, because rock was encountered at one-half foot to seven feet below ground surface, and medium dense, saturated granular soils were not encountered below ground-water.⁹ As a result, of the solid composition below the site, a *less-than-significant* impact would occur.

- iv. *No Impact.* The site is located in a relatively flat area, surrounded by gently rolling slopes. The threat of a mudslide or landslide is, therefore, highly unlikely. *No impact* would occur.
- b. *Potentially Significant Unless Mitigation Incorporated.* As stated in response to criteria a.iv), the site terrain is relatively flat and is not bordered by any steep slopes. Therefore, the potential for landslides is low. As explained in the response to criteria a.iii), the potential for liquefaction was determined to be low in the 2006 geotechnical study.

Lateral spreading and subsidence have the potential to occur in areas where non-uniform soils are subjected to seismic forces or load-bearing. As stated in the 2006 geotechnical report, fill material and soft, weak and potentially compressible clayey soils exist within the upper ½ to 5 feet on-site; on average these materials are estimated to extend to depths of about 3 feet over 80 percent of the site.¹⁰ Lateral spreading and subsidence of these soils could occur if exposed to sufficient force or bearing. Implementation of Mitigation Measure GEO-1 would reduce potentially significant impacts to a *less-than-significant* level.

⁹ Stevens, Ferrone, and Bailey; 2006, Geotechnical Feasibility Investigation, Rollingwood Drive Residential Development, Vallejo, CA. Page 6.

¹⁰ Stevens, Ferrone, and Bailey; 2006, Geotechnical Feasibility Investigation, Rollingwood Drive Residential Development, Vallejo, CA. Page 8.

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Mitigation Measure GEO-1: In portions of the site where clayey soils and fill material will not be removed during grading and where proposed improvements (new fill, pavements, slabs on grade) will be constructed, these existing sub-surface materials should be removed and recompact. The process may consist of removing the upper two feet of these materials, and scarifying and re-compacting the lower one foot prior to placing new fill or improvements. The process will need to be confirmed during a future geotechnical investigation. Subsequent to this investigation and prior the issuance of a grading permit, the applicant shall submit a plan to the City for review and approval, which will identify the steps for removal or treatment of soils on-site.

Significance after Mitigation: Less than significant.

- c. *Less than Significant.* The project will result in the loss of topsoil due to excavation and grading activities during construction. Unless mitigated, construction-period erosion is considered a potentially significant impact. Erosion prevention would be addressed through the development of a Storm Water Pollution Prevention Plan (SWPPP). The SWPPP, which would be required as part of project approval, would identify a series of Best Management Practices (BMPs) that would be employed to mitigate the potentially significant impacts associated with erosion, such as sedimentation of receiving water bodies Best Management Practices could include, but may not be limited to spraying water and placing berms on disturbed areas, and covering soil stock piles with tarps. Beyond the SWPPP, no mitigation is required to address topsoil loss and possible erosion. A *less-than-significant* impact would occur.

- d. *Potentially Significant Unless Mitigation Incorporated.* This site exhibits clayey expansive surface materials, which could be subject to volume changes during seasonal fluctuations in moisture levels. Ex-

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pansion of soils during periods of high-moisture could result in subsidence as described in the Stevens, Ferrone, and Bailey geotechnical report (2006). Implementation of Mitigation Measure GEO-2 would reduce potential impacts to a *less-than-significant* level.

Mitigation Measure GEO-2: To reduce the potential for post-construction distress to the proposed residential structures resulting from swelling and shrinkages of clayey soils, post-tensioned slabs should be used for foundation support over the engineered and compacted fill after the removal of the clay-rich overburden.

Significance after Mitigation: Less than significant.

- e. *No Impact.* Septic tanks and/or alternative waste water disposal systems are not included as part of this project. As a result, *no impact* would occur.

7. HAZARDS AND HAZARDOUS MATERIALS

- a. *Less than Significant.* Hazardous materials used during construction are a potential concern. These materials typically include gasoline, diesel fuel, lubricating oil, grease, hydraulic fluid, solvents, caulking and paint. Potential impacts during construction, including unforeseen accidents, from the use of these materials on-site would be reduced to less-than-significant levels through compliance with applicable regulations, as well as the use of standard handling practices followed by trained personnel.

Following construction, the project would involve the use of hazardous materials at volumes typically associated with residential development. Types of materials commonly utilized are cleaners (various types of detergents), fertilizers, and pesticides. Considering the types and volumes of materials to be used on-site, a potential accident

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(spill of a solvent) does not represent a significant hazard to the environment. Furthermore, the project would not involve the disposal of hazardous materials on-site such that a potentially significant risk to public health would exist. As a result, a *less-than-significant* impact would occur.

- b. *Less than Significant.* The proposed project would create a significant hazard to the public or the environment if it resulted in the reasonably foreseeable upset or release of hazardous materials into the environment. As discussed in response to criteria a) above, residential use at the property will result in the use of commonly employed household chemicals and hazardous materials. The residential uses would be expected to generate only small quantities of hazardous materials, such as household batteries, paint, and cleaners. The accidental release of hazardous materials is unlikely to have a substantial affect on the environment. As a result, the potential impact is *less than significant*.
- c. *No Impact.* There are no schools within ¼ mile of the project site. The nearest school is Saint Patrick's High School at 1500 Benicia Road, which is located approximately 0.6 mile from the project site.¹¹ As a result, *no impact* would occur.
- d. *Potentially Significant Unless Mitigation Incorporated.* The project site is listed on the HAZNET regulatory database. HAZNET is a Department of Toxic Substances Control (DTSC) database with information on over 500,000 annual hazardous waste shipments entered from over one million shipping manifests per year. The HAZNET record indicates that 0.03 tons of organic solids and 8.42 tons of asbestos containing waste were removed from the site on September 20, 2002. The site was subsequently listed as inactive (no

¹¹ Google Maps confirmed that 1500 Benicia Road is 0.6 miles from the intersection of Benicia Road and Rollingwood Drive, <http://maps.google.com>, accessed August 21, 2007.

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longer producing waste) on August 19, 2003. This documented waste removal was most likely related to demolition or renovation activities which involved the removal of asbestos containing building materials.

The site is also listed as having some lead and arsenic in the soil as a result of a study completed by Kleinfelder, Inc. on behalf of Rollingwood Middle School in 2003. The physical address of this study became attached to the Rollingwood Middle School address and thus does not show up as an issue in the regulatory database as a listed site for potential lead and arsenic concern. The DTSC provided oversight per the requirements of the California Department of Education, approving the preliminary endangerment assessment in September 2003. In June 2004, at the request of the Vallejo Unified School District, the DSTC placed the property on the inactive list because the school district decided not to pursue the development of the property.

According to DTSC, the Kleinfelder study completed for Rollingwood Middle School sampled for lead, arsenic and pesticides.¹² The Kleinfelder study found relatively minimal concentrations of lead and pesticides in the soil, but up to 110 mg/kg arsenic in the areas of the former drive-in and Little League ballpark. The residential Environmental Screening Level (ESL) for arsenic is 0.06 mg/kg. The arsenic was thought to result from historical use of weed suppression. The specific details of the study, such as number and depth of samples, locations or range of concentrations is not known at this time but can be reviewed at the DTSC Sacramento offices. The depth of soil affected is likely to be shallow based on low solubility and mobility of arsenic in soil. Based on the indication of significantly higher arsenic concentrations in soils compared with the ESLs, the

¹² Kamili Siglowide, Department of Toxic Substance Control, State of California, personal communication with Stellar Environmental Solutions, September 2007.

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impact is considered *potentially significant*. The following mitigation measures would be implemented to reduce potential impacts to a less than significant level.

Mitigation Measure HAZ-1: Prior to any site improvements, including grading and preliminary site work, the applicant shall review the Kleinfelder study at the DTSC Sacramento offices. Depending on the extent of the soil sampling completed to date and the determinations of the Kleinfelder study, additional sampling shall be conducted to better define the extent of contamination on-site. Once sufficient sampling has been completed in accordance with DTSC guidelines, the samples shall be evaluated and mitigation measures developed, as appropriate.

Mitigation Measure HAZ-2: Before any soil can be exported off-site it shall be profiled for landfill disposal requirements. If harmful concentrations of pesticides, lead and arsenic are reported in the soil, per DTSC guidelines, the contaminated soil shall not be reused for landscaping. Rather, landscaping soils shall be imported from an off site source.

Significance after Mitigation: Less than significant. If impacts related to hazardous materials cannot be mitigated to a less than significant level, a new environmental review would be required.

- e. *Less than Significant.* As shown on Figure 2-2 of the Project Description, the project site is within a largely urbanized area where the level of risk associated with wildland fires is relatively low. The proposed project will lessen the potential for wildfires compared to the existing open space on the majority of the site, which contains flammable weeds and vegetation that are susceptible to burning. Potential impacts related to wildland fires are therefore *less than significant*.

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- f. *No Impact.* The project would not involve any permanent changes to existing roads or other infrastructure that could adversely affect the implementation of an emergency response or evacuation plan. During construction, temporary lane closures may be required; however these closures would be adjusted, as necessary, to accommodate the passage of emergency vehicles at all times. Therefore, *no impact* would occur.
- g. *No Impact.* The project site is not within an airport land use plan or within two miles of a public airport or public use airport. The closest airports are the Napa County Airport, approximately 13.4 miles from the site, Buchanan Field Airport, approximately 13.2 miles from the site, and the Oakland International Airport, approximately 30 miles to the south. As a result, *no impact* would occur.
- h. *No Impact.* The project site is not located within the vicinity of a private airstrip such that potentially significant impacts to public safety would exist.¹³ As a result, *No impact* would occur.

8. HYDROLOGY AND WATER QUALITY

- a. *Less than Significant.* The proposed project would result in the replacement of permeable surfaces, such as the existing Little League fields, with impervious surfaces, such as streets and building roofs. This change would reduce the natural infiltration of storm water that would take place on-site, increasing stormwater peak flows and runoff volumes.

Approximately six acres of the project site currently drain to a swale that straddles the northern boundary of the project site. The swale drains to an existing storm drain system located on Dryden Drive. The Project would reduce the runoff area from six to two acres, thus

¹³ My Airport/Facility Directory website, <http://www.myafd.com>, accessed on August 22, 2007.

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reducing stormwater runoff to Dryden Drive. Storm runoff throughout the remainder of the site will be collected in standard street level inlets and piped to the on-site park where it will be naturally filtered through landscaping for treatment. From the park, stormwater will drain via gravity flow to an existing storm drain below Benicia Road. The increase in storm water runoff will require the replacement of an approximately 450-foot portion of the existing storm drain within the right of way for Benicia Road.

The drainage improvements specified above will be constructed within areas that have already been disturbed through the installation of existing infrastructure or areas that would already be disturbed as a result of the project. Therefore, construction of the drainage improvements would not, by themselves, result in a potentially significant impact beyond those already identified in this analysis. As a result, a *less than-significant* impact would occur.

- b. *Less than Significant Impact.* Construction activities could result in soil erosion and siltation that could harm water quality by increasing turbidity and may allow for runoff contaminated with diesel fuel, gasoline, paint, concrete, wood preservatives, or any other construction-materials likely to be used or stored on the site during construction.

Construction materials and equipment could also contaminate site soils and the ground surface. This contamination could be spread by runoff of storm water, wash water, or moisture used to condition soils during site grading and construction. This represents a potentially significant impact.

The project applicant is required to comply with all Phase II NPDES Permit requirements for the construction period because the construction site is larger than 1 acre in size. Under the Phase II program, the applicant is required to submit a Notice of Intent (NOI)

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with the State Water Resource Control Board's (SWRCB) Division of Water Quality. The NOI includes general information on the types of construction activities that will occur on the site.

As previously discussed in the Geology and Soils section, the applicant will also be required to prepare a Storm Water Pollution Prevention Plan (SWPPP). The SWPPP will include a description of appropriate Best Management Practices (BMPs) to minimize the discharge of pollutants from the site. Typical requirements include, but are not limited to, the following:

- ◆ Performing major vehicle maintenance, repair jobs, and equipment washing at appropriate off-site locations;
- ◆ Maintaining all vehicles and heavy equipment and frequently inspecting for leaks;
- ◆ Designating one area of the construction site, well away from streams or storm drain inlets, for auto and equipment parking and routine vehicle and equipment maintenance;
- ◆ Cleaning-up spilled dry materials immediately. Spills are not to be "washed away" with water or buried;
- ◆ Using the minimum amount of water necessary for dust control;
- ◆ Cleaning-up liquid spills on paved or impermeable surfaces using "dry" cleanup methods (e.g. absorbent materials such as cat litter, and/or rags);
- ◆ Cleaning-up spills on dirt areas by removing and properly disposing of the contaminated soil;
- ◆ Storing stockpiled materials, wastes, containers and dumpsters under a temporary roof or secured plastic sheeting;
- ◆ Properly storing containers of paints, chemicals, solvents, and other hazardous materials in garages or sheds with double containment during rainy periods;

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- ◆ Applying concrete, asphalt, and seal coat during dry weather. Keeping contaminants from fresh concrete and asphalt out of the storm drains and creeks by scheduling paving jobs during periods of dry weather and allowing new pavement to cure before storm water flows across it;
- ◆ Covering catch basins and manholes when applying seal coat, slurry seal and fog seal.

BMPs identified in the SWPPP could also include soil stabilization techniques such as: hydroseeding and short-term biodegradable erosion control blankets; silt fences or some kind of inlet protection at downstream storm drain inlets; post-construction inspection of all drainage facilities for accumulated sediment; and post-construction clearing of all drainage structures of debris and sediment. Finally, the project applicant will be required to submit a Notice of Termination (NOT) when site soils are stable and permanent erosion and sediment control is in place.

Following construction, project development would increase the resident population at the site, therefore requiring more waste treatment, potentially hazardous materials storage, and disposal. Wastewater would be routed into the City's sewer system and therefore be subject to all discharge requirements. The impact of the project on water quality standards or on waste discharge requirements will depend on the adequacy of infrastructure for liquid waste disposal.

The project would also increase the potential for non-point source (NPS) pollutants by increasing impermeable surface area and runoff velocities. NPS pollutants are washed by rainwater from rooftops, landscape areas, streets and parking areas into the drainage network. Increased levels of NPS pollutants and litter entering downstream drainage facilities could have adverse effects on wildlife, fauna, and human health.

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Non-point pollutants from the proposed project would likely be consistent with suburban high-density residential areas and parking lots. Increases in the levels of oil and grease, petroleum hydrocarbons, metals, and possibly nutrients on the project site are likely. This is considered a *significant* impact. As part of the final project approval process, the applicant would be required to submit a drainage plan for review and approval by the Regional Water Quality Control Board (RWQCB). The drainage plan would identify the features that would be incorporated into the project to ensure that the development does not result in a substantial increase in NPS pollutants to receiving water bodies. These features would include, but may not be limited, bio-filtration of storm water runoff through the on-site park.

Therefore, completion of a construction-period SWPPP and a post-construction drainage plan, subject to approval by the RWQCB, would reduce potential impacts to a *less-than-significant* level.

- c. *Less than Significant.* Groundwater recharge occurs on the majority of the site through direct infiltration of storm water among unpaved areas. The degree of groundwater recharge will be reduced due to an increase in impervious surface area and collection of storm water through a network of new storm drains and pipes. However, given the surface area of the site in relation to undeveloped land in the surrounding areas where recharge would continue to occur, the amount of groundwater recharge occurring below the site that would be lost is considered to be insignificant. Furthermore, the project would not rely upon groundwater as a water source. As a result, the project would have a *less-than-significant* impact in regards to groundwater supply.
- d. *Less than Significant.* The predominant drainage pattern on the site is from south to north, consistent with the slope of the terrain. This pattern would be maintained during and after construction. A natu-

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rally occurring drainage swale that parallels the north side of the property appears to have some potential riparian habitat and may be considered an annual drainage area. The swale was dry at the time of the site inspection on August 3, 2007, however the presence of riparian vegetation within (or adjacent to the swale) indicates the semi-regular existence of water.

Increased erosion and siltation are frequently associated with new construction when excavation, earth movement, and stockpiling of materials take place. Erosion and siltation could temporarily impact the drainage swale on the north side of the site during construction. Siltation or sedimentation of the swale is a potentially significant impact in that obstruction of flows within it could cause erosion, siltation or flooding in other portions of the site or on adjacent parcels. Through the development of a construction-period SWPPP, as discussed above in response to criteria b), potential impacts related to sedimentation of the drainage swale would be reduced to a *less-than-significant* level.

- e. *Less than Significant.* The project applicant has completed a preliminary hydrologic analysis to calculate the potential hydrologic increase in stormwater volumes that would result from the project. Based on this analysis, it was determined that the project would not cause an increase in stormwater runoff.¹⁴ As a result, a *less-than-significant* impact would occur.

- f. *No Impact.* Beyond potentially significant impacts to water quality identified above in response to criteria b), the project would not otherwise degrade surface or groundwater quality. All potential impacts to these resources were identified in this response and no additional impact would occur.

¹⁴ Falgout, Mark A. Project Engineer, Ruggeri-Jensen-Azar and Associates. Email correspondence, August 16, 2007.

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- g. * *No Impact.* The property site is not within a 100-year flood zone as the property is not included within the Federal Emergency Management Agency's (FEMA) Flood Insurance Rate Map (FIRM) of Vallejo.¹⁵ *No impact* would occur.
- h. *No Impact.* As stated in response to criteria g, the project site is not located within a 100-year flood hazard area and would therefore not have any effect on flood flows generated during a 100-year storm event. *No impact* would occur.
- i. *No Impact.* Lake Chabot Dam and Lake Herman Dam are the closest dams to the proposed project; however, the project site does not lie down gradient from either dam.¹⁶ Additionally, no levees are within close proximity to the project site.¹⁷ The proposed project would not be impacted by flooding as a result of a levee or dam failure.
- j. *No Impact.* The project location is not within close proximity of a large body of enclosed or open water that could be the source of a seiche or tsunami. Furthermore, the project location is within a developed area of Vallejo where undeveloped hillsides do not pose a risk related to mudflows. As a result, the project would experience no impact related to the possibility of sieche, tsunami, or mudflows.

¹⁵ Federal Emergency Management Agency, Flood Insurance Rate Map 0606310625B.

¹⁶ Google Maps confirmed that Lake Chabot Dam and Lake Herman Dam are the closest dams to the project site, <http://maps.google.com>, accessed August 21, 2007.

¹⁷ City of Vallejo General Plan does not cite levees as a possible threat to residents.

9. LAND USE

- a. *No Impact.* The proposed project includes the construction of 214 residential units on an infill site, located near the intersection of Rollingwood Drive and Benicia Road. The site currently contains four little league fields, but is otherwise vacant. Although residential development would be a new use of the site, it would represent a continuation of existing residential uses that define the adjacent areas to the north and west of the project site. Furthermore, the project does not include any construction off-site that would potentially affect the cohesion of surrounding communities. As a result, the project would have *no impact* in terms of physically dividing a community.

- b. *No Impact.* Due to the residential nature of the surrounding area, the project would not create or exacerbate a conflict between land uses. As stated above in response to criteria a), the project would represent a continuance of residential uses located to the north and west of the site. Furthermore, the project would not adversely affect the function or quality of the cemeteries to the south and east of the project site. The cemeteries would continue to operate during and after construction. As a result, *no impact* would occur.

- c. *Less than Significant.* The Project will require a change in the City's General Plan land use and zoning designations. Currently, the project site is designated as Retail, and zoned as PF – Public and Quasi-Public Facilities and CP – Pedestrian Commercial.¹⁸ The project applicant proposes a change in the land use designation to Low Density Residential, and the zoning to Planned Development Residential. The land use designation and zoning changes will not conflict with any land use plan, policy or regulation that has been adopted for the purpose of avoiding or mitigating an environmental impact. As a result, the project will result in a *less-than-significant* impact.

¹⁸ City of Vallejo, Enterprise GIS Website, <http://gis.ci.vallejo.ca.us/welcome.asp>, accessed July 16, 2007.

10. MINERAL RESOURCES

- a. *No Impact.* There are no known mineral resources located on the project site.¹⁹ A deposit of greenstone and greywacke has been identified at Sulphur Springs Mountain, a portion of which is within the City of Vallejo's Sphere of Influence. However, the site is approximately two miles from the project site and will not be affected by the project. As a result, *no impact* would occur.

11. NOISE

- a. *Potentially Significant unless Mitigation Incorporated.* The project includes the development of 214 housing units and a 4.1-acre park. Primary outdoor use areas for residences (e.g., private rear yard areas in single-family developments or common use areas in multi-family developments) are considered "normally acceptable" in noise environments up to 60 dBA Ldn. Noise levels up to 65 dBA Ldn may be allowed at the discretion of the City where it is not economically or aesthetically reasonable to meet the more restrictive outdoor goal. Small decks and entry porches do not need to meet these goals. Interior noise levels are required to be maintained at or below 45 dBA Ldn.

Noise measurements were made to establish the existing noise environment at the project site and vicinity. Ambient noise levels result primarily from local traffic along Rollingwood Drive and Benicia Road. Noise levels generated by traffic along Rollingwood Drive were 66 dBA Ldn at a distance of 65 feet from the centerline. Traffic along Benicia Road generated a day-night average noise level of 62 dBA Ldn at a distance of 50 feet from the roadway centerline. Am-

¹⁹ City of Vallejo, General Plan 1999 Update, page XI-5.

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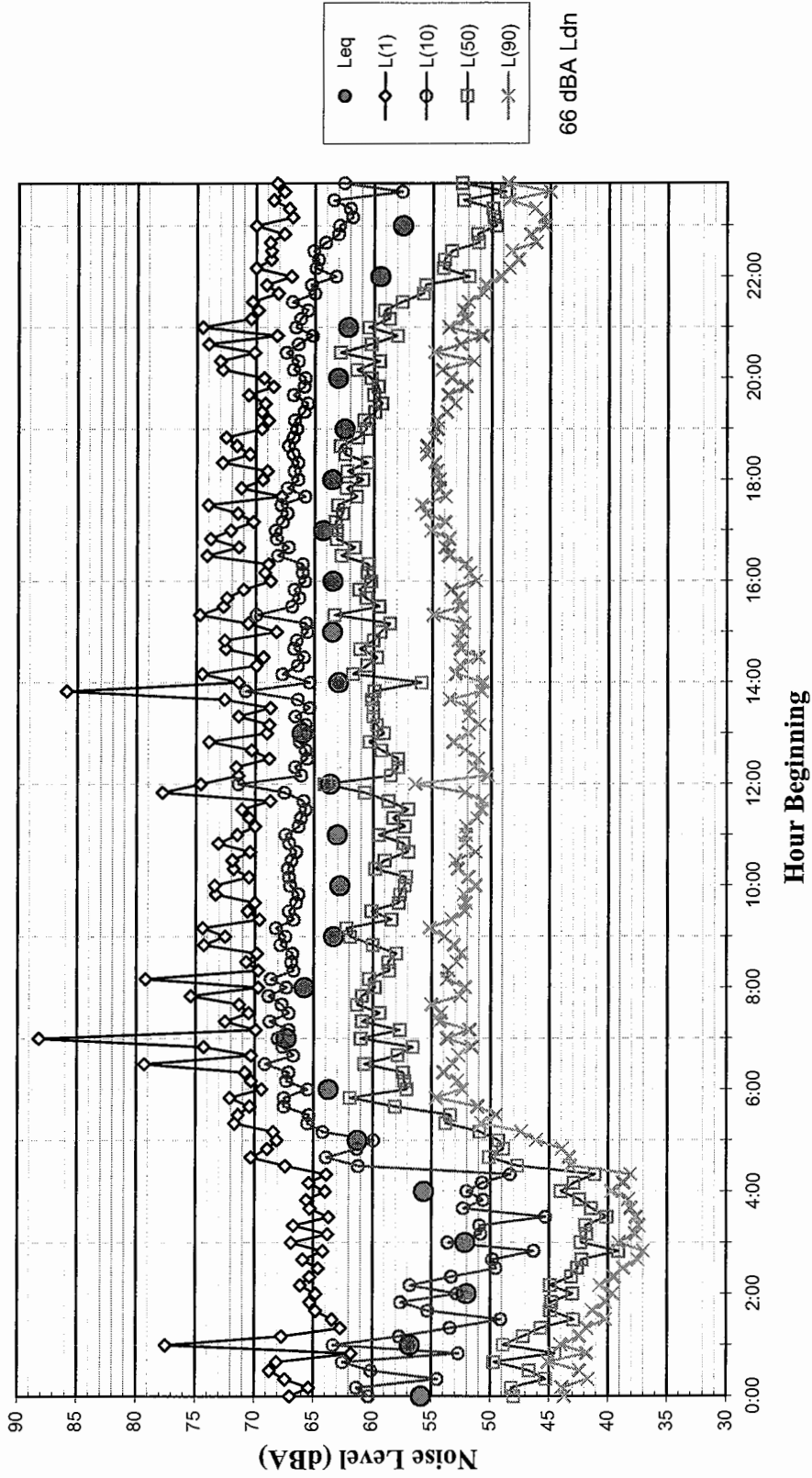
bient noise data measured at this site are summarized on Figures 4-2 and 4-3.

The future noise environment at the project site is anticipated to increase by about 2 dBA Ldn as traffic increases along adjacent roadways. Noise levels generated by future traffic anticipated along Rollingwood Drive are calculated to be 68 dBA Ldn at a distance of 65 feet from the centerline. Future traffic noise levels along Benicia Road are calculated to be 64 dBA Ldn at a distance of 50 feet from the roadway centerline. Assuming no additional attenuation from structures or topography, future noise levels would exceed 60 dBA Ldn within 250 feet of the centerline of Rollingwood Drive and within 115 feet of Benicia Road.

Paseo homes proposed nearest Rollingwood Drive and Benicia Road would be exposed to future exterior noise levels ranging from 60 dBA Ldn to 66 dBA Ldn. These homes would have small entry porches and no private exterior use areas. The exterior noise and land use compatibility standard would not be applicable at these entry porches and mitigation in the form of noise barriers would not be required.

Single-family homes proposed nearest Rollingwood Drive would be exposed to future exterior noise levels ranging from 63 dBA Ldn to 68 dBA Ldn. The exterior noise and land use compatibility standard of 60 dBA Ldn would be applicable at proposed private outdoor use areas adjoining Rollingwood Drive and mitigation would be required to reduce noise levels to "normally acceptable" levels. Six- to eight-foot noise barriers, located as shown on Figure 4-4, would be required to meet the 60 dBA Ldn noise standard. Six-foot noise barriers, located as shown on Figure 4-5, would be required to meet the 65 dBA Ldn noise standard.

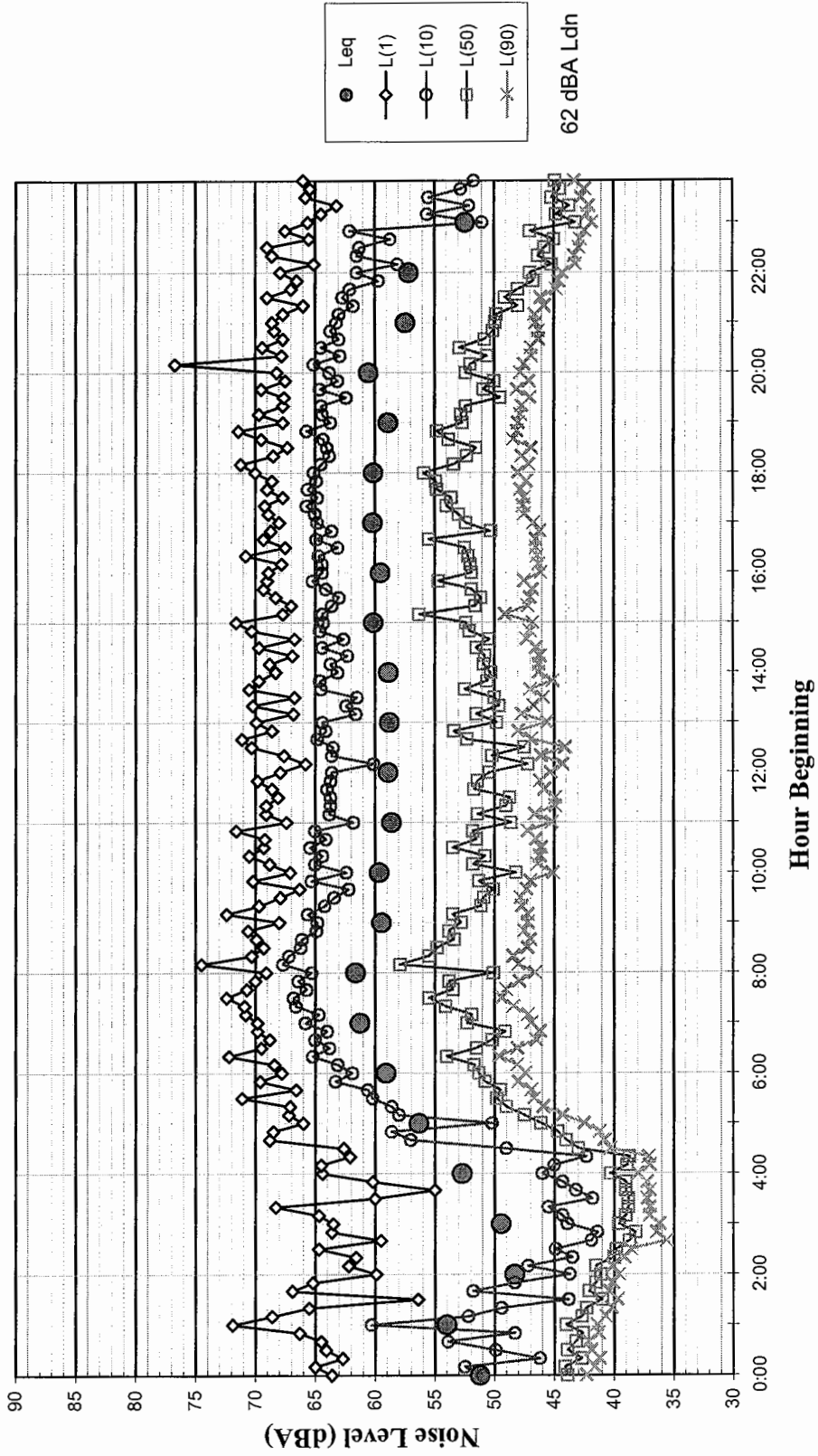
Noise Levels at LT-1
~65 feet from the Center of Rollingwood Avenue
Wednesday, July 18, 2007



Source: Illingworth & Rodkin

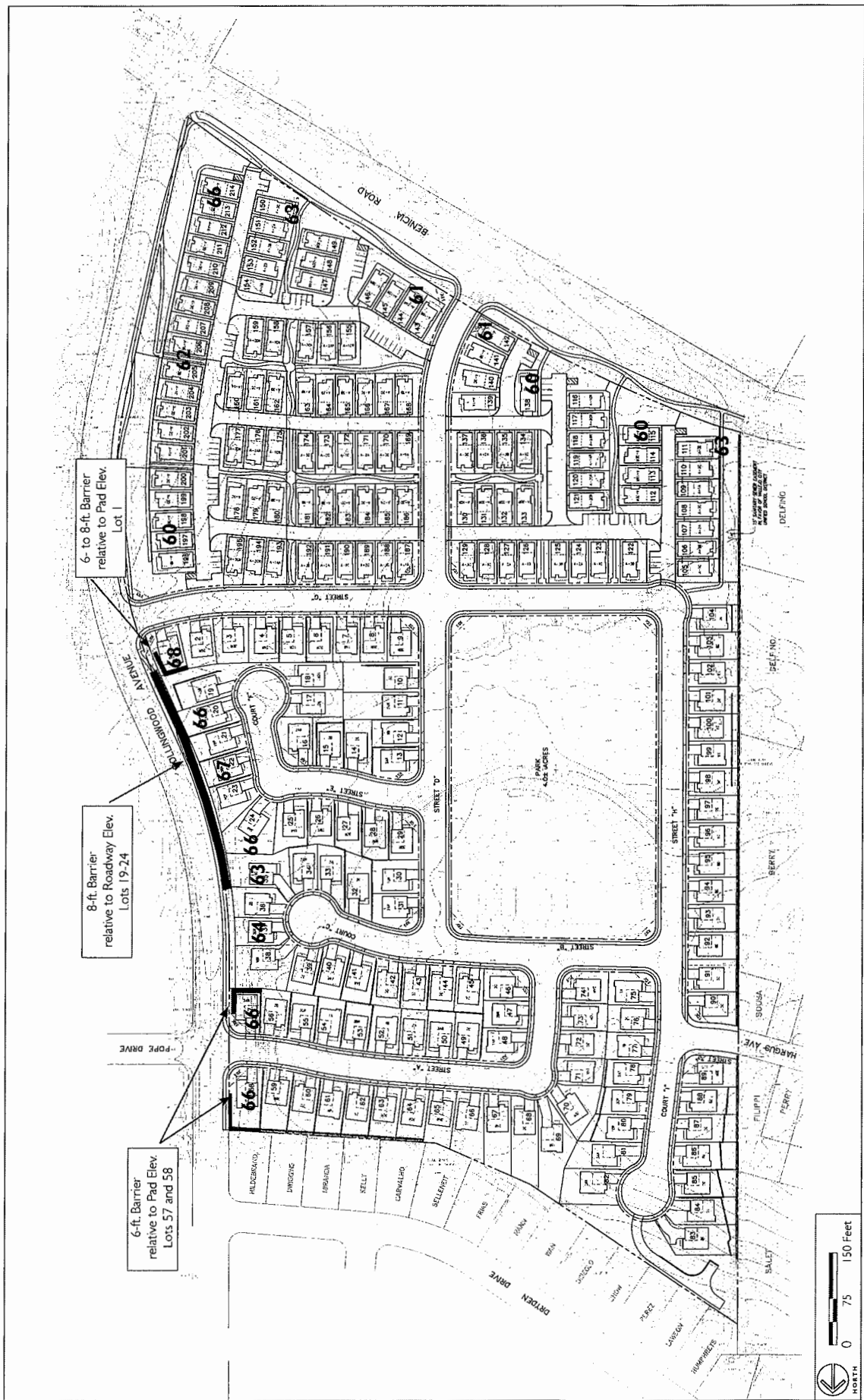
FIGURE 4-2
 AMBIENT NOISE LEVELS AT LT-1

**Noise Levels at LT-2
 ~50 feet from the Center of Benicia Road
 Wednesday, July 18, 2007**



Source: Illingworth & Rodkin

FIGURE 4-3
 AMBIENT NOISE LEVELS AT LT-2



Source: Illingworth & Rodkin

FIGURE 4-4
 FUTURE NOISE LEVELS AND NOISE BARRIERS - 60 dBA L_{dN} EXTERIOR STANDARD

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Source: Ilingworth & Rodkin

FIGURE 4-5
 FUTURE NOISE LEVELS AND NOISE BARRIERS - 65 dBA L_{dn} EXTERIOR STANDARD

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TABLE 4-2 **DEFINITIONS OF ACOUSTICAL TERMS**

Term	Definitions
Decibel, dB	A unit describing the amplitude of sound.
A-Weighted Sound Level, dBA	Decibel level as measured using the A-weighting filter network which de-emphasizes the very low and very high frequency components of the sound in a manner similar to the frequency response of the human ear and correlating well with subjective reactions to noise. All sound levels in this report are A-weighted, unless reported otherwise.
L ₀₁ , L ₁₀ , L ₅₀ , L ₉₀	The A-weighted noise levels that are exceeded 1%, 10%, 50%, and 90% of the time during the measurement period.
Community Noise Equivalent Level, CNEL	The average A-weighted noise level during a 24-hour day, obtained after addition of 5 decibels to sound levels measured from 7:00 p.m. to 10:00 p.m. and 10 decibels to sound levels measured between 10:00 p.m. and 7:00 a.m.
Day/Night Noise Level, L _{dn}	The average A-weighted noise level during a 24-hour day, obtained after addition of 10 decibels to levels measured in the night between 10:00 p.m. and 7:00 a.m.
Ambient Noise Level	The composite of noise from all sources near and far. The normal or existing level of environmental noise at a given location.

Source: Illingworth and Rodkin.

Standard residential construction provides approximately 15 dBA of exterior to interior noise reduction assuming the windows are partially open for ventilation. Interior noise levels would exceed 45 dBA Ldn where exterior noise levels exceed 60 dBA Ldn assuming that the residential units are constructed following standard California building practices.

Exterior and interior noise levels at portions of the project site would exceed the City of Vallejo's noise and land use compatibility standards for residential land uses. This is a potentially significant impact.

Mitigation Measure NOISE-1: Where noise-sensitive exterior use areas would be exposed to an Ldn greater than 60 dBA, mitigation would be required to reduce noise levels to meet exterior and interior noise level limits.

- ◆ Construct noise barriers to reduce exterior noise levels to 60 dBA Ldn or less as shown in Figure 4-4. If the City determines that it is not economically or aesthetically reasonable to meet the 60 dBA Ldn standard, construct noise barriers as shown in Figure 4-5 to reduce noise levels to 65 dBA Ldn or less. Noise barriers shall be constructed out of materials with a minimum surface weight of 3 lbs./ft². Suitable materials include 1-in. thick wood fence boards, concrete, masonry, or metal provided that the barrier is solid over its face and at its base. In locations identified for noise barriers, the applicant shall examine the creation of earthen berms as an alternative option. Provided that berms can reduce potential impacts to a less than significant level and would not conflict with right-of-way constraints, the applicant shall coordinate with the City to identify the preferable option. The applicant shall also examine the option of combining noise barriers and berms, such as a four foot berm atop a four foot wall to achieve comparable noise reduction.
- ◆ Retain a qualified Acoustical Specialist to prepare for City review and approval a detailed acoustical analysis of interior noise reduction requirements and specifications for all units proposed within the 60 dBA Ldn contours of area roadways, in accordance with City standards. Interior noise levels must be maintained at or below 45 dBA Ldn. Building sound insulation requirements shall include forced air mechanical ventilation in noise environments exceeding 60 dBA Ldn. Special building construction techniques (e.g., sound-rated windows and building facade treatments) may be required where exterior noise levels exceed 65 dBA

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CNEL. Because the noise exposure is between 65 dBA Ldn and 70 dBA Ldn these treatments would be limited to sound rated windows and doors at the most affected rooms. Special sound rated exterior wall assemblies would not be required. The specific determination of required door and window sound transmission ratings shall be made on a unit-by-unit basis during project design. Results of the analysis, including the description of the necessary noise control treatments, shall be submitted to the City along with the building plans prior to issuance of a building permit.

Significance after Mitigation: Less than significant.

- b. *Less than Significant.* Construction techniques including the use of jackhammers, rock drills and other high-power or vibratory tools, and the operation of rolling stock equipment during earthmoving activities (tracked vehicles, compactors, etc.) may generate perceptible vibration at residential land uses in the immediate vicinity of the project site. Construction phases that have the highest potential of producing vibration would be short-term in duration and perceptible vibration would be intermittent. This is a less significant impact.
- c. *Less than Significant.* Turning Movement Volume data for eight intersections in the site vicinity²⁰ were reviewed to calculate the relative change in traffic noise levels resulting from the project. The addition of project traffic would increase noise levels by less than 1 dBA at receivers along roadways affected by project trips. This is a less-than-significant impact because the increase would not be perceptible or substantial.

²⁰ DMJM Harris/AECOM, Rollingwood Residential Turning Movement Volumes, August 10, 2007.

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Project construction activities would generate noise, and would temporarily increase noise levels at adjacent land uses. Construction-related noise levels are normally highest during the earthmoving phase and during the construction of project infrastructure. These phases of construction require heavy equipment, which generate the highest noise levels over extended periods of time. Typical hourly average construction generated noise levels are about 81 dBA Leq to 88 dBA Leq measured at a distance of 50 feet from the center of the site during busy construction periods (e.g., earth moving equipment, impact tools, etc.). Construction-related noise levels are normally less during building erection, finishing, and landscaping phases. There would be variations in construction noise levels on a day-to-day basis depending on the actual activities occurring at the site. Construction generated noise levels drop off at a rate of about 6 dBA per doubling of distance between the source and receptor. Shielding by buildings or terrain could provide an additional 5 to 10 decibels of attenuation at distant receptors.

Noise impacts resulting from construction activities would depend on the noise generated by various pieces of construction equipment, the timing and duration of noise generating activities, and the distance between construction noise sources and noise sensitive receptors. Construction noise impacts primarily occur when construction activities occur during noise-sensitive times of the day (early morning, evening, or nighttime hours), the construction occurs in areas immediately adjoining noise sensitive land uses, or when construction durations last over extended periods of time. Where noise from construction activities exceeds 60 dBA Leq and exceeds the ambient noise environment by at least 5 dBA Leq at noise-sensitive uses in the project vicinity for a period greater than one construction season, the impact would be considered significant.

Typically, small residential projects do not generate significant noise impacts when standard construction noise control measures are en-

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forced at the project site and when the duration of the noise generating construction period affecting a particular receiver or group of receivers is limited to one construction season (typically one year) or less. Construction noises associated with projects of this type are disturbances that are necessary for the construction or repair of buildings and structures in urban areas. Reasonable regulation of the hours of construction, as well as regulation of the arrival and operation of heavy equipment and the delivery of construction materials, are necessary to protect the health and safety of persons, promote the general welfare of the community, and maintain the quality of life. Limiting the hours when construction can occur to daytime hours is often a simple method to reduce the potential for noise impacts. In areas immediately adjacent to construction, controls such as constructing temporary noise barriers and utilizing “quiet” construction equipment can also reduce the potential for noise impacts.

The following standard controls are assumed to be included in the project:

- ◆ Noise-generating construction activities shall be limited to the hours between 7:00 a.m. and 6:00 p.m., Monday through Friday. No construction shall occur on weekends or holidays.
- ◆ Locate stationary noise generating equipment as far as possible from sensitive receptors. Acoustically shield stationary noise sources when located in areas adjoining sensitive receptors.
- ◆ Utilize “quiet” air compressors and other “quiet” equipment where technology exists.
- ◆ Prohibit unnecessary idling of construction equipment.
- ◆ Properly maintain and muffle all internal combustion-driven construction equipment.
- ◆ The contractor shall prepare a detailed construction plan identifying the schedule for major noise-generating construction activities. The construction plan shall identify a procedure for co-

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ordination with the adjacent noise sensitive residential uses so that construction activities can be scheduled to minimize noise disturbances.

- ◆ Designate a "disturbance coordinator" who would be responsible for responding to any local complaints about construction noise. The disturbance coordinator will determine the cause of the noise complaint (e.g., starting too early, bad muffler, etc.) and will require that reasonable measures warranted to correct the problem be implemented. Conspicuously post a telephone number for the disturbance coordinator at the construction site and include it in the notice sent to neighbors regarding the construction schedule. (The City shall be responsible for designating a noise disturbance coordinator and the individual project sponsor shall be responsible for posting the phone number and providing construction schedule notices).

With the incorporation of these standards measures, the noise impact resulting from project construction would be considered *less-than-significant*.

- d. *Less than Significant*. There are no public or private airports within the site vicinity. Noise levels generated by aircraft that may occasionally fly over the site would be compatible with the proposed residential land uses. This is a less than significant impact.

12. POPULATION AND HOUSING

- a. *Less than Significant*. The Project would increase the number of residential units in Vallejo by 214 and increase the population by approximately 616 residents by 2010.²¹ According to the Association of

²¹ Average household size calculated from ABAG's Projections 2007 for the City of Vallejo. Household population of 128,000 / 44,370 households = 2.88 persons per household. Project population assumes an average household size of 2.88 persons

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Bay Area Government's (ABAG) Projections 2007, the City's population is expected to increase from 122,900 in 2005 to 129,800 in 2010, amounting to an increase of 6,900 people. During the same period, the number of households in the City is projected to increase by 2,040.²² Therefore, the projected increase in population and dwelling units associated with the construction of the Project through 2010 would not exceed ABAG's growth projections for the City of Vallejo and not result in unexpected or unplanned growth.²³ As a result, a *less-than-significant* impact would occur.

- b-c. *No Impact.* The project site contains one abandoned, dilapidated residential structure, the demolition of which will not displace any habitable dwelling units or residents. Additionally, the project will result in a net increase of 214 residential units within the City, contributing to Vallejo's fair share housing allocation. The project would not displace a substantial number of housing units or people that would necessitate construction of replacement housing. As a result, *no impact* would occur.

13. PUBLIC SERVICES

- a. *Less than Significant.* The Project would add 214 residential units to a site that does not contain any residences. Current fire codes would be incorporated into all design aspects of the units and project site. While these features would not eliminate the risk of a fire altogether, they would reduce the potential for impacts from fires.

The closest fire station is located approximately 0.6 miles from the proposed project site at 1335 Fulton Avenue. The station is operated

x 214 new dwelling units = 616 people.

²² Association of Bay Area Governments, *Projections 2007*, page 200.

²³ City of Vallejo population estimates included in the Vallejo General Plan were not used. The age of the data resulted in population estimates that were higher than projections provided by ABAG and the U.S. Census Bureau.

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by the City of Vallejo Fire Department and includes 1 engine and 1 utility vehicle and is continuously staffed with 1 captain, 1 engineer and 1 fire fighter. Based on the inclusion of on-site fire prevention features and, the close proximity of the fire station, it is not expected that the Project would require new or expanded fire protection services that could result in significant impacts. As a result, a less-than-significant impact would occur.²⁴

- b. *Less than Significant.* The Project would result in a population increase of approximately 616 people.²⁵ The officer-to-population ratio in Vallejo is roughly 1.2 officers per 1,000 residents.²⁶ Based on this ration, the proposed project would require roughly 0.5 new police officers. It is not expected that new or expanded facilities would be required to accommodate additional officers that may be needed as a result of the project. As a result, a less-than-significant impact would occur.
- c. *Less than Significant.* The introduction of 214 residential units will result in approximately 25-35 new students attending Vallejo City Unified School District.²⁷ The school district is currently experienc-

²⁴ White, Greg. Inspector, City of Vallejo Fire Department. Personal communication with Kyle Simpson, DC&E, August 29, 2007.

²⁵ Average household size calculated from ABAG's Projections 2007 for the city of Vallejo. Household population of 128,000 / 44,370 households = 2.88 persons per household. Project population assumes an average household size of 2.88 persons x 214 new dwelling units = 616 people.

²⁶ Florendo, Rick. Sergeant, Vallejo Police Department. Personal communication with Kyle Simpson, DC&E, August 20, 2007.

²⁷ Gonella, Floyd. Superintendent, Vallejo City Unified School District, personal communication with Kyle Simpson, DC&E. August 24, 2007. The generation of 25-35 new students as a result of the project was calculated by VCUSD and is based on the square-footage of the residential development and the price per unit.

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ing excess capacity and will be able to accommodate students generated as a result of this project.²⁸

- d. *No Impact.* The proposed project includes one 4.1-acre park centrally located within the park. The fully improved park will be given to the Greater Vallejo Recreation District and will include a sports field, picnic areas, and play structures. As a result, the project would not cause substantial adverse impacts associated with constructing new or physically altering existing parks. *No impact* would occur.
- e. *Less than Significant.* The Solano County Library system would serve the Project with the main library, John F. Kennedy Library in downtown Vallejo, and Springstowne Library in eastern Vallejo. The Solano County library has service ratios of 0.76 square feet per capita and 2.9 volumes per capita. These ratios are currently achieved.²⁹

Springstowne Library, the closest library to the project site, is 3,024 square feet (SF) and is located at 1003 Oakwood Avenue. In February 2001, the Solano County Library Facilities Master Plan called for the relocation of Springstowne Library and the construction of a 36,000 SF library facility in eastern Vallejo by 2020. A construction timeline has not been established for the new Springstowne Library.³⁰ The construction of the new library facility is not required as a result of the proposed project, and will require a complete environmental review for the construction.

²⁸ Gonella, Floyd. Superintendent, Vallejo City Unified School District, personal communication with Kyle Simpson, DC&E. August 14, 2007.

²⁹ Cousineau, Ann. Director of Library Services, Solano County Library, personal communication with Kyle Simpson, DC&E, August 9, 2007.

³⁰ Cousineau, Ann. Director of Library Services, Solano County Library, personal communication with Kyle Simpson, DC&E, August 9, 2007.

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The proposed development will not result in the construction of a new library facility. The construction of the new Springstowne Library was identified separately as part of the Library Facilities Master Plan. As a result, a *less-than-significant* impact related to library services is anticipated.

- f. *No Impact.* Beyond the public services already examined within this analysis, no other impacts related to public services would occur.

14. RECREATION

- a. *No Impact.* The Project includes the creation of a 4.1-acre park that would be centrally located within the development. The park will be maintained by the Greater Vallejo Recreation District (GVRD). The public park will be accessible to residents of the proposed project as well as the surrounding community. The introduction of approximately 616 residents to eastern Vallejo will increase local park usage; however, the park included as part of the proposed project will offset local usage and would minimize deterioration of other parks in the City of Vallejo. As a result, a *less-than-significant* impact would occur.³¹
- b. *Less than Significant.* Portions of the project site are currently used for little league baseball and operated by East Vallejo Little League (EVLL). The little league fields are owned by Vallejo City Unified School District (VCUSD), but no lease agreement currently exists between EVLL and the school district.

The school district will discontinue little league activities at the current site in fall/winter of 2007. In late 2007, one or possibly two lit-

³¹ Average household size calculated from ABAG's Projections 2007 for the city of Vallejo. Household population of 128,000 / 44,370 households = 2.88 persons per household. Project population assumes an average household size of 2.88 persons x 214 new dwelling units = 616 people.

tle league fields will be constructed at Springstowne Middle School in Vallejo to accommodate EVLL and to maintain the current level of little league baseball activities. One additional tee ball field will be built at Pennycook Elementary in Vallejo. Upon completion of the little league fields, EVLL will use the fields under a two-year lease agreement with VCUSD.³² Recreational use at the middle and elementary schools will increase as a result of the construction of the little league fields.

The movement of the little league fields is not occurring as a result of the project and any potential environmental impacts related to the construction of the new little league fields would be examined through a separate environmental review processes. As a result, the proposed project will not create significant environmental impacts related to the construction of new recreational facilities. A *less-than-significant* impact would occur.

15. TRAFFIC/TRANSPORTATION

a. *Less than Significant.*

Study Scope and Approach

This analysis evaluates the traffic-related impacts of the proposed development project at eight key intersections in the study area for the following four scenarios:

- ◆ Existing Conditions;
- ◆ Existing plus Project Conditions;
- ◆ Cumulative (2020) Conditions; and
- ◆ Cumulative (2020) plus Project Conditions.

³² Gonella, Floyd. Advisor to the State Administrator, Vallejo City Unified School District, personal communication with Kyle Simpson, DC&E. August 14, 2007.

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The Cumulative scenario represents traffic conditions projected to occur in the year 2020. Cumulative traffic volumes have been developed based on forecasts from the City of Vallejo demand forecast model, which represents conditions anticipated to occur with buildout of the City's General Plan.

Trip generation rates for the proposed project were calculated using published Institute of Transportation Engineers (ITE) trip generation rates taken from the Trip Generation Manual, Seventh Edition.

The traffic analysis focuses on the following eight study intersections:

1. Rollingwood Drive / Keats Drive;
2. Rollingwood Drive / Dryden Drive;
3. Rollingwood Drive / Glen Cove Road / Benicia Road;
4. Glen Cove Road / I-780 Westbound Ramps;
5. Glen Cove Road / I-780 Eastbound Ramps;
6. Columbus Parkway / Benicia Road;
7. Street D / Benicia Road (study site access intersection); and,
8. Rollingwood Drive / Street G (study site access intersection).

Figure 1 in Appendix A shows the location of the project site and Figure 2, also in Appendix A, shows the study intersections (six existing, two proposed). Figure 3, located in Appendix A, shows the project's site layout.

Roadway Network

General vehicle access to the proposed project would be provided via Rollingwood Drive to the east and Benicia Road to the south. An emergency vehicle access would also be provided via Hargus Avenue to the west. Access to these three local roadways would be provided by four roadways to be constructed with the development. Street G is planned to intersect Rollingwood Drive, while Street D, the main

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north-south roadway within the development, is planned to intersect Benicia Road. Street A, at the northernmost section of the proposed project, is planned to intersect Rollingwood Drive and Pope Drive. An emergency vehicle access (EVA), via Hargus Avenue, is to be extended to the east to intersect Court I.

Interstate 780 (I-780)

Interstate 780 is a major east-west interstate highway connecting Interstate 80 (I-80) in Vallejo to Interstate 680 (I-680) in Benicia, with an interchange at Glen Cove Road. In the vicinity of the project site, I-780 is comprised of four lanes, with average daily traffic on this stretch of freeway approximately 64,000 vehicles.³³

Interstate 80 (I-80)

Interstate 80 is a major north-south interstate highway through the San Francisco Bay Area extending west from San Francisco via the San Francisco-Oakland Bay Bridge through Berkeley, Vallejo, and Sacramento, into Nevada. Three lanes are provided in each direction in the vicinity of the project site, with average daily traffic on this stretch of freeway of approximately 142,000 vehicles.³⁴ Access to I-80 from the project site is provided by I-780.

State Route 29 (SR 29)

State Route 29 is a north-south highway connecting Interstate 80 (I-80) in Vallejo with State Route 20 northwest of Clear Lake. It connects Vallejo with Napa and serves as a major arterial through the Napa Valley. In Vallejo, it runs on Sonoma Boulevard and operates as a four-lane divided roadway in the vicinity of the project site. Average daily traffic on this stretch of freeway is approximately 15,100

³³ 2006 *Traffic Volumes on the California State Highway System*, Department of Transportation, State of California.

³⁴ 2006 *Traffic Volumes on the California State Highway System*, Department of Transportation, State of California.

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vehicles.³⁵ Access to SR 29 from the project site is provided by I-780, which turns into Curtola Parkway and intersects Sonoma Boulevard.

State Route 37 (SR 37)

State Route 37 is an east-west freeway extending from I-80 in the east to U.S. Highway 101 (U.S. 101) in Novato in the west. SR 37 is a four-lane freeway in Vallejo and connects Vallejo with points in the North Bay. Primary access to State Route 37 is via I-780, Curtola Parkway, Mare Island Way and Wilson Avenue. Average daily traffic on this stretch of freeway is 35,000 vehicles.³⁶

Glen Cove Road

Glen Cove Road is a major north-south roadway extending from Magazine Street to Benicia Road where it becomes Rollingwood Drive. Glen Cove Road consists of four lanes with a two-way left turn lane from Benicia Road to Warren Avenue which is located just north of the I-780 interchange. Through the interchange, Glen Cove Road has a single through lane in each direction with a single left turn lane in the southbound direction and two left turn lanes in the northbound direction. Just south of the I-780 interchange, Glen Cove Road consists of four lanes until Carrot Lane where it splits off from Glen Cove Parkway, a four-lane principal artery that provides access to the Glen Cove subdivision, and becomes a single lane, one-way residential roadway until Belmont Avenue. South of Belmont Avenue, Glen Cove Road is a two-lane residential roadway.

Benicia Road

Benicia Road is a two-lane east-west collector roadway running from Columbus Parkway to Solano Avenue.

³⁵ 2006 *Traffic Volumes on the California State Highway System*, Department of Transportation, State of California.

³⁶ 2006 *Traffic Volumes on the California State Highway System*, Department of Transportation, State of California.

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Columbus Parkway

Columbus Parkway is a major north-south roadway currently consisting of two lanes in the vicinity of the project between Springs Road and Benicia Road, and four lanes from Springs Road north to the I-80 interchange. The remaining two-lane section is scheduled to be widened in conjunction with the Bordoni Residential Development approved on the east side of Columbus Parkway between Regents Park Drive and Benicia Road. Columbus Parkway runs from an I-780 interchange near the Vallejo city limits to an I-80 interchange in northern Vallejo.

Rollingwood Drive

Rollingwood Drive is a two-lane, north-south residential roadway, extending from Benicia Road to Tennessee Street. Rollingwood Drive becomes Glen Cove Road south of the Benicia Road intersection.

Dryden Drive

Dryden Drive is a two-lane, east-west residential roadway, extending from Rollingwood Drive to Brooke Drive.

Keats Drive

Keats Drive is a two-way residential roadway, extending from Rollingwood Drive to Georgia Street.

Data Collection

Traffic counts were conducted at each of the study intersections during the morning (7-9 AM) and evening (4-6 PM) peak periods on Wednesday, July 11, 2007. It should be noted that these traffic counts were conducted while existing schools in the area were not in session. Trip generation for the five schools in the vicinity of the project was manually calculated and added to the traffic network to provide an accurate depiction of worst-case traffic conditions in the area. The distribution of the school trips was determined based on the City of Vallejo's travel demand model. Located in Appendix A,

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Figure A shows the trip distribution for the schools located within the project vicinity. The trip generation for the five schools is included in the Technical Appendix and is available for review at the Vallejo Planning Division. The driving distance of the five schools from the project site and the number of students that attend each school (while in session) is shown below:

◆ St. Patrick-St. Vincent High School	0.5 mi	663 students
◆ Hogan High School	1.3 mi	1,835 students
◆ Springstowne Junior High School	1.6 mi	1,024 students
◆ Annie Pennycook Elementary School	0.8 mi	604 students
◆ Glen Cove Elementary School	1.2 mi	585 students

Located in Appendix A, Figure 4 shows the existing traffic volumes (including forecast school related traffic) and Figure 5 (located in Appendix A) shows the existing intersection geometry and traffic control at each of the study intersections.

Public Transit

The project area is directly served by two Vallejo Transit (VT) routes: the 3 and 6. Route 6 is a loop route, with a terminal at the York & Marin Transfer Center, which offers connections to the other VT routes. It runs hourly, from 5:30 AM to 7:30 PM on weekdays and from 7:00 AM to 7:30 PM on weekends, with a stop at Benicia Road and Rollingwood Drive. Route 3 runs only three trips between the York & Marin Transfer Center and Glen Cove Road—one AM trip and two PM trips, passing by the project site along Rollingwood Drive. Figure 6, located in Appendix A, displays the existing transit service within the vicinity of the project site.

Although not within walking distance of the project site, regional transit service in and out of Vallejo is provided by the following services: VT Routes 80 to the El Cerrito del Norte BART station and 85 to Fairfield and Solano Mall; the Baylink Ferry and Baylink Express Route 200 to San Francisco; Benicia Breeze Route 75 to Benicia

and the Pleasant Hill BART station; the Napa VINE Routes 10S and 10N to Calistoga; and American Canyon Transit.

Bicycle and Pedestrian Facilities

Currently, bicycles on the road must share the road with vehicular traffic as there are no existing bike lanes on most roadways. However, Glen Cove Road between Benicia Road and Fulton Avenue offers sufficient shoulder width for bicycles, and Columbus Parkway includes a Class II bicycle lane. Benicia Road also offers sufficient shoulder width on some segments that can accommodate bicyclists. According to the Solano Countywide Bicycle Plan, issued in October 2004, Class II bike lanes are under consideration for both Benicia Road and Georgia Street. Figure 7, located in Appendix A, shows the existing and proposed bicycle network in the vicinity of the project site.

Within the project site, the roadways are expected to operate as low-speed, low-volume facilities. Bike lanes would not be provided, however, the residential roadways are expected to adequately accommodate bicyclists. Pedestrian sidewalks are proposed on all streets and cul-de-sacs within the project site. Pedestrian sidewalks are not proposed on the alley roadways. However, landscaped pedestrian walkways are proposed between alley home units. These walkways would connect to the proposed sidewalks along the Benicia Road and Rollingwood Drive frontages.

Currently, there are no sidewalks directly adjacent to the project site. Sidewalks exist along the east side of Rollingwood Drive across the project site and on Benicia Road east of the Rollingwood Drive / Glen Cove Road intersection, on both sides of the roadway. There are also sidewalks on Glen Cove Road south of the Benicia Road / Rollingwood Drive intersection and on Rollingwood Drive in both directions north of Pope Drive. Most local roadways north and east of the project site also have sidewalks. Currently, the only marked

crosswalks in the vicinity of the project site are at the Benicia Road / Glen Cove Road intersection, on the south and east legs. There is an additional marked crosswalk across Benicia Road at Banning Way / Taylor Avenue.

Pedestrians and bicyclists would have access to the Glen Cove Commercial Center (south of the project site at the intersection of Glen Cove Road and Robles Way) via the aforementioned pedestrian and bicyclist facilities. Pedestrians and bicyclists would not have convenient access to the Rose Drive Center (east of the project site at the intersection of Rose Drive and Columbus Parkway) via the existing facilities. Currently, accessing the Rose Drive Center would be difficult for project-based pedestrians and bicyclists as Benicia Road is a two-lane road with low shoulders and no sidewalks for a significant portion of the route.

Analysis Methodology

Traffic conditions in the study area are assessed through the evaluation of peak hour Levels of Service (LOS) at critical intersections. The LOS concept qualitatively characterizes traffic conditions associated with varying levels of traffic. An LOS determination is a measure of congestion, which is the principal measure of roadway service. Levels of Service criteria for signalized intersections are illustrated in Table 4-3. These range from LOS A which indicates a free-flow condition to LOS F which indicates a jammed condition. LOS A, B and C are generally considered to be satisfactory service levels while LOS D is marginally acceptable, LOS E is undesirable, and LOS F is unacceptable.

Traffic conditions at study intersections are evaluated for morning and evening peak hours using the methodology of the Transportation Research Board's 2000 Highway Capacity Manual. With this methodology, a level of service is assigned based on average vehicle delay. For signalized and all-way stop intersections, delay is

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TABLE 4-3 LEVEL OF SERVICE METHODOLOGY

LOS	Description	Signalized Intersections (sec/veh)	Unsignalized Intersections (sec/veh)
A	Little or no delay	≤ 10.0	≤ 10.0
B	Short traffic delay	> 10.0 and ≤ 20.0	> 10.0 and ≤ 15.0
C	Average traffic delay	> 20.0 and ≤ 35.0	> 15.0 and ≤ 25.0
D	Long traffic delay	> 35.0 and ≤ 55.0	> 25.0 and ≤ 35.0
E	Very long traffic delay	> 55.0 and ≤ 80.0	> 35.0 and ≤ 50.0
F	Extreme traffic delay	> 80.0	> 50.0

Source: Highway Capacity Manual, Transportation Research Board, 2000.

presented in terms of average controlled delay for all movements. For side-street stop-controlled intersections, delay is presented for the worst case stop-controlled approach.

Intersection Operations

Table 4-4 shows the Existing Conditions for the eight study intersections. Each of the study intersections currently operate at LOS C or better during the AM and PM peak hours.

Standards of Significance

A project would normally have a significant effect on the environment if it would cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., results in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads or congestion at intersections), or change the condition of an existing street (i.e., street closures, changing direction of travel) in a manner that would substantially affect access or traffic load and capacity of the street system.

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TABLE 4-4 **INTERSECTION LOS SUMMARY – EXISTING CONDITIONS**

#	Intersection	Traffic Control	Existing Conditions		
			Peak Hour	LOS	Delay (sec/veh)
1	Rollingwood Drive / Keats Drive	1-Way Stop	AM	B	13.2
			PM	C	16.2
2	Rollingwood Drive / Dryden Drive	1-Way Stop	AM	B	11.0
			PM	B	11.6
3	Rollingwood Drive / Benicia Road	Signal	AM	B	13.2
			PM	B	17.8
4	Glen Cove Road / I-780 WB Ramps	Signal	AM	B	12.0
			PM	B	15.7
5	Glen Cove Road / I-780 EB Ramps	Signal	AM	C	26.4
			PM	B	17.5
6	Columbus Parkway / Benicia Road	1-Way Stop	AM	C	20.3
			PM	C	19.7
7	Street B ^a / Benicia Road	1-Way Stop	AM	-	-
			PM	-	-
8	Rollingwood Drive / Street C ^a	1-Way Stop	AM	-	-
			PM	-	-

^a New streets within the proposed project

Source: DMJM Harris, 2007.

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The City of Vallejo's specific criteria utilized for this analysis are listed below:

- ◆ Impact Significance Criterion #1: At a study intersection the project would cause the existing baseline level of service to degrade to worse than LOS D.
- ◆ Impact Significance Criterion #2: At a study intersection where the baseline level of service is worse than LOS D, the project would cause the total intersection delay to increase by five or more seconds, or the project would cause the intersection's volume to capacity ratio to increase by five or more percent.
- ◆ Impact Significance Criterion #3: As a transportation facility on the Metropolitan Transportation System (MTS) roadway network, the project would cause the level of service to degrade below the adopted CMP standard of LOS E. In the study area the MTS network includes SR 29 and SR 37.
- ◆ Impact Significance Criterion #4: Result in projected parking demand that would exceed the proposed parking supply on a regular and frequent basis.
- ◆ Impact Significance Criterion #5: Result in potential conflicts for pedestrians or bicyclists, or fail to provide adequate bicycle and pedestrian access.
- ◆ Impact Significance Criterion #6: Increase transit demand above the levels able to be adequately provided by local transit operators or agencies.
- ◆ Impact Significance Criterion #7: Create direct transportation or circulation impacts associated with inconsistencies with General Plan policies.

Trip Generation

The project proposes to build 214 housing units and 657 parking spaces (428 covered spaces and 229 on-street spaces) on 32 acres in the City of Vallejo. The number of vehicle trips that would be gener-

ated by the proposed project was estimated through a trip generation analysis. Trip generation rates and inbound/outbound splits for the land use under consideration were taken from the Institute of Transportation Engineers (ITE) Trip Generation Manual, Seventh Edition. Table 4-5 summarizes the vehicle trip generation characteristics of the proposed project.

All of the dwelling units within the proposed project site were analyzed as Single Family Detached Housing (ITE LU 210). This land use was utilized because it yields the highest trip generation of all applicable housing types and is the most conservative. Where applicable, the trip generation results were developed utilizing the fitted curve equation. If the fitted curve equation was not applicable, the average rate was applied.

The park was to be analyzed using the City Park land use (ITE LU 411). However, insufficient data was available to use the City Park land use trip generation rate. In lieu of the City Park land use, the County Park land use (ITE LU 412) was applied. The average rate was applied as the fitted curve equation was not available due to insufficient data.

Trip Distribution

In the Existing plus Project scenario, traffic forecast to be generated by the proposed project was added to existing traffic levels. Project traffic was assigned to the local roadway network based on travel patterns developed using the City of Vallejo's current travel demand model. Figure 8 (located in Appendix A) summarizes the assignment used to distribute project-generated vehicles.

Existing plus Project Conditions

Figure 9, in Appendix A, shows the project traffic volumes. Table 4-6 presents the results of the Existing and Existing plus Project analyses at each of the study intersections. Level of Service

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TABLE 4-5 PROJECT TRIP GENERATION SUMMARY

Land Use	Size	Time Pe- riod	Generated Trips		
			In	Out	Total
Residential ^a	214 DU	AM Peak Hour	40	119	159
		PM Peak Hour	134	79	213
		Daily	1,047	1,047	2,094
Park ^b	4.1 acres	AM Peak Hour	0	0	0
		PM Peak Hour	0	0	0
		Daily	5	5	10
Total		AM Peak Hour	40	119	159
		PM Peak Hour	134	79	213
		Daily	1,052	1,052	2,104

^a ITE Land Use Code 210 – Single Family Detached Housing

^b ITE Land Use Code 412 – County Park

Notes:

No peak hour information is provided for the City Park land use

Average rate used for the City Park land use as insufficient data provided for fitted curve equation

ITE Land Use Code 210 – Daily Equation: $\ln(T) = 0.92 \ln(X) + 2.71$, where X = dwelling units

ITE Land Use Code 210 – AM Peak Hour Equation: $T = 0.70(X) + 9.43$, where X = dwelling units

ITE Land Use Code 210 – PM Peak Hour Equation: $\ln(T) = 0.90 \ln(X) + 0.53$, where X = dwelling units

ITE Land Use Code 412 – Daily Equation: $T = 2.28(X)$, where X = acres

ITE Land Use Code 412 – AM Peak Hour Equation: $T = 0.01(X)$, where X = acres

ITE Land Use Code 412 – PM Peak Hour Equation: $T = 0.06(X)$, where X = acres

Source: DMJM Harris, 2007; ITE Trip Generation Manual, Seventh Edition.

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TABLE 4-6 **INTERSECTION LOS SUMMARY – EXISTING PLUS PROJECT CONDITIONS**

#	Intersection	Traffic Control	Peak Hour	Existing Conditions		Existing plus Project Conditions	
				LOS	Delay (sec/veh)	LOS	Delay (sec/veh)
1	Rollingwood Drive / Keats Drive	1-Way Stop	AM	B	13.2	B	13.4
			PM	C	16.2	C	16.5
2	Rollingwood Drive / Dryden Drive	1-Way Stop	AM	B	11.0	B	11.0
			PM	B	11.6	B	11.7
3	Rollingwood Drive / Benicia Road	Signal	AM	B	13.2	B	14.1
			PM	B	17.8	C	20.6
4	Glen Cove Road / I-780 WB Ramps	Signal	AM	B	12.0	B	12.1
			PM	B	15.7	B	15.7
5	Glen Cove Road / I-780 EB Ramps	Signal	AM	C	26.4	C	27.4
			PM	B	17.5	B	19.1
6	Columbus Parkway / Benicia Road	1-Way Stop	AM	C	20.3	C	20.5
			PM	C	19.7	C	20.2
7	Street B ^a / Benicia Road	1-Way Stop	AM	-	-	B	11.3
			PM	-	-	B	10.9
8	Rollingwood Drive / Street C ^a	1-Way Stop	AM	-	-	B	11.5
			PM	-	-	B	12.3

^a New streets within the proposed project.

Source: DMJM Harris, 2007.

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calculation worksheets for all scenarios are located in the Technical Appendix, available for review at the Vallejo Planning Division.

Street A is expected to be utilized by project trips. Although project trips from this roadway are not shown in Figure 9 (Appendix A), traffic volumes were assigned. During the AM peak hour, 127 project trips (32 inbound, 95 outbound) are expected to utilize Street D and Street G. The remaining 32 trips, or 20%, are expected to utilize Street A. During the PM peak hour, 172 project trips (108 inbound, 64 outbound) are expected to utilize Street D and Street G. The remaining 41 trips, or 20 percent, are expected to utilize Street A.

As proposed, the project site is to provide emergency vehicle access via Hargus Avenue. General traffic would not be permitted to utilize this access point. However, if full vehicle access was permitted on Hargus Avenue, project trips would likely utilize the access point.

Based on the expected trip generation and distribution, approximately 29 trips (7 inbound, 22 outbound) would utilize Hargus Avenue during the AM peak hour and 39 trips (24 inbound, 15 outbound) would utilize Hargus Avenue during the PM peak hour. Approximately 380 daily trips (190 inbound, 190 outbound) would utilize Hargus Avenue on a weekday. Although the City of Vallejo has no significance standards regarding traffic congestion on a street of this size and capacity, the street could accommodate increased trips, and the level of service on Hargus Avenue would not fail as a result of increased traffic congestion. A *less-than-significant* impact would occur.

- b. *Less than Significant.* The Cumulative scenario represents traffic forecasts for the year 2020, and includes the anticipated growth in the City of Vallejo consistent with the buildout of the General Plan.

The Cumulative baseline traffic volumes were developed based on demand model forecasts by the City of Vallejo, which estimate a yearly growth rate in traffic generally between 2 to 3 percent per year in the study area. Traffic volumes for the Cumulative base scenario are shown in Appendix A, Figure 10.

Traffic operations at the study intersections were assessed during the weekday morning and evening peak hours of travel. Table 4-7 summarizes the traffic operations at each of the study intersections under the Cumulative scenario without the project. As shown in Table 4-7, all eight intersections would continue to operate at acceptable levels of service (LOS D or better) in the Cumulative scenario during the AM and PM peak hours.

Traffic volumes from the proposed project were added to the Cumulative scenario to determine Cumulative traffic impacts that may result from the proposed project. Table 4-8 summarizes the traffic operations at the eight study intersections under the Cumulative plus Project scenario. Detailed LOS calculations worksheets are available for review at the Vallejo Planning Division.

All eight intersections continue to operate at an acceptable level of service (LOS D or better) with the addition of the proposed project traffic during the AM and PM peak hours. Based on City standards, the increased delay as a result of the project is not considered a significant impact. A *less-than-significant* impact would occur.

c. *Potentially Significant Unless Mitigation Incorporated.*

Internal Site Access

Access to the proposed project would be provided by four new roadways; two with access to Rollingwood Drive, one with access to Benicia Road, and one with access to Hargus Avenue (for emergency vehicles only). The new intersections would be one-way stop controlled coming out of the project site. As shown on the site plan

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TABLE 4-7 INTERSECTION LOS SUMMARY –CUMULATIVE CONDITIONS

#	Intersection	Traffic Control	Peak Hour	Cumulative Conditions	
				LOS	Delay (sec/veh)
1	Rollingwood Drive / Keats Drive	1-Way Stop	AM	C	17.0
			PM	C	23.9
2	Rollingwood Drive / Dryden Drive	1-Way Stop	AM	B	12.6
			PM	B	13.8
3	Rollingwood Drive / Benicia Road	Signal	AM	B	14.2
			PM	C	33.6
4	Glen Cove Road / I-780 WB Ramps	Signal	AM	B	13.7
			PM	B	19.1
5	Glen Cove Road / I-780 EB Ramps	Signal	AM	C	32.0
			PM	B	19.9
6	Columbus Parkway / Benicia Road	1-Way Stop	AM	D	33.8
			PM	D	32.6
7	Street B ^a / Benicia Road	1-Way Stop	AM	-	-
			PM	-	-
8	Rollingwood Drive / Street C ^a	1-Way Stop	AM	-	-
			PM	-	-

^aNew streets within the proposed project.
 Source: DMJM Harris, 2007.

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TABLE 4-8 INTERSECTION LOS SUMMARY – CUMULATIVE PLUS PROJECT CONDITIONS

#	Intersection	Traffic Control	Peak Hour	Cumulative Conditions		Cumulative plus Project Conditions	
				LOS	Delay (sec/veh)	LOS	Delay (sec/veh)
1	Rollingwood Drive / Keats Drive	1-Way Stop	AM	C	17.0	C	17.3
			PM	C	23.9	C	24.5
2	Rollingwood Drive / Dryden Drive	1-Way Stop	AM	B	12.6	B	12.7
			PM	B	13.8	B	14.0
3	Rollingwood Drive / Benicia Road	Signal	AM	B	14.2	B	15.7
			PM	C	33.6	D	35.9
4	Glen Cove Road / I-780 WB Ramps	Signal	AM	B	13.7	B	14.2
			PM	B	19.1	B	19.7
5	Glen Cove Road / I-780 EB Ramps	Signal	AM	C	32.0	C	33.4
			PM	B	19.9	C	21.8
6	Columbus Parkway / Benicia Road	1-Way Stop	AM	D	33.8	D	34.5
			PM	D	32.6	D	34.2
7	Street B ^a / Benicia Road	1-Way Stop	AM	-	-	B	12.2
			PM	-	-	B	11.5
8	Rollingwood Drive / Street C ^a	1-Way Stop	AM	-	-	B	13.2
			PM	-	-	B	14.9

Source: DMJM Harris, 2007.

^aNew streets within the proposed project.

(Figure 3 of Appendix A), the roadways providing access to Rollingwood Drive and Benicia Road are tentatively titled Street A, Street D, and Street G. These three roadways are all planned to be 36 feet wide. The Hargus Avenue extension is planned to be 20 feet wide.

At the intersections with Rollingwood Drive and Benicia Road, each stop controlled approach would accommodate a right turn and a left turn movement for exiting vehicles. Internal circulation would be provided by six new two-way roadways within the project site. Sidewalks are to be provided throughout the proposed project site. Safety concerns were not identified at any of these locations.

The roadways within the northern portion of the proposed project are planned to be 36 feet wide. The three cul-de-sacs within this section of the proposed project are designed with sufficient space for vehicles to turn around. The roadway design would adequately accommodate all vehicle types – including vehicles without reversible operations, such as garbage trucks and emergency vehicles.

The roadways within the southern portion of the proposed project are planned to range from 22 to 24 feet wide. The minimum roadway cross section width within the City of Vallejo is 20 feet per the City of Vallejo emergency access protocols. Six of the roadway segments (ranging from 100-200 feet long) within this section include designated areas for passenger vehicles to turn around. However, garbage trucks and emergency vehicles would be unable to access these locations due to insufficient space for such maneuvers. Implementation of Mitigation Measure TRAF-1 will reduce this impact to a less-than-significant level.

External Site Access

Near the project site, Rollingwood Drive is a two-way, 30-foot wide street with no center median and minimal shoulders. The project proposes to construct two full movement access points intersecting

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with Rollingwood Drive. Vehicles entering the project site from Rollingwood Drive would utilize shared through-left and shared through-right turn lanes. Project-related traffic traveling northbound on Rollingwood Drive attempting to turn left onto the project streets (Street A and Street G) would delay northbound vehicles. Northbound vehicles, traveling past the project site, would be unable to pass on the right side due to the narrow roadways and minimal shoulders.

The northbound left turn movement at the Rollingwood Drive / Street G intersection would serve fewer than 50 left turning vehicles per hour resulting in no more than nine seconds of delay to northbound vehicles in the Cumulative plus Project Conditions scenario. The northbound movement is expected to operate at LOS A. The intersection is expected to operate at LOS B. Fewer left turning vehicles and lower movement delays are expected at the Rollingwood Drive / Street A. These delays are insignificant and would not warrant left turn pockets on Rollingwood Drive. Queuing at these intersections is expected to be no more than one vehicle and the intersections are not expected to be unsafe for motorists.

A cemetery is located on the eastern side of Rollingwood Drive. The cemetery access point is a one way stop controlled intersection located between the proposed Rollingwood Drive / Street "A" / Pope Drive intersection (to the north) and the proposed Rollingwood Drive / Street "G" intersection (to the south). The cemetery generates varied traffic demands based on scheduled events. Although this demand would increase delays on Rollingwood Drive, cemetery-generated traffic is unrelated to the proposed project and would not require any mitigation by the project sponsor.

Near the project site, Benicia Road is a two-way, 40-foot wide street with shoulders and no center median. The project proposes to construct one access point intersecting with Benicia Road approximately

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550 feet west of Rollingwood Drive. Vehicles entering the project site from Rollingwood Drive or Benicia Road would utilize shared through-left and shared through-right turn lanes. Project-related traffic traveling eastbound on Benicia Road attempting to turn left onto Street D would delay eastbound vehicles. Eastbound vehicles, traveling past the project site, would be able to pass on the right side as the shoulder, without a sidewalk, is of adequate width to accommodate a passing vehicle. When a pedestrian is present, passing vehicles will not be able to utilize the shoulder and must wait for the turning vehicle to complete its turn onto Street D. Eastbound vehicles not able to use the shoulder will be delayed no more than eight seconds and Benicia Road will operate at LOS A.

The eastbound left turn movement at the Rollingwood Drive / Street D intersection would serve no more than ten left turning vehicles per hour resulting in less than eight seconds of delay to eastbound vehicles in the Cumulative plus Project Conditions. The eastbound movement is expected to operate at LOS A. The intersection is expected to operate at LOS B. Additionally, eastbound through vehicles would likely experience less delay as they would utilize the shoulder, without a sidewalk, to pass vehicles attempting to turn left. These delays are insignificant and would not warrant a left turn pocket on Benicia Road. Queuing at this intersection is expected to be no more than one vehicle and the intersection is not expected to be unsafe for motorists.

The proposed project access point on Benicia Road would be located approximately 550 feet west of the Rollingwood Drive / Benicia Road intersection. At the Rollingwood Drive / Benicia Road intersection, the eastbound left turn (EBL) pocket is approximately 200 feet long and the eastbound right turn (EBR) pocket is approximately 250 feet long.

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The longest eastbound queue would be 200 feet (EBR during the AM peak hour) in the Cumulative plus Project Conditions. The proposed project access point would be located 350 feet from the back of this queue. The proposed project access point is not expected to impact intersection operations at the Rollingwood Drive / Benicia Road intersection or create hazardous driving conditions. The proposed project access point is to be constructed at an acceptable location.

Currently, the southbound right turn pocket on Rollingwood Drive intersects Benicia Road at an acute angle. The relocation of this movement to form an orthogonal, right angle has been considered and evaluated. However, this modification has been discarded as the existing intersection configuration is operationally acceptable and not hazardous to motorists.

The proposed project is not expected to impact bus operations and would not alter any existing bus facility. Safety for motorists and transit riders would not be compromised with the implementation of the proposed project.

Sidewalks are provided near the proposed project site, and striped pedestrian crosswalks and pedestrian signals are provided at all signalized study intersections. Sidewalks are proposed along the Rollingwood Drive and Benicia Road frontages. Sidewalks would be constructed along these frontages to provide access to local pedestrian facilities. The sidewalk would be designed and constructed to meet City of Vallejo standards and tie in to the existing sidewalk facilities. In addition to sidewalks, the following improvements are required to meet City of Vallejo standards along Rollingwood Drive and Benicia Road, where applicable: road widening, grading, drainage, pavement, curb, gutter, street lights, the under-grounding of overhead utility wires, a bus bay and bus shelter. Signage to prohibit parking on Rollingwood Drive and Benicia Road must be installed, as well.

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Sight Distance

Requirements pertaining to the obstruction and visibility at roadway intersections are defined in the City of Vallejo Municipal Code 10.14.010. Based on the Code, no obstructions (e.g., buildings, landscaping, etc.) may exist within a 35 foot “triangle” – where the apex meets the intersection of the property lines nearest the intersection. All project roadways intersecting with Rollingwood Drive and Benicia Road would provide adequate sight distance for drivers given the speed of the roadways.

Mitigation Measure TRAF-1: Prior to project approval, the project applicant shall revise the site plan to include turnaround space at the ends of the alleys for garbage trucks and emergency vehicles. The project applicant shall coordinate with the City of Vallejo to determine the specific requirements for these turnarounds.

Significance after Mitigation: Less than significant.

- d. *No Impact.* The City of Vallejo emergency access protocols are based on the guidelines outlined within the Uniform Fire Code and the 2001 California Fire Code. All units within the project site should be designed in accordance with these codes. Emergency vehicles, such as fire trucks, would be able to make turns at all intersections within the proposed project site. No obstructions (e.g., gates) would prohibit emergency vehicles from entering the proposed project site from Rollingwood Drive or Benicia Road. The project sponsor should coordinate emergency vehicle access control and procedures at the Hargus Avenue access point.

As discussed in the site access section, several roadways within the proposed project do not provide an adequate turning radius for emergency vehicles. This could hinder emergency access to certain

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units within the site. Such roadways should be designed in accordance with the emergency access protocols defined by the City of Vallejo. All road widths are greater than 20 feet and would adequately accommodate emergency vehicles.

- e. *No Impact.* The project proposes two covered spaces per residential unit (428 spaces), plus an additional 229 uncovered spaces – 657 total parking spaces. The City of Vallejo Municipal Code 16.62.100 and 16.62.150 require at least 428 parking spaces for this project, 214 of which must be covered. The project proposes to supply enough parking to meet City requirements. The parking demand summary is shown in Table 4-9.

According to the ITE Parking Generation Manual, Third Edition, approximately 1.83 spaces per residential unit are needed to meet parking demand expected from a project similar to the proposed project. The project meets City of Vallejo requirements and national demand rates, and therefore is not anticipated to result in any significant parking related impacts.

The parking demand was determined by using the average rate for the Single Family Detached Housing (ITE LU 210) and City Park (ITE LU 411). The fitted curve equation was not utilized for either of these land uses as the sample sizes were unreliable.

The four roadways surrounding the park would provide 1,600 linear feet of on-street parking space. This on-street parking would accommodate approximately 64 vehicles. This would exceed the peak parking demand at the park.

Parking would not be required on Rollingwood Drive. The existing parking restrictions on Rollingwood Drive would not be changed by the proposed project. Parking within the proposed project site would adequately accommodate the parking demand.

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TABLE 4-9 **PARKING DEMAND SUMMARY**

Land Use	Rate	Demand
Residential ^a	1.83 spaces / DU	392 spaces
Park ^b	15 spaces / acre	62 spaces
Total		454 spaces

^a ITE Land Use Code 210 – Single Family Detached Housing

^b ITE Land Use Code 411 – City Park

Notes: Average rate used as insufficient data provided for fitted curve equation.

ITE Land Use Code 210 – Peak Period Equation: $P = 1.83(X)$, where X = dwelling units

ITE Land Use Code 411 – Peak Period Equation: $P = 15.00(X)$, where X = acres

Source: DMJM Harris, 2007; ITE Parking Generation Manual, Third Edition.

- f. *No Impact.* The proposed project would not conflict with any policies, plans or programs supporting alternative transportation. As a result, *no impact* would occur.

- g. *No Impact.* The closest airports are the Napa County Airport, approximately 13.4 miles from the site, Buchanan Field Airport, approximately 13.2 miles from the site, and the Oakland International Airport, approximately 30 miles to the south. As a result, the proposed project would not result in a change in air traffic patterns, air traffic levels or safety risks. *No impact* would occur.

16. UTILITIES AND SERVICES

- a. *Less than Significant.* The City of Vallejo water supply is secured through agreements with the State Water Project (SWP) and Solano County Water Project, as well as utilization of local lakes. Two separate water systems, including the City of Vallejo water system and the Lakes System serve the City of Vallejo. The water supply for the project site comes from the City of Vallejo water system.

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Water entering the City of Vallejo water system is processed at the Fleming Hill Water Treatment Plant. The WTP has a production capacity of 42 million gallons per day (MGD). In 2006, the City of Vallejo water system used a maximum of 31.5 MGD. Therefore, there was an excess capacity of 11.5 MGD in 2006.³⁷

Residential units in the City of Vallejo use an average demand of 261.8 gallons per day.³⁸ The addition of 214 residential units would add an average demand of approximately 56,025 gallons per day.³⁹ Based on the availability of 11.5 MGD in 2006, the project average demand for 56,025 gallons per day would result in a *less-than-significant* impact on water supply.

- b. *Less than Significant.* A fee would be assessed on the Project to be used by the City for the construction of an 18-inch water line from the Benicia/Rollingwood intersection to the Keats/Devlin intersection and/or the demolition of two existing 0.5 million gallon water tanks and the construction of a new approximately 2.5 million gallon water tank. The Project will also be required to install approximately 1,150 linear feet of a 12-inch water line along the project frontage on Benicia Road that will connect to an existing 12-inch water line ending near the western property line. The existing 6-inch line will be abandoned in place.

Roughly 350 linear feet of 12-inch water line will be installed along Hargus Avenue that will connect to the existing system. The existing 6-inch water line will be abandoned in place.

³⁷ Judy, Roger. Administration Analyst, City of Vallejo, Water Division. Personal communication with Kyle Simpson, DC&E, August 2, 2007.

³⁸ Judy, Roger. Administration Analyst, City of Vallejo, Water Division. Personal communication with Kyle Simpson, DC&E, August 2, 2007.

³⁹ 214 residential units x 261.8 gallons of water per day = 56,025.2 gallons of water per day.

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On-site improvements include the construction of a 12-inch water line from Hargus Avenue along the west and south side of the park that will connect to both Rollingwood Drive and Benicia Road. The remainder of the site will be served with 8-inch mains.

The construction and installation of the water mains identified above would require earthwork and grading activities that could lead to construction-period erosion. Soils that are currently covered by ruderal vegetation and asphalt paving will be excavated and exposed during construction. As discussed in the Geology and Soils section of this chapter, project impacts associated with construction-related erosion are considered to be potentially significant. In addition, erosion could result if drainage was concentrated from the development and allowed to erode through neighboring land. Siltation could result from loose disturbed soil being mobilized by storm water that could affect adjacent storm drains and waterways. Erosion and siltation are most likely to occur on steep slopes where the potential for erosion and sediment transport is highest; however, erosion could also occur in areas where soils are stockpiled.

As previously discussed in the Geology and Hydrology sections of this chapter, the applicant would be required to develop a Storm Water Pollution Prevention Plan (SWPPP), which would identify a series of Best Management Practices (BMPs); the BMPs would collectively reduce the potential impacts associated with erosion, as discussed above, to a *less-than-significant* level.

- c. - e. *Less than Significant.* The Project includes construction of an 8-inch main line from the project site to the existing Vallejo Sanitation and Flood Control District (VSFCD) wastewater collection system. The 8-inch main would connect to the existing system on the property of the Second Baptist Church, located at 1170 Benicia Road, via Hargus Avenue. The connection through Hargus Avenue would not result in significant environmental impacts, as construction would occur

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within the existing right of way where disturbance has already occurred.

Wastewater generated as a result of the project would flow to the current wastewater treatment plant, located at 450 Ryder Street in southwest Vallejo. The average dry weather wastewater flow into the wastewater treatment plant is 10 MGD.⁴⁰ The treatment plant has a permitted dry weather capacity of 15.5 million gallons per day (MGD) and a wet weather capacity of 60 MGD. An additional 30 MGD storage basin is located onsite to accommodate overflow.

The project will generate approximately 46,200 gallons of wastewater per day.⁴¹ The wastewater treatment plant has an excess capacity of 5.5 MGD and would be able to accommodate the wastewater generated by the project. As a result, the project would not necessitate the expansion of the treatment plant, construction of a new plant, or contribute to the exceedance of Regional Water Quality Control Board requirements. As a result, the project would result in a *less-than-significant* impact.

- f. *Less than Significant.* Approximately six acres of the project site currently drain to a swale that straddles the northern boundary of the project site. The swale drains to an existing storm drain system located north of the project site, on Dryden Drive. As a result of the

⁴⁰ Tafolla, Dan. Environmental Services Director, Vallejo Sanitation and Flood Control District, personal communication with Kyle Simpson, DC&E, July 31, 2007.

⁴¹ Silva, Jillian. Environmental Specialist, Vallejo Sanitation and Flood Control District, personal communication with Kyle Simpson, DC&E, August 17, 2007. VSFCDD estimates that Vallejo residents generate approximately 80 gallons of wastewater per day, and, on average, each residential unit is occupied by 2.7 residents. 2.7 people x 80 gallons = 216 gallons per household per day. The proposed project contains 214 residential units. 214 residential units x 216 gallons per day = 46,224 gallons of wastewater per day.

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on-site drainage plan, the Project would reduce the runoff area from six to two acres, thus reducing the volume of stormwater runoff that flows to the Dryden Drive system. Stormwater runoff throughout the remainder of the site will be collected in standard inlets and piped to the on-site park where it will be passed through the park landscaping for retention and treatment. After percolating through the park, stormwater will drain from the park to street-level drain inlets on a portion of the storm drain located within Benicia Road. Approximately 450-feet of the storm drain within Benicia Road will need to be replaced to achieve gravity flow. Replacement would take place within the right-of-way for Benicia Road. As a result, a *less-than-significant* impact would occur.

- g. *No Impact.* The City of Vallejo has an exclusive contract with Vallejo Garbage Service, Inc. for the collection and transportation of solid waste and recyclable material to the Devlin Road Transfer Station in American Canyon, CA. Waste from the Devlin Road facility is transported to Keller Canyon Landfill in Contra Costa County for ultimate disposal.⁴² Keller Canyon Landfill is permitted to receive up to 3,500 tons of waste per day and currently receives about 2,500 tons of waste per day. The California Integrated Waste Management Board lists the expected closure date of the landfill to be December 31, 2030. The landfill has a total capacity of 75 million cubic yards and a remaining capacity of over 68 million cubic yards. Therefore, the proposed project would be served by a landfill with sufficient permitted capacity to accommodate its solid waste disposal needs. As a result, *no impact* would occur.
- h. *Less than Significant.* The proposed project would comply with federal, State and local statutes and regulations related to solid waste and recycling. California's Integrated Waste Management Act of 1989 (AB 939) set a requirement for cities and counties throughout the

⁴² Trent Cave, Manager, Napa-Vallejo Waste Management Agency, personal conversation with Kyle Simpson, DC&E, on July 26, 2007.

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State to divert 50 percent of all solid waste from landfills by January 1, 2000, through source reduction, recycling and composting. To help achieve this, the Act requires that each city and county prepare and submit a Source Reduction and Recycling Element. AB 939 also establishes the goal for all California counties to provide at least 15 years of on-going landfill capacity. Furthermore, the City of Vallejo has adopted a construction and demolition (C&D) debris recycling ordinance to redirect C&D materials away from landfills.⁴³ The ordinance requires that at least 50 percent of the C&D debris generated from a project site be salvaged and/or recycled and at least 75 percent of concrete and asphalt generated from the project site be salvaged and/or recycled. In addition, the City of Vallejo requires that curbside recycling and yardwaste collection be provided for all residential subdivisions within the City's jurisdiction.⁴⁴

Compliance with the City's C&D ordinance during construction and the provision of curbside recycling and yardwaste collection service after construction will ensure that the project complies with local and state laws related to waste reduction. A *less-than-significant* impact would occur.

17. MANDATORY FINDINGS OF SIGNIFICANCE

- a. The proposed project was not found to have any biological or cultural resources-related impacts that could not be mitigated to a *less-than-significant level* through the implementation of mitigation measures.
- b. The project would not result in any potentially significant cumulative impacts.

⁴³ City of Vallejo Municipal Code, Chapter 7.53, Construction and Demolition Debris Recycling Ordinance.

⁴⁴ Derek Crutchfield, City of Vallejo Recycling Coordinator, personal communication with Ted Heyd, DC&E, August 24, 2007.

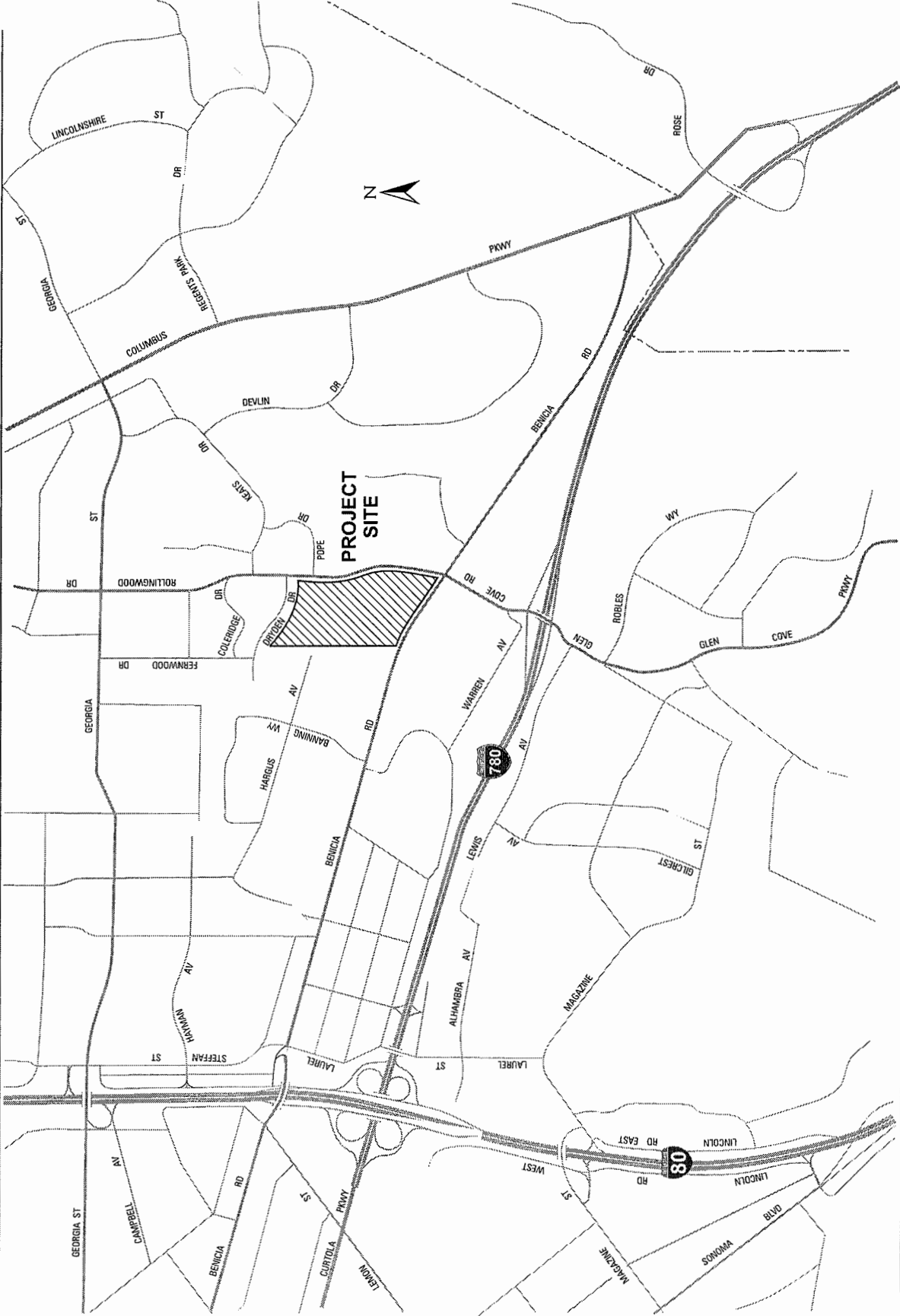
CITY OF VALLEJO
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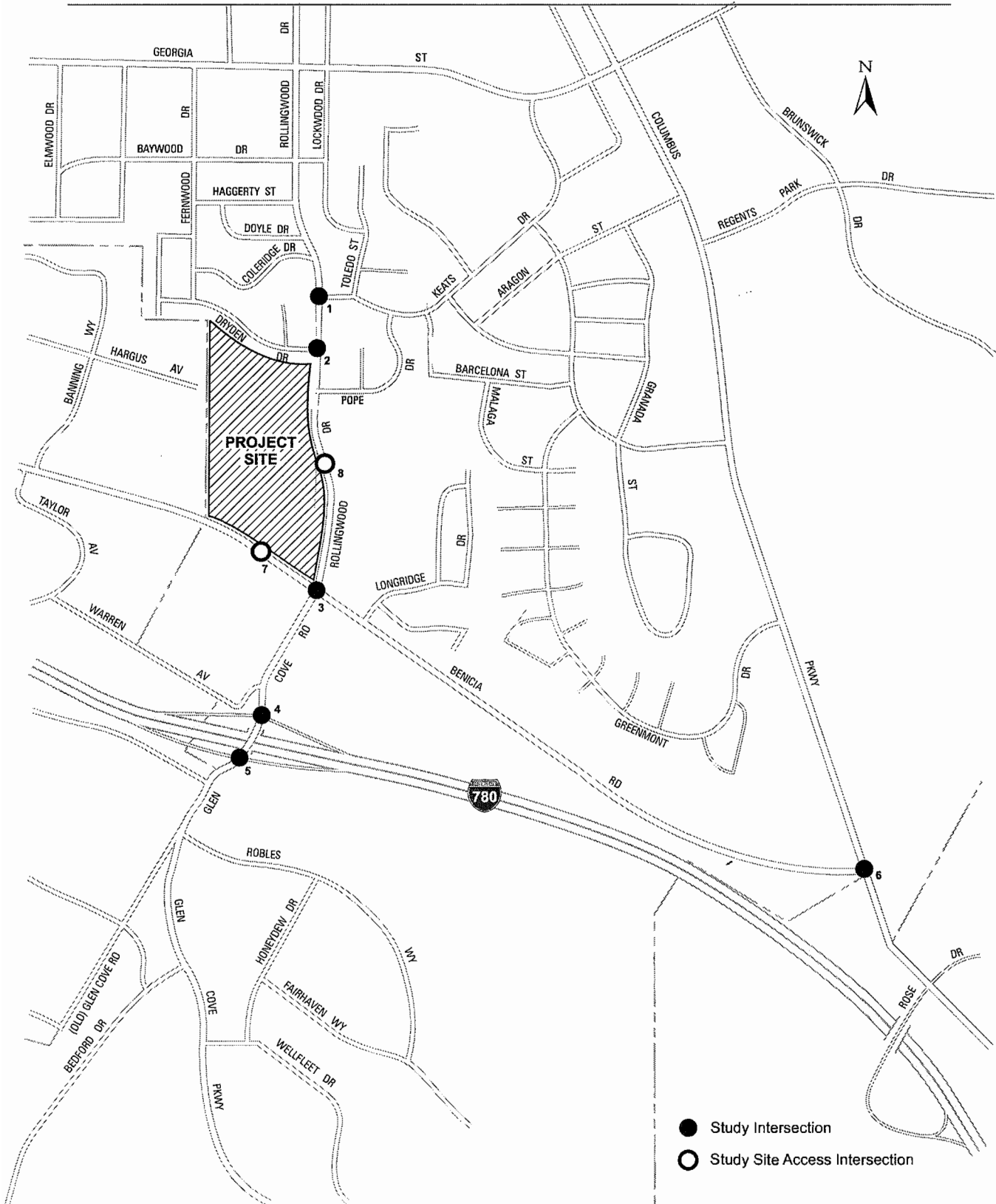
- c. The proposed project would not have any environmental effects which would cause substantial adverse effects on human beings, either directly or indirectly, that could not be mitigated to a less-than-significant level. As such, no significant impacts are anticipated.

A P P E N D I X A

TRAFFIC ANALYSIS
FIGURES



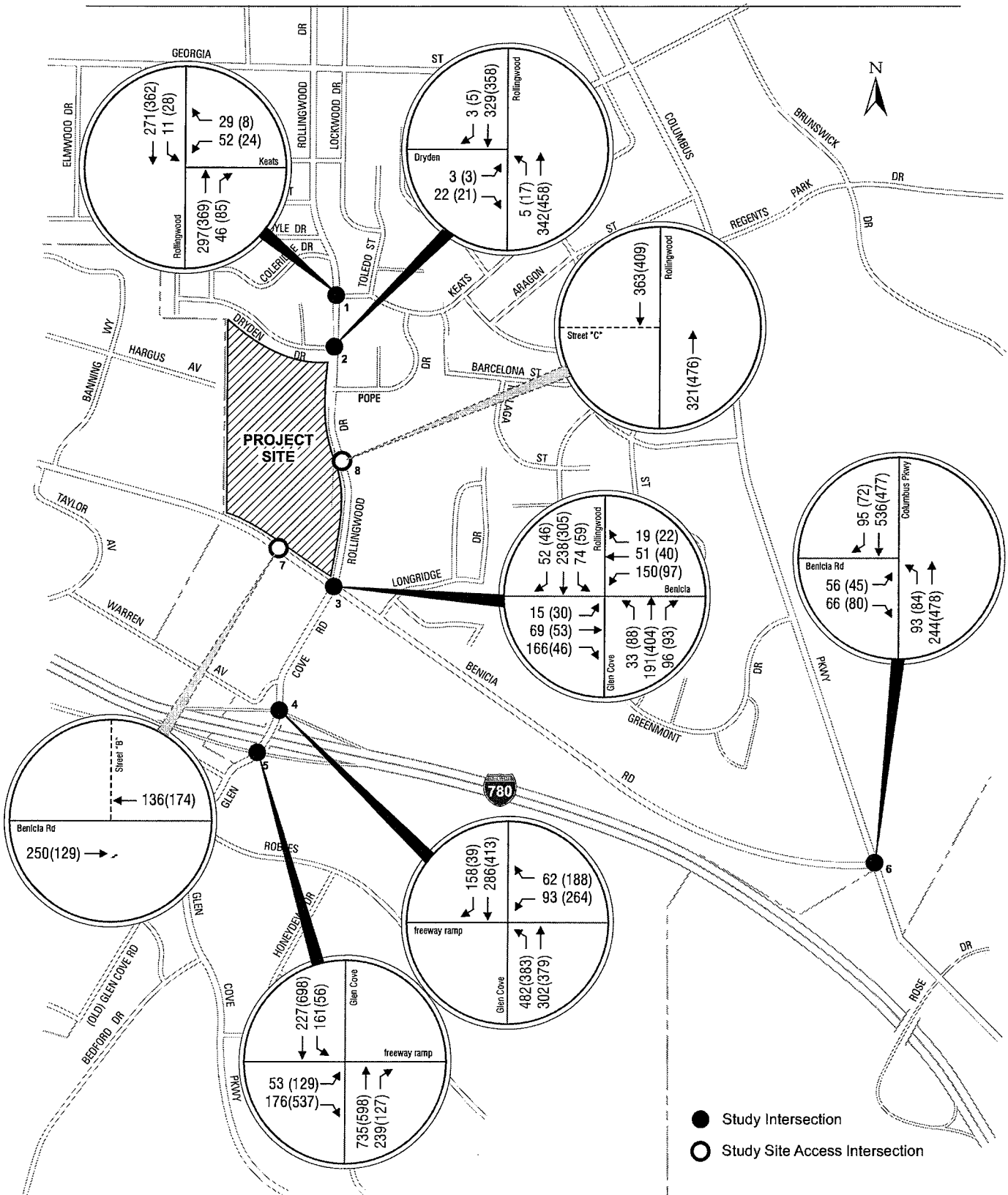




Study Intersections.cdr

ROLLINGWOOD RESIDENTIAL
Figure 2
STUDY INTERSECTIONS



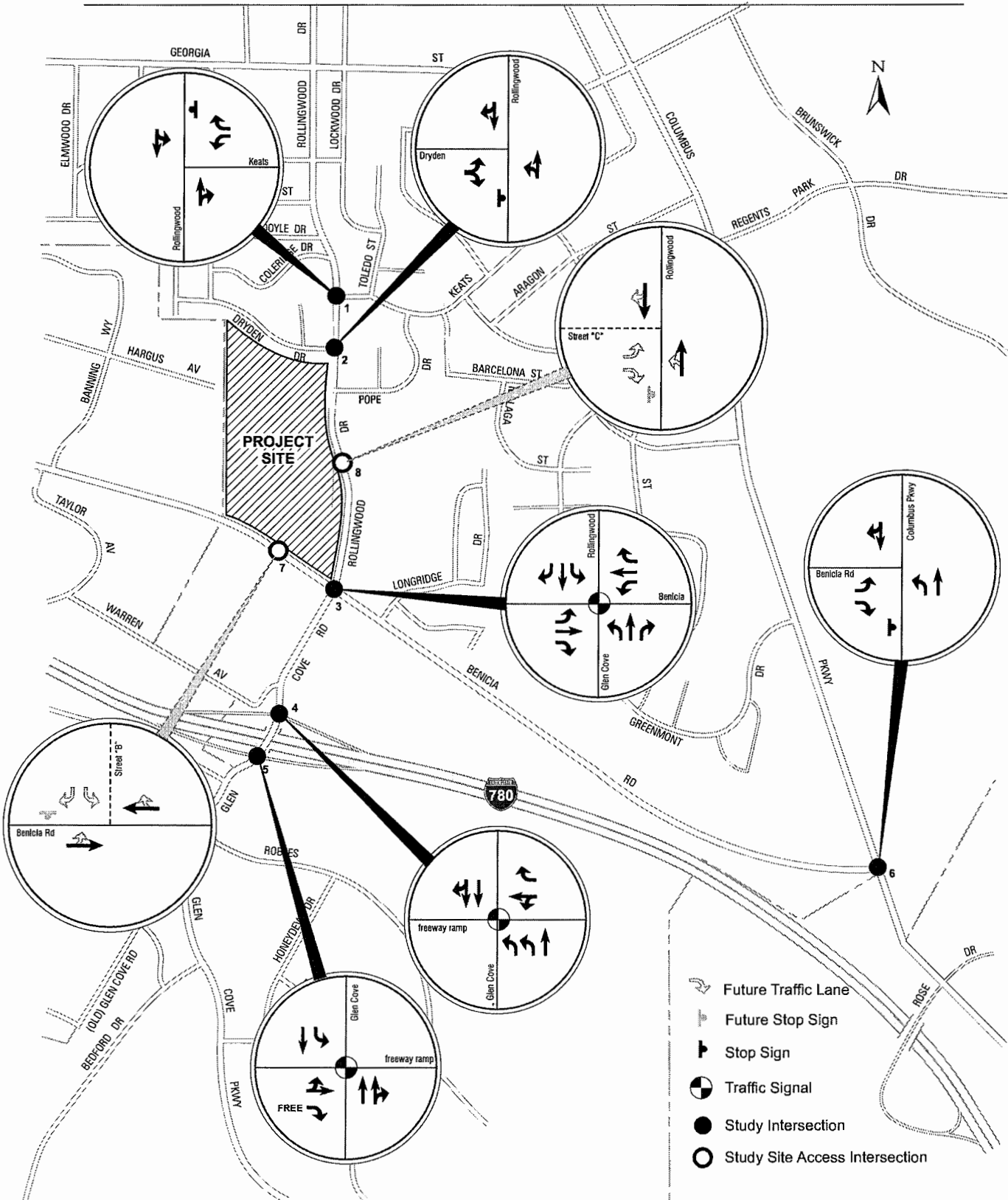


Existing Volumes.cdr

ROLLINGWOOD RESIDENTIAL

Figure 4

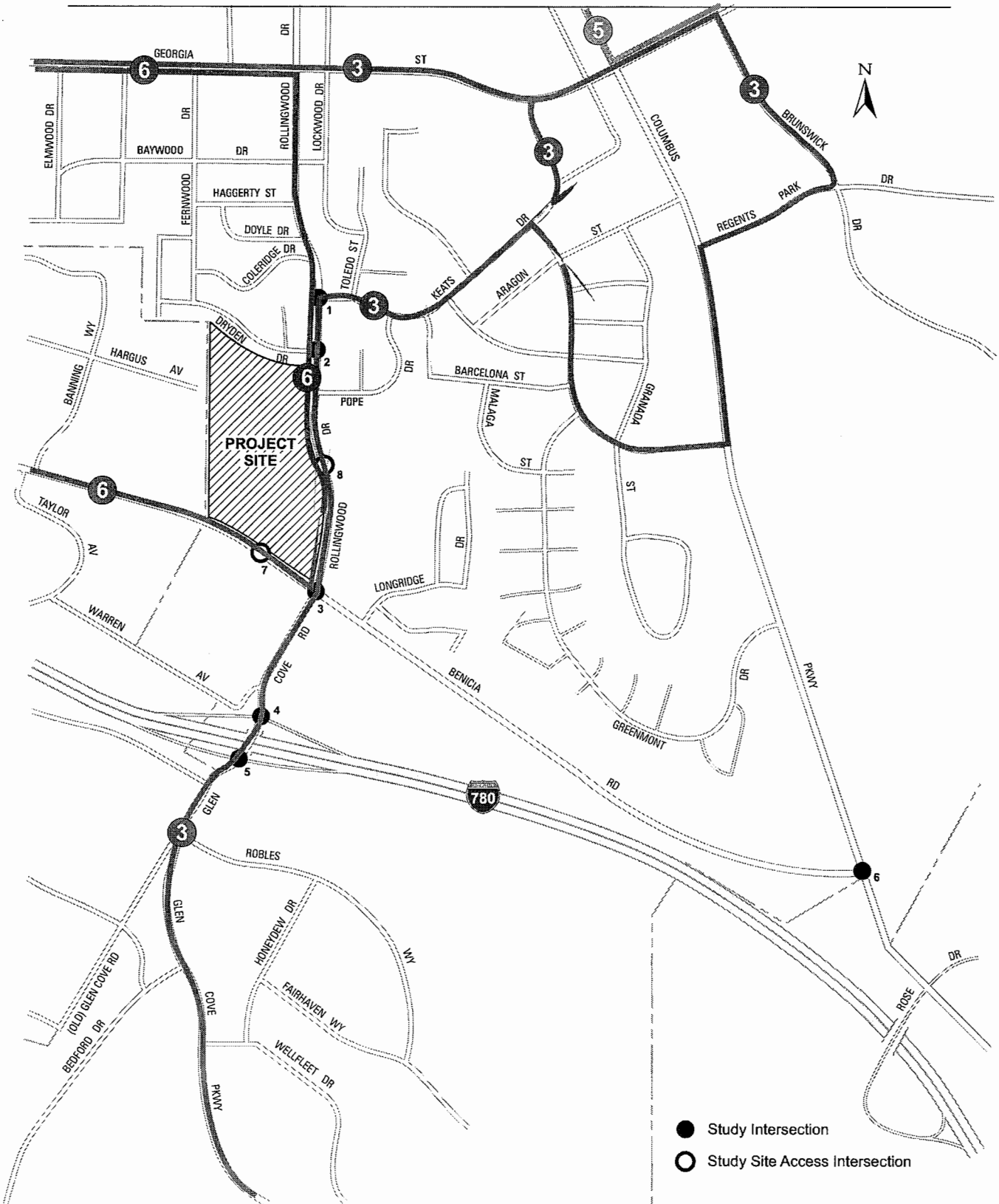
EXISTING TRAFFIC VOLUMES
AM (PM) Peak Hour



Existing Geometry.cdr

ROLLINGWOOD RESIDENTIAL

Figure 5
EXISTING INTERSECTION GEOMETRY

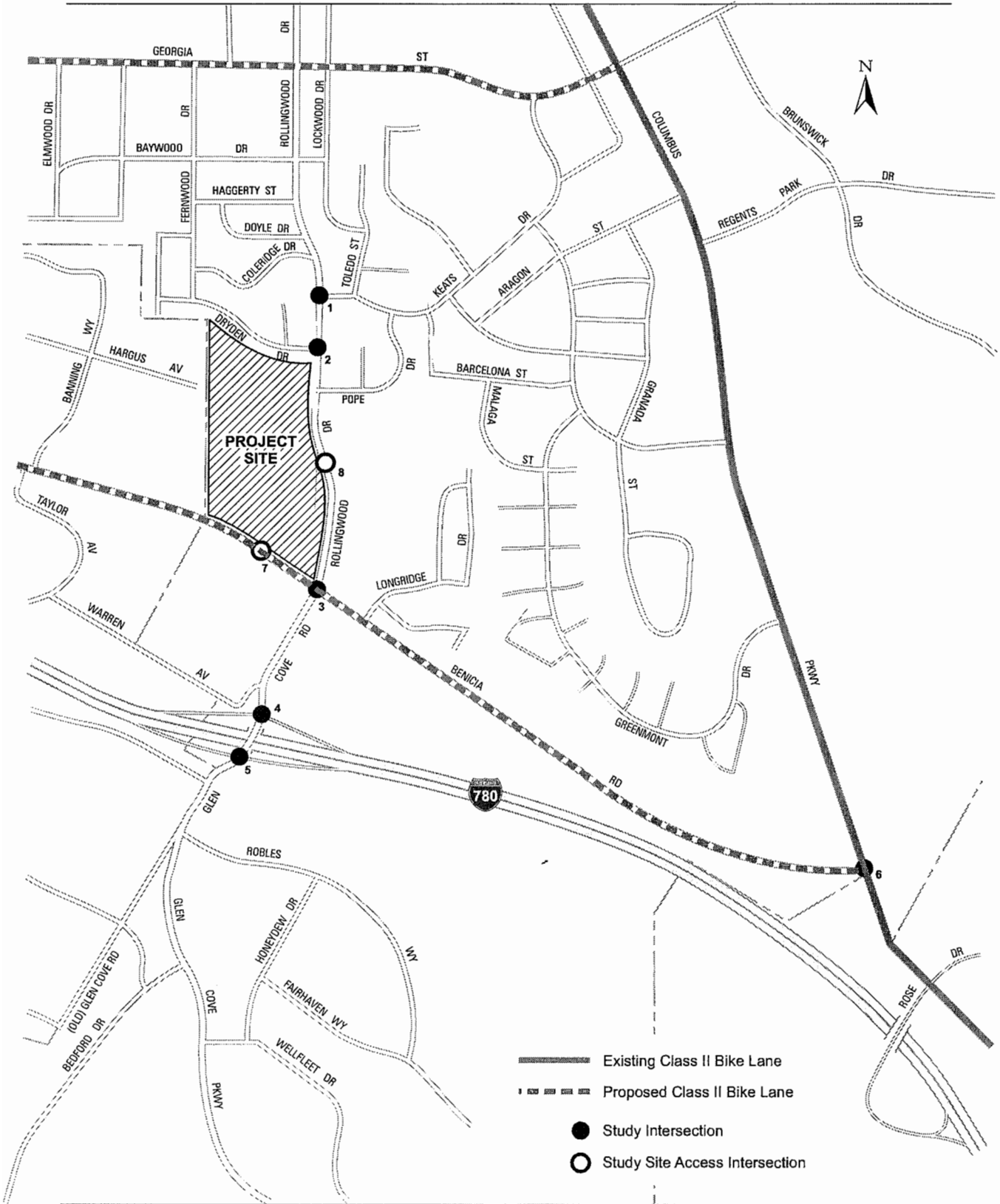


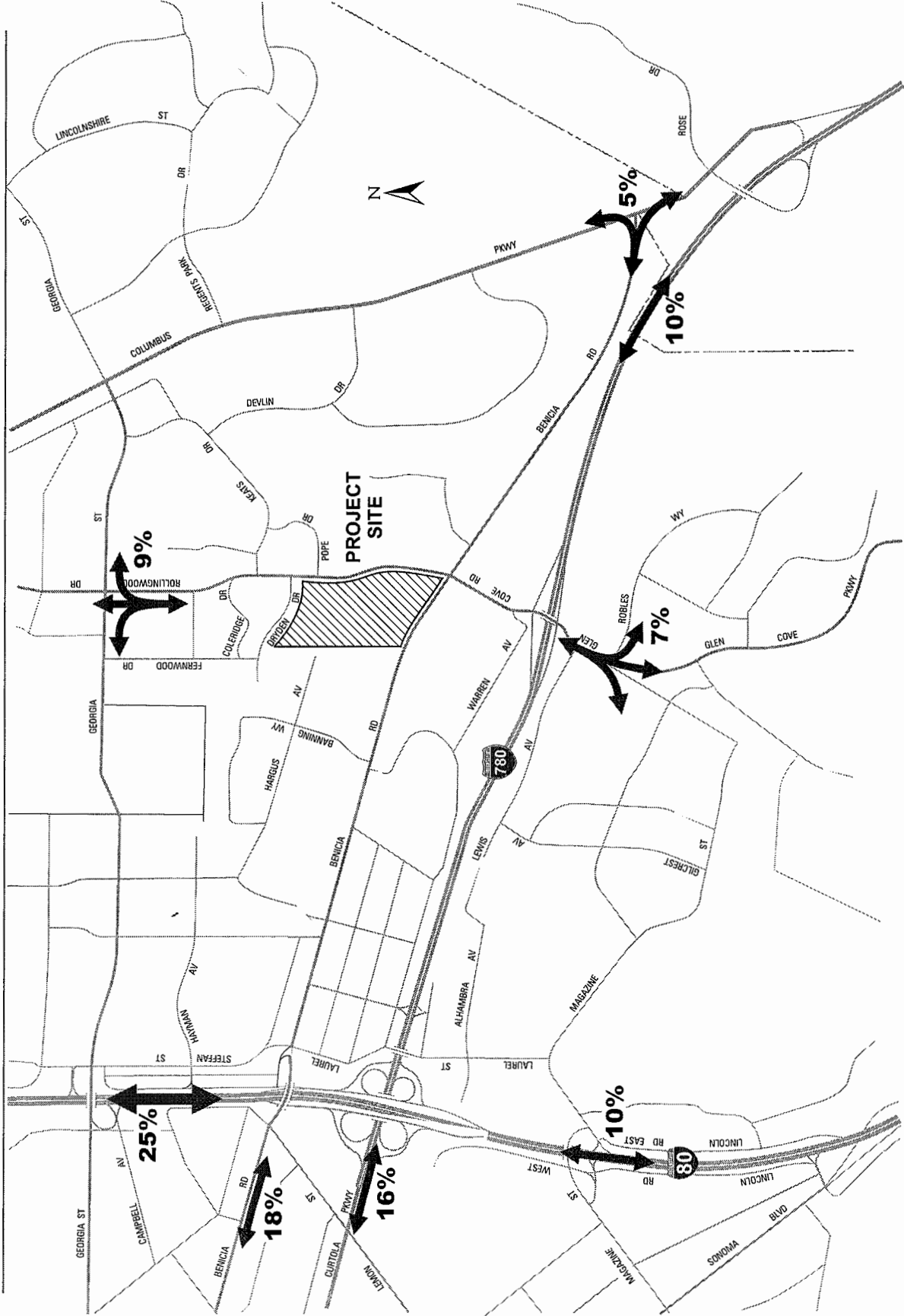
Transit.cdr

ROLLINGWOOD RESIDENTIAL

Figure 6

TRANSIT ROUTES IN PROJECT VICINITY



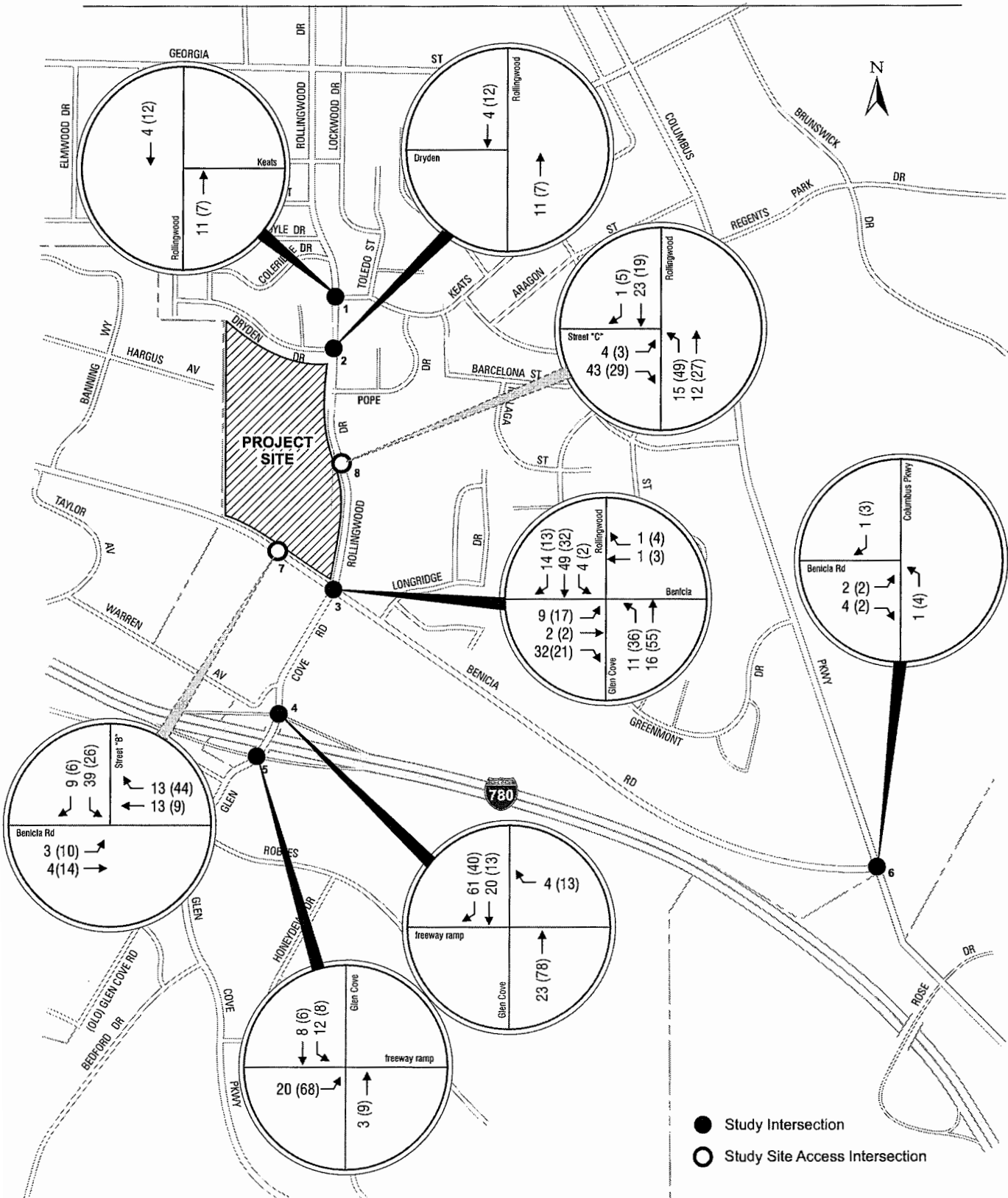


ROLLINGWOOD RESIDENTIAL

Figure 8

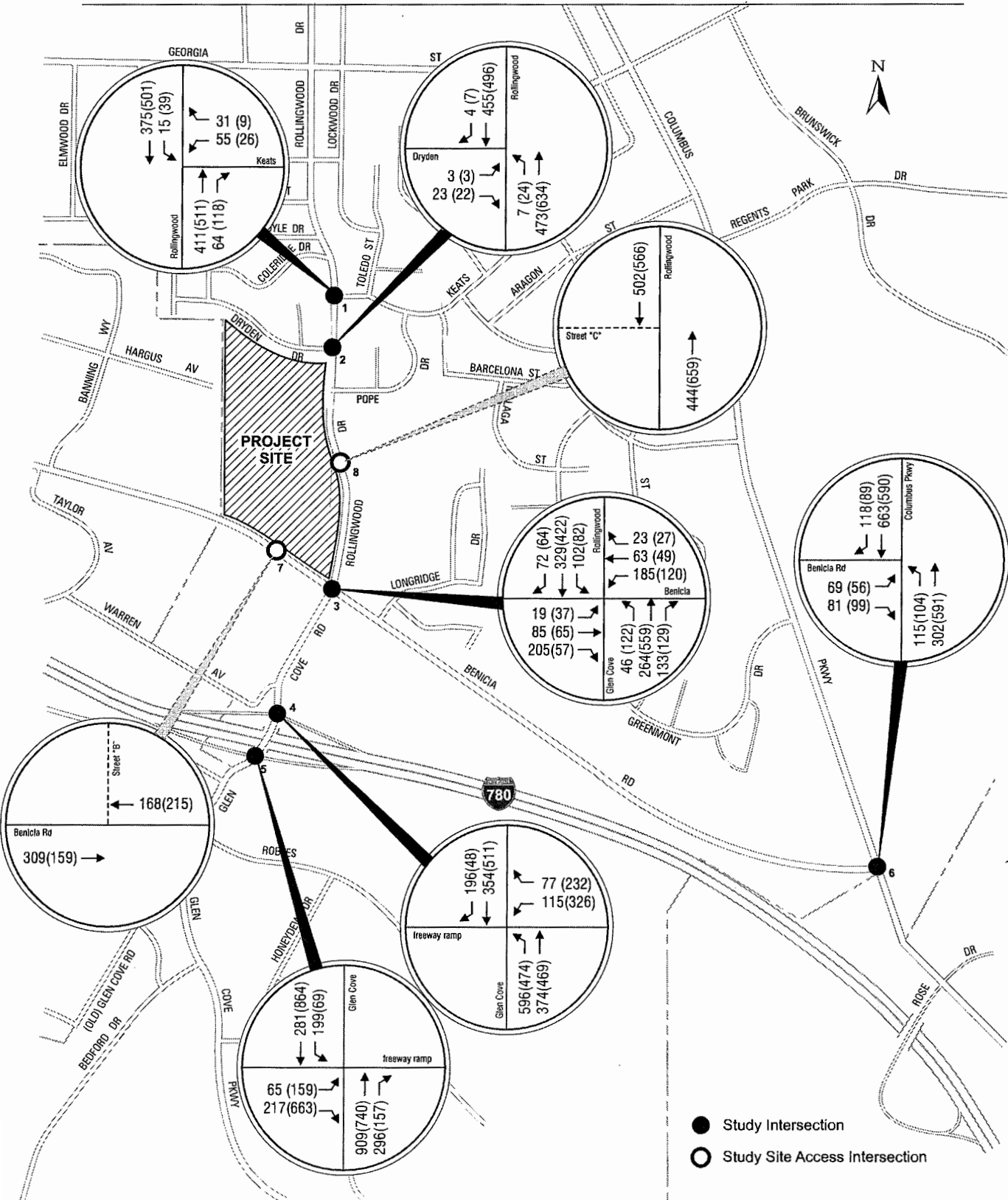
PROJECT TRIP DISTRIBUTION

The Distribution



Project Volume.cdr

ROLLINGWOOD RESIDENTIAL
Figure 9
PROJECT TRAFFIC VOLUMES
 AM (PM) Peak Hour



Cumulative Volumes.cdr

ROLLINGWOOD RESIDENTIAL
 Figure 10

**CUMULATIVE (YEAR 2020) TRAFFIC VOLUMES
 AM (PM) Peak Hour**

To The Citizens of Vallejo, California

I am writing to voice my concern over the recent decision to destroy our East Vallejo Baseball Field used daily by our children and their families, to allow developers to build more housing.

While I am not opposed to “controlled” growth in our community, I am absolutely against replacing the limited play areas in our neighborhood and replace them with homes, especially when homes are plentiful while Baseball Fields are not.

I expect that since I am just a “home-owner” and not a rich developer, my comments are not given much weight, but I am NOT the only voice concerned over the apparent decision made to tear down our East Vallejo Baseball Field.

Please consider what the greater impact will be when our Baseball Field is destroyed:

- 1) Children will no longer be able to walk down the street and play in a healthy environment.**
- 2) Parents will have to transport their children to another location just to allow them to play.**
- 3) Our children will be relegated to playing in the streets.**
- 4) And most importantly,
Children will wander the neighborhood with the possibility of getting into trouble.**

Are new homes truly more important than a Baseball Field, a place for family and friends to congregate and enjoy a peaceful, serene environment? Are we as a city that much in need of more concrete and asphalt that we are willing to destroy a place that our children can meet and play? I WOULD HOPE NOT!!

Should you find it in your heart to review and reconsider, I believe you will agree that leaving the Baseball Field for our neighborhood families is far more important than another “subdivision” of over-priced homes.

Thank you for giving me, along with other concerned citizens, the opportunity to voice our feelings over this very sensitive issue.

Please allow for the possibility that the residence that live and play in East Vallejo Baseball Field may actually know what is best in this situation and truly have the health and welfare of our children at heart.

Thanks for your time and attention to this letter, please email with any concerns on this matter at fmaldo07@yahoo.com

Sincerely,
Fernando Maldonado

CITY OF VALLEJO

Development Services Department

Attachment 7 - A



Planning Division
707-648-4326
707-552-0163 Fax

Building Division
707-648-4374
707-552-0163 Fax

Code Enforcement Division
707-648-4469
707-649-3540 Fax

555 SANTA CLARA STREET • P.O. BOX 3056 • VALLEJO • CA • 94590-5934

October 31, 2007

Fernando Maldonado
Fmaldo07@yahoo.com

Dear Mr. Maldonado,

I am writing in response to the email you sent "To the Citizens of Vallejo, California" regarding the development proposed at the site utilized by the East Vallejo Little League. The property on which the East Vallejo Little League ball fields are located was owned by the Vallejo Unified School District and leased to the East Vallejo Little League (EVLL). As you probably know, the School District has been suffering financial difficulties and, in an attempt to increase financial stability, has been selling off surplus properties. This property was determined to be surplus as no new schools are anticipated in the vicinity and was sold to a private developer. As the lease to EVLL has expired, the School District has no legal responsibility to continue providing use of the property to EVLL.

The proposed housing development includes a 4.1 acre neighborhood park, which would include one ball field along with other park amenities including a playground, a half basketball court, and soccer field that would provide recreational opportunities to the community at large. This new park will give children of all ages a place to play a variety of sports and will have picnicking and barbeque areas for families. In addition, the School District has been working with EVLL and has offered to provide space for three additional playing fields for their use, two at Springstown Junior High School and one at Pennycook Elementary School.

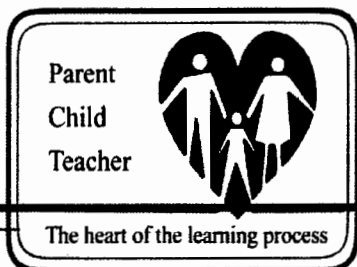
I appreciate your concern about the children of Vallejo and am glad to be able to assure you that the proposed development will provide a neighborhood park to serve the children and families in the area. As your letter was sent by email and did not include a return address, I am sending this response by email.

Sincerely,

Anthony Intintoli, Mayor

BOARD OF EDUCATION

Hazel Wilson
Daniel Glaze
Bill Pendergast
Catherine Prevolos
Cris "Oggee" Villanueva



Vallejo City Unified School District

STATE ADMINISTRATOR
Richard J. Damelio, Ed. D.

November 26, 2007

Ms. Alicia Griffin
East Vallejo Little League
1201 Granada Street
Vallejo, CA 94591

Subject: Application / Permit to Use School Property

Dear Alicia:

The Vallejo City Unified School District is in receipt of your application to use the fields at Springstowne Middle and Pennycook Elementary Schools for the purpose of Little League Baseball. The final approval of your application is pending the outcome of KB Home's meeting with the City of Vallejo on December 3, 2007, and receipt of an Insurance Certificate in the minimum amount of \$1,000,000 naming the Vallejo City Unified School District as additionally insured. However, we have tentatively scheduled both fields.

If you have any questions, please don't hesitate to call my office at (707) 556-8921 ext. 50112.

Sincerely,

A handwritten signature in black ink, appearing to read "Becky Oraboni".

Becky Oraboni
Facilities Coordinator

Cc: Floyd Gonella, Ed.D., Advisor to the State Administrator
Mel Jordan, Assistant Superintendent, Business Services and Operations
Brian Dolan, City of Vallejo
Todd Callaghan, KB Home

500 ft Conflict of Interest Map



CONFLICT OF INTEREST MAP (500-foot radius)

GPA 07-0001, ZMA 07-0002, PD 07-0007, and TM 07-0008
Northwest corner of Benicia Road and Rollingwood Avenue;
APNs: 0072-170-050, -060, -200, and -210



Start: 555 Santa Clara St
Vallejo, CA 94590-5922, US

End: [1-205] Rollingwood Dr
Vallejo, CA 94591, US










Notes:

Only text visible within note field will print.

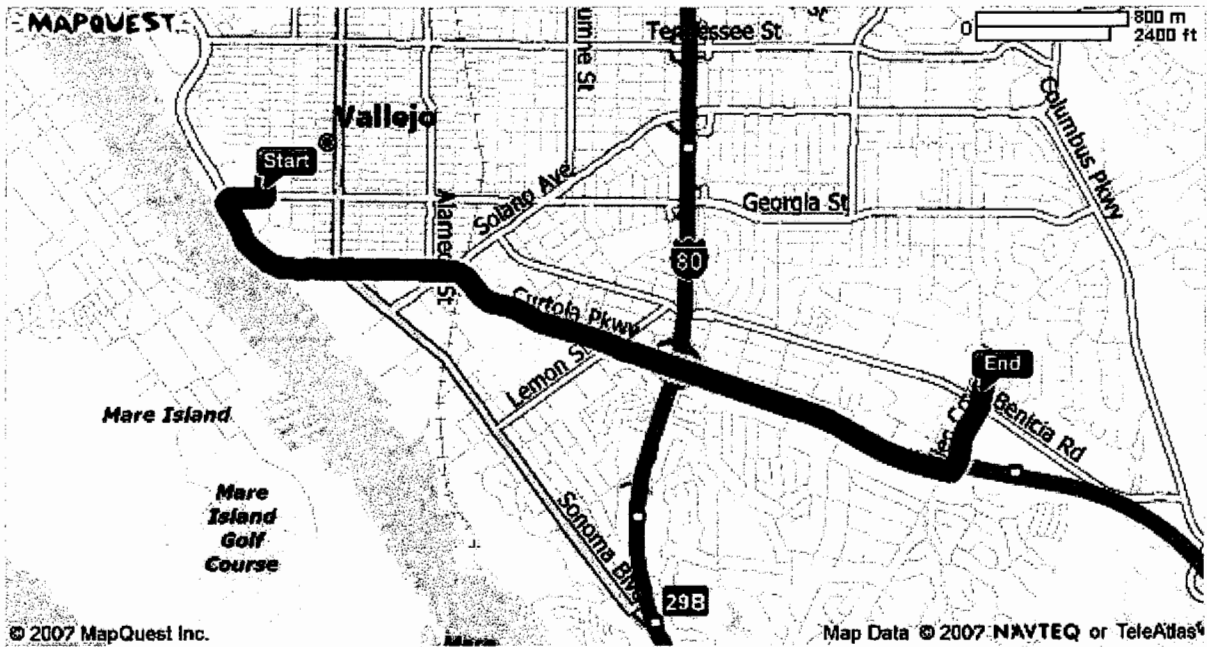
Directions

Distance

Total Est. Time: 7 minutes **Total Est. Distance:** 3.78 miles

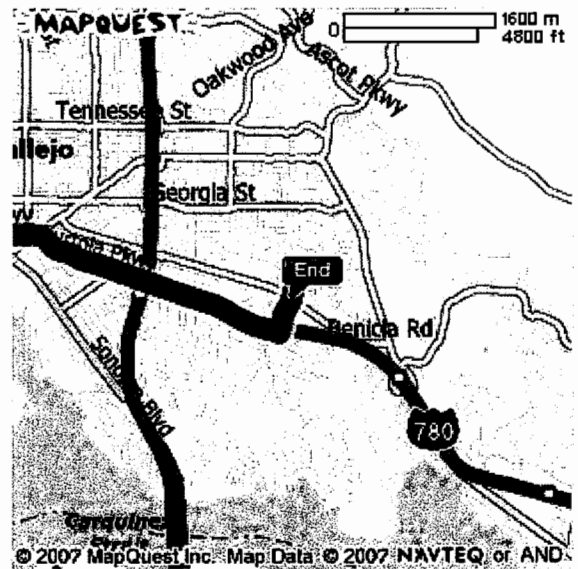
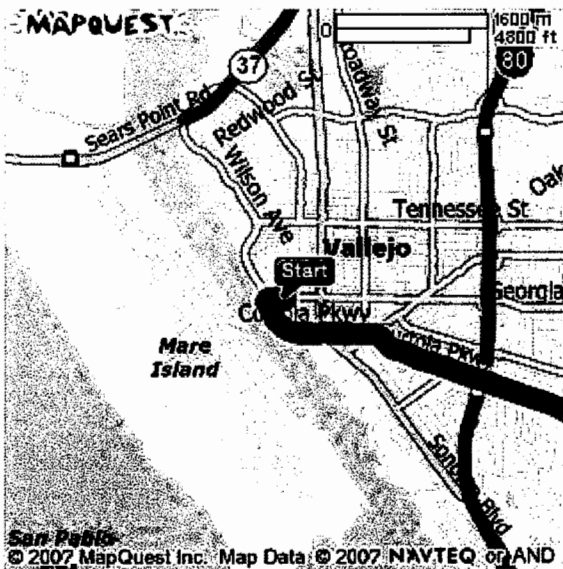
- 
1: Start out going SOUTH on SANTA CLARA ST toward GEORGIA ST. <0.1 miles
- 
2: Turn RIGHT onto GEORGIA ST. 0.1 miles
- 
3: Turn LEFT onto MARE ISLAND WAY. 0.3 miles
- 
4: MARE ISLAND WAY becomes CURTOLA PKWY. 1.4 miles
- 
5: CURTOLA PKWY becomes I-780 E. 1.1 miles
- 
6: Take the GLEN COVE RD exit. 0.2 miles
- 
7: Turn LEFT onto GLEN COVE RD. 0.2 miles
- 
8: GLEN COVE RD becomes ROLLINGWOOD DR. 0.1 miles
- 
9: End at [1-205] Rollingwood Dr
Vallejo, CA 94591, US

Total Est. Time: 7 minutes **Total Est. Distance:** 3.78 miles



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